



**MINUTES OF ORDINARY MEETING OF COUNCIL  
HELD ON 21 AUGUST 2012**

I hereby certify that the Minutes of the Ordinary Council Meeting held are a true and accurate record of the proceedings contained therein.

\_\_\_\_\_  
**Shire President Confirmed**

**Date:** \_\_\_\_\_

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**SHIRE OF WYNDHAM EAST KIMBERLEY  
AGENDA  
OF THE ORDINARY COUNCIL MEETING  
HELD ON TUESDAY, 21 AUGUST 2012 AT 5:00 PM.**

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**1. DECLARATION OF OPENING / ANNOUNCEMENT OF VISITORS**

The Shire President declared the meeting open at 5:10 pm.

**2. RECORD OF ATTENDANCE / APOLOGIES / LEAVE OF ABSENCE)  
PREVIOUSLY APPROVED**

**ATTENDANCE**

Cr J Moulden	Shire President
Cr R Addis	Deputy Shire President
Cr J McCoy	Councillor
Cr J Parker	Councillor
Cr D Ausburn	Councillor
Cr R Dessert	Councillor
Gary Gaffney	Chief Executive Officer
J Ellis	Director Corporate Services
K Hannagan	Director Infrastructure
J Ninnette	Planning Officer
E Davidson	Planning Officer
J Thompson	Acting Strategic Planner
T Whalan	Executive Assistant (Minute Taker)
Lana Reed	Communications and Media Officer

**GALLERY**

**APOLOGIES**

**LEAVE OF ABSENCE PREVIOUSLY APPROVED**

Cr K Torres	Councillor
Cr C Gore Birch-Gault	Councillor

### **3. DECLARATION OF INTEREST**

- **Financial Interest**

Nil

- **Impartiality Interest**

Nil

- **Proximity Interest**

Nil

### **4. RESPONSE TO PREVIOUS PUBLIC QUESTIONS TAKEN ON NOTICE**

Nil

### **5. PUBLIC QUESTION TIME**

Nil

### **6. PETITIONS**

Nil

**7. APPLICATIONS FOR LEAVE OF ABSENCE**

**Cr Jane Parker applies for a leave of absence for 23 October Ordinary Council Meeting meeting.**

**Cr Jackie McCoy applies for a leave of absence for 23 October Ordinary Council Meeting meeting.**

**COUNCIL DECISION**

**Minute No. 9852**

**Moved: Cr D Ausburn  
Seconded: Cr R Addis**

**That Council grants Cr Jane Parker a leave of absence for Ordinary Council Meeting, 23 October 2012.**

**Carried Unanimously 6/0**

**COUNCIL DECISION**

**Minute No. 9853**

**Moved: Cr D Ausburn  
Seconded: Cr R Addis**

**That Council grants Cr Jackie McCoy a leave of absence for Ordinary Council Meeting, 23 October 2012.**

**Carried Unanimously 6/0**

**8. CONFIRMATION OF MINUTES**

**8.1 CONFIRMATION OF MINUTES OF ORDINARY COUNCIL MEETING OF 24 JULY 2012**

**RECOMMENDATION**

**That Council confirms the Minutes of the Ordinary Council Meeting held on 24 July 2012**

**COUNCIL DECISION**

**Minute No. 9854**

**Moved: Cr D Ausburn  
Seconded: Cr J Parker**

**That Council confirms the Minutes of the Ordinary Council Meeting held on  
24 July 2012**

**Carried Unanimously 6/0**

**8.2 CONFIRMATION OF MINUTES OF SPECIAL COUNCIL MEETING OF 7  
AUGUST 2012**

**RECOMMENDATION**

That Council confirms the Minutes of the Special Council Meeting held on 7 August  
2012.

**COUNCIL DECISION**

**Minute No. 9855**

**Moved: Cr J Parker  
Seconded: Cr D Ausburn**

**That Council confirms the Minutes of the Special Council Meeting held on  
7 August 2012**

**Carried Unanimously 6/0**

**9. ANNOUNCEMENTS BY THE PERSON PRESIDING WITHOUT  
DISCUSSION**

Nil

**10. DEPUTATIONS / PRESENTATIONS / SUBMISSIONS**

Nil

**11. MINUTES OF COUNCIL COMMITTEE MEETINGS**

**11.1 MINUTES OF AUDIT COMMITTEE MEETINGS**

Nil

## 12. REPORTS

### 12.1 MATTERS ARISING FROM COMMITTEES OF COUNCIL

#### **RECOMMENDATION**

That the Audit (Finance and Risk) Committee recommends to Council to require the Wyndham Community Club Committee to:

1. Either
  - a. meet their outstanding rates debt on assessment A501 within 60 days; or
  - b. enter into a suitable payment plan approved by the Chief Executive Officer;and
2. Formalise the lease of Lot 472 Great Northern Highway, Wyndham with the Shire within 90 Days.

#### **COUNCIL DECISION**

**Minute No. 9856**

**Moved: Cr R Addis**

**Seconded: Cr R Dessert**

**That the Audit (Finance and Risk) Committee recommends to Council to require the Wyndham Community Club Committee to:**

1. **Either**
  - a. **meet their outstanding rates debt on assessment A501 within 60 days; or**
  - b. **enter into a suitable payment plan approved by the Chief Executive Officer;****and**
2. **Formalise the lease of Lot 472 Great Northern Highway, Wyndham with the Shire within 90 Days.**

**Carried Unanimously 6/0**

## 12.2 CORPORATE SERVICES

### 12.2.1 Monthly Financial Report

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Gill Old, Manager Financial Services
<b>REPORTING OFFICER:</b>	Jo-Anne Ellis, Director Corporate Services
<b>FILE NO:</b>	FM.09.5

#### **PURPOSE**

For Council to note and accept the Monthly Financial Report for June 2012.

#### **BACKGROUND**

Council is required to prepare Monthly Financial Reports as required by the Local Government (Financial Management Regulations) 1996.

#### **STATUTORY IMPLICATIONS**

Section 6.4 Local Government Act 1995  
Regulation 34, Local Government (Financial Management Regulations) 1996.

#### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of the report.

#### **FINANCIAL IMPLICATIONS**

Monthly financial reporting is a primary financial management and control process, it provides Council with the ability to oversee the Shire's financial performance against budgeted target.

#### **STRATEGIC IMPLICATIONS**

*Governance, Key Result Area 5,*  
Council's financial position and forward planning is sound

#### **COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

#### **COMMENT**

Councils 2011/2012 Budget was adopted on 19 July 2011. Budget review of 2011/2012 budget was adopted by Council on 17 April 2012.

Comments in relation to budget to actual variances are included as a note in the Financial Report.

Please note the final amounts for the end of financial year 2011/2012 may differ due to additional information received after the date of this report

**ATTACHMENTS**

Attachment 1 – Monthly Financial Report

**VOTING REQUIREMENT**

Simple Majority

**OFFICER'S RECOMMENDATION**

That Council accepts the Monthly Financial Report for the month of June 2012.

**COUNCIL DECISION**

**Minute No. 9857**

**Moved: Cr D Ausburn**

**Seconded: Cr J Parker**

**That Council accepts the Monthly Financial Report for the month of June 2012.**

**Carried Unanimously 6/0**



# Shire of Wyndham East Kimberley

## Monthly Financial Report 2011/2012

**As at 30 June 2012**

**Presented to Council 21 August 2012**

Contents:

- Statement of Financial Activity
- Note to Statement of Financial Activity (Net Current Asset Position)
- Note to Statement of Financial Activity (Explanation of Material Variances)
- Note to Statement of Financial Activity (Budget Remaining to Collect/Spend)
- Monthly Report on Investment Portfolio (Cash)

Financial Activity Legend:  
Above Budget Expectations: ▲  
Below Budget Expectations: ▼

# Shire of Wyndham East Kimberley

## Statement of Financial Activity Year to Date Actual v Year to Date Budget as at 30 June 2012

	YTD Actual 2011/12 \$	Amended YTD Budget 2011/12 \$	YTD Variance 2011/12 \$	%	
<b>Revenue</b>					
General Purpose Funding	6,524,888	4,280,257	2,244,631	52%	▲
Governance	1,031,637	514,476	517,161	101%	▲
Law, Order, Public Safety	41,177	107,936	(66,759)	-62%	▼
Health	57,683	54,550	3,133	6%	▲
Education and Welfare	171,867	143,750	28,117	20%	▲
Housing	538,222	887,488	(349,267)	-39%	▼
Community Amenities	2,179,198	2,145,118	34,080	2%	▲
Recreation and Culture	569,935	569,450	485	0%	▲
Transport	5,376,012	4,181,751	1,194,261	29%	▲
Economic Services	244,666	174,000	70,666	41%	▲
Other Property and Services	296,129	307,252	(11,123)	-4%	▼
	<u>17,031,413</u>	<u>13,366,028</u>	<u>3,665,385</u>	<u>27%</u>	<u>▲</u>
<b>Expenses</b>					
General Purpose Funding	(528,409)	(508,060)	(20,349)	4%	▼
Governance	(2,597,629)	(2,643,230)	45,601	-2%	▼
Law, Order, Public Safety	(578,671)	(618,795)	40,124	-6%	▼
Health	(348,845)	(414,222)	65,377	-16%	▼
Education and Welfare	(432,824)	(432,887)	63	0%	▼
Housing	(596,421)	(607,755)	11,334	-2%	▼
Community Amenities	(4,873,718)	(5,103,504)	229,786	-5%	▼
Recreation & Culture	(4,135,204)	(3,997,673)	(137,531)	3%	▲
Transport	(7,126,090)	(5,866,691)	(1,259,399)	21%	▲
Economic Services	(922,213)	(897,133)	(25,080)	3%	▲
Other Property and Services	(189,332)	(360,647)	171,316	-48%	▼
	<u>(22,329,356)</u>	<u>(21,450,596)</u>	<u>(878,760)</u>	<u>4%</u>	<u>▼</u>
<b>Adjustments for Cash Budget Requirements:</b>					
<b>Non-Cash Expenditure and Revenue</b>					
(Profit)/Loss on Asset Disposals	(297,721)	(760,643)	462,922	0%	▼
Movement in Accruals and Provisions	97,049	0	97,049	0%	▼
Depreciation on Assets	3,177,225	3,156,801	20,424	0%	▼
<b>Capital Expenditure and Revenue</b>					
Purchase Land Held for Resale	(18,216)	0	(18,216)	0%	▼
Purchase Land and Buildings	(7,181,301)	(8,425,723)	1,244,421	-15%	▼
Purchase Infrastructure Assets - Roads	(1,675,164)	(7,121,894)	5,446,730	-76%	▼
Purchase Infrastructure Assets - Footpaths	0	0	0	0%	▼
Purchase Infrastructure Assets - Drainage	(63,680)	(540,000)	476,320	0%	▼
Purchase Infrastructure Assets - Other	(2,117,222)	(3,067,464)	950,242	-31%	▼
Purchase Plant and Equipment	(627,360)	(1,019,500)	392,140	0%	▼
Purchase Furniture and Equipment	(467,895)	(752,421)	284,526	-38%	▼
Grants / Contributions for Development of Assets	7,954,968	10,656,009	(2,701,041)	0%	▼
Proceeds from Disposal of Assets	707,303	1,135,002	(427,699)	0%	▼
Proceeds from Sale of Land Held for Resale	0	0	0	0%	▼
Repayment of Debentures	(1,114,499)	(1,114,499)	0	0%	▼
Proceeds from New Debentures	0	0	0	0%	▼
Transfers to Reserves (Restricted Assets)	(821,324)	(649,782)	(171,542)	0%	▲
Transfers from Reserves (Restricted Assets)	2,469,942	3,083,826	(613,884)	0%	▼
ADD Estimated Surplus/(Deficit) July 1 B/Fwd	7,677,243	7,677,243	0	0%	▼
LESS Estimated Surplus/(Deficit) July 1 B/Fwd	9,020,407	714,855	8,311,552	114%	▲
<b>Amount Required to be Raised from Rates</b>	<u>6,628,002</u>	<u>6,542,467</u>	<u>85,535</u>	<u>1%</u>	<u>▼</u>

ADD

LESS

21 August 2012

12/06/12

## Shire of Wyndham East Kimberley

### Note to Statement of Financial Activity

#### Net Current Assets as at 30 June 2012

#### NET CURRENT ASSETS

#### Composition of Estimated Net Current Asset Position

	YTD Actual 2011/12	Brought Forward 1 July 2011
<b>CURRENT ASSETS</b>		
Cash - Unrestricted	7,245,497	2,232,123
Cash - Restricted	6,468,456	8,117,074
Cash - Restricted Unspent Grants	0	4,200,124
Receivables	3,376,598	3,482,496
Inventories	11,865	15,002
	<u>17,102,416</u>	<u>18,046,818</u>
<b>LESS: CURRENT LIABILITIES</b>		
Payables and Provisions	(1,604,553)	(2,252,502)
Less: Cash - Restricted	<u>(6,468,456)</u>	<u>(8,117,074)</u>
<b>NET CURRENT ASSET POSITION</b>	<u><u>9,029,407</u></u>	<u><u>7,677,243</u></u>

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

### Explanation of Material Variances

Variances +/- \$50,000 at Financial Statement Level

Variances +/- \$5,000 and 10% at Account Level

#### Operating

<b>Recurrent Income - Excluding Rates</b>		▲	
<b>General Purpose Funding</b>		▲	
Recovery of Legal Expenses - Rates	\$ 11,200	▲	Over achieved to annual budget estimate. Offsets higher expenditure in debt collection expenses.
Grants Commission - General Purpose Grant and Local Road Funding	\$ 2,278,800	▲	Advance payment of Financial Assistance Grant funding for 2012/2013
Interest Income - Municipal, Reserve and EKDP funds	\$ 46,700	▼	Budget overestimated and portion of interest quarantined for Kimberley Zone
<b>Governance</b>		▲	
Contributions (Secretariat) - Kimberley Zone	\$ 20,000	▲	Unbudgeted revenue received for Kimberley Aboriginal Community Housing Project fund management - Kimberley Zone
Housing Construction Facility Funding and Interest - Stage 1 - Feasibility Study - Kimberley Zone	\$ 433,600	▲	Unbudgeted funding received for housing construction facility project (\$404K) and interest (\$30K) paid on funds - Kimberley Zone.
WALGA Reimbursement for State Council Representation - Kimberley Zone	\$ 8,300	▼	Overestimated budget offset by reduced expenditure
Other income - Other Governance	\$ 17,400	▲	Unbudgeted revenue received for disposal of shares
Integrated Planning Framework (IPF) - Strategic Community Plan and Asset Management	\$ 55,100	▲	Unbudgeted \$50K received for Strategic Community Plan Northern Towns Fund and GST adjustment \$5K to contribution for Asset Management.
<b>Law, Order and Public Safety</b>		▼	
Reimbursements for Warmun and Gibb River Community evacuation expenses	\$ 64,100	▼	Reimbursement yet to be received. Carried forward to be received in 2012/2013 budget.
<b>Health</b>		▲	
Food Business Registration and Surveillance	\$ 5,100	▲	Overachieved estimated registrations.
<b>Education and Welfare</b>		▲	
Fees - Wyndham Childcare	\$ 27,200	▲	Over achieved due to higher number of enrolments.
<b>Housing</b>		▼	
Profit on Sale of Assets - Staff Housing	\$ 349,000	▼	Staff houses identified for sale remained unsold at end of financial year.
<b>Community Amenities</b>		▲	
Rubbish Disposal Charges and Landfill Recycling Income	\$ 144,400	▲	Over achieved due to higher usage of facility.
Wyndham Foreshore Protection and Enhancement Plan Funding	\$ 10,500	▼	Funding received on achievement of milestones not yet met. Carried forward to be received in 2012/2013 budget.
Local Planning Strategy Contribution	\$ 12,500	▼	Contribution not received. Negotiations continuing.
Kununurra Youth Services - Other Revenue	\$ 10,000	▲	Unbudgeted grant received for Kununurra Youth Program (media). Fully expended and offsets over expenditure in Kununurra Youth Officer program expenses.
Kununurra Youth Services - Colocation Fees and Reimbursements	\$ 9,000	▼	Revenue not achieved due to vacancies.
Traineeship Income Youth Services	\$ 13,800	▲	Unbudgeted grants received offset against employee expenses.
Youth Services Grants - Wyndham and East Kimberley Youth Services	\$ 102,500	▼	Grant payments pending independent audit. Carried forward to be received in 2012/2013 budget.
<b>Recreation and Culture</b>		▲	
Foreshore Leases	\$ 5,400	▼	Under achieved due to decreased lease charges invoiced on monthly basis.
Grant Income - Recreation Services	\$ 10,000	▼	Grant unsuccessful. Expenses incurred covered by income on activity.
Memberships and Group Fitness - Kununurra Leisure Centre	\$ 26,100	▲	Higher usage of these activities.
Grant Income - Club Development	\$ 50,000	▼	Grant paid on acquittal of expenditure. Carried forward to be received in 2012/2013 budget.
Grant Income - Kid Sport	\$ 30,000	▲	Unbudgeted grant received. Project expenses to occur in 2012/2013.

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

(continued)

### Operating (continued)

#### Recurrent Income - Excluding Rates (continued)

##### Transport ▲

Contribution to Valentine Falls Estate Rectification	\$ 147,700	▲ Un budgeted income. Fund held in bond DRAWN down to cover maintenance cost.
WANDRRA Funding and Emergency Works	\$ 1,141,900	▲ Grant income budgeted as capital, re-analysis of nature of income has resulted in actuals being transferred to operating.
Aircraft Landing Fees - Kununurra & Wyndham	\$ 122,400	▼ Under achieved to budget estimates. Reserved revenue.
Passenger Head Tax and Screening Fees - East Kimberley Regional Airport	\$ 68,400	▲ Over achieved to budget estimates, offset by higher expenses. Reserved revenue.
Leases - East Kimberley Regional Airport Terminal	\$ 37,700	▼ Under achieved due to vacant proposed leasable areas. Reserved revenue.

##### Economic Services ▲

Licences - Building Control	\$ 30,300	▼ Under achieved due to less activities than expected.
Project Officer Position Grant - other economic services	\$ 100,000	▲ Final grant payment received not in budget.

##### Other Property and Services ▼

Profit on Sale of Assets - Plant	\$ 9,600	▼ Full program of plant disposals identified not completed.
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#### Recurrent Expenditure ▼

##### General Purpose Funding ▼

Debt Collection Costs - Rates	\$ 12,600	▲ Offset by higher revenue of recovery of debt collection costs.
Emergency Services Levy Payment - Rates	\$ 6,300	▲ ESL on SWEK housing facilities not accounted for in budget estimate.

##### Governance ▼

Conferences and Travel - Members	\$ 7,600	▼ Savings achieved.
Integrated Planning Framework	\$ 86,900	▼ Project continuing in 2012/2013.
50 Year Anniversary Kununurra	\$ 14,000	▼ Savings achieved, activities rescoped.
Doubtful Debts	\$ 178,300	▲ Significant rates debt for owner under Administration provisioned for.
Admin Overheads Allocated - Other Governance	\$ 24,800	▲ Offset by savings achieved in other direct salaries and costs.
Occupational Safety and Health	\$ 19,800	▼ Nominal activity on project during year, carried forward to 2012/2013.
Zone and Kimberley Regional Collaborative Group Meeting Expenses	\$ 7,600	▼ Savings achieved, reallocation of funds to other activities.
Strategic & Business Planning Expenditure Phase 1 & 2 - Kimberley Regional Collaborative Group (KRCG)	\$ 82,600	▲ Refer to Zone minute 201206.7.2.4, reallocation of unspent funds to other activities.
Projects / Business Planning - Kimberley Zone	\$ 100,000	▼ Refer to Zone minute 201206.7.2.4, reallocation of unspent funds to other activities.
Housing Construction Facility - Stage 1 - Feasibility Study - Kimberley Zone	\$ 5,700	▲ Un budgeted expense set off against funds received for housing construction facility project - Kimberley Zone.

##### Law, Order and Public Safety ▼

Vehicle Expenses - Other Law, Order and Public Safety	\$ 19,100	▼ Savings achieved offset by other vehicle expenses in fleet.
Direct Salaries - Other Law, Order and Public Safety	\$ 17,200	▼ Savings achieved offset by salary expenses in other areas.

##### Health ▼

Vehicle Expenses - Preventative Services	\$ 16,100	▼ Savings achieved offset by other vehicle expenses in fleet.
Direct Salaries - Other Health	\$ 38,900	▼ Savings achieved offset by salary expenses in other areas.
Admin Overheads Allocated - Other Health	\$ 5,000	▼ Offset by expenses in other direct salaries and costs.

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

(continued)

### Operating (continued)

#### Recurrent Expenditure (continued)

##### Education and Welfare ▼

No material variances to report

##### Housing ▼

Building Maintenance - Dryandra Rd Kununurra	\$ 8,900	▼ Savings achieved offset by expenses on other properties.
Building Maintenance - Eugenia St Kununurra	\$ 8,400	▼ Savings achieved offset by expenses on other properties.
Building Maintenance - Hibiscus Dve Kununurra	\$ 11,700	▲ Initial works required on newly purchased property offset by savings on other properties.

##### Community Amenities ▼

Refuse Collection	\$ 16,600	▲ Over expended due to increased activities.
Litter Control	\$ 16,500	▼ Savings offset by other waste management activities.
Cumbungi Management - Protection of Environment	\$ 43,400	▼ Savings due to reduction in need and seasonal variances.
Waste Management Strategy - Protection of Environment	\$ 79,700	▼ Delays in project, activity will continue in 2012/2013.
Wyndham Foreshore Protection and Enhancement Plan	\$ 25,000	▼ No activity on project, will occur in 2012/2013.
Legal Expenses - Town Planning and Regional Development	\$ 9,100	▲ More activity requiring legal advice than expected.
Salary Expenses - Town Planning and Regional Development	\$ 125,800	▲ Over expended offset by under expenditure in other salary areas.
Community Grants	\$ 9,600	▼ Approved grants not claimed at end of financial year, carried forward to 2012/2013.
Graffiti Removal	\$ 12,300	▼ Savings, activity overestimated.
Playgrounds and Park Asset Maintenance	\$ 9,600	▼ Project activity undertaken in 2012/2013.
Public Conveniences	\$ 36,400	▼ Savings and demolition of old facilities.
Vehicle Expenses - Community Development Services	\$ 12,100	▼ Savings achieved offset by other vehicle expenses in fleet.
Salary and Administrative overheads - Community Development Services	\$ 17,100	▼ Savings achieved offset by salary and administrative expenses in other areas.
Salary, Vehicle & Program Expenses - Youth Services	\$ 107,800	▼ Youth positions & programs were reassessed with funding bodies. Externally funded.

##### Recreation and Culture ▲

Building Maintenance Peter Reid Memorial Hall	\$ 8,600	▼ Savings offset by expenses in other recreation and culture areas.
Kununurra Swimming Pool and Leisure Centre Operating Expenses	\$ 10,600	▲ Higher than expected costs for utilities.
Foreshores and Boat Ramps - Kununurra	\$ 16,600	▼ Savings offset by expenses in other recreation and culture areas.
Salary & Overhead Expenses - Kununurra Swimming Pool and Leisure centre	\$ 34,500	▲ Offset by savings in cost of salary and overhead in other areas.
Salary Expenses - Recreation Services	\$ 13,400	▲ Offset by savings in cost of salary and overhead in other areas.
Salary Expenses - Wyndham Youth and Recreation Services	\$ 21,100	▼ Savings offset by expenses in other recreation and culture areas.
Kununurra Hostel / Community Oval Preliminary Costs - East Kimberley Development Package	\$ 60,700	▲ Project funding withdrawn to actually construct asset. Preliminary costs apportioned as an operating expense, original budget as a capital expense. Externally funded.
Parks and Gardens Maintenance (Kununurra and Wyndham)	\$ 167,678	▲ Higher than expected maintenance and utility costs.
Direct Salaries Club Development	\$ 6,100	▲ Leave accruals not accounted for in budget. Externally funded.

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

(continued)

### Operating (continued)

#### Recreation and Culture (continued)

Kununurra Community Library Upgrade Project - Libraries	\$ 50,000	▼	Project carried forward to 2012/2013.
Building Operating Kununurra - Libraries	\$ 7,300	▼	Savings. Reimbursement to KDHS portion of expense.
Building Maintenance Historical Society	\$ 7,000	▼	Savings achieved.

#### Transport ▲

Rural Road Maintenance	\$ 59,900	▼	Timing, seasonal impacts.
Urban Road Maintenance - Kununurra & Wyndham	\$ 27,300	▼	Timing, seasonal impacts.
Valentine Falls Estate - Maintenance	\$ 28,200	▲	Un budgeted expenses, set off against fund held in the bond DRAWN down to cover maintenance cost.
WANDRRA Monsoonal Trough and Associated Flooding (10 March 2011) - Expenditure	\$ 1,464,500	▲	Expense budgeted as capital, re analysis of nature of expenditure has resulted in this being transferred to operating. Externally funded.
Passenger Screening Expenses - East Kimberley Regional Airport	\$ 78,500	▲	Increased activities in this area, offset by increased revenue. Reserve funded.
Vehicle Expenses and Plant Operating Costs - East Kimberley Regional Airport	\$ 28,500	▼	Savings. Reserve funded.
Consultancy Fees and Technical Services - East Kimberley Regional Airport	\$ 56,300	▼	Savings. Reserve funded.
Master planning - East Kimberley Regional Airport	\$ 36,600	▼	Project will be completed in 2012/2013. Reserve funded.
Building and Grounds Maintenance - East Kimberley Regional Airport	\$ 31,700	▲	Higher than expected operating costs, new facility.
Salary expenses - East Kimberley Regional Airport	\$ 16,800	▼	Savings. Reserve funded.
Building Maintenance and Repairs to Perimeter Fencing - Wyndham Airport	\$ 38,400	▼	Projects will be completed in 2012/2013. Reserve funded.
Operating and Outbuilding Maintenance - Wyndham Airport	\$ 83,900	▼	Savings. Reserve funded.

#### Economic Services ▲

Direct Salaries & Salary Overhead - Building Control	\$ 8,100	▲	Over expended offset by under expenditure in other salary areas.
Direct Salaries & Salary Overhead - Other Economic Services	\$ 9,500	▼	Under expended. Externally funded.
Sponsorship - Other Economic Services	\$ 20,000	▲	Expense against 2012/2013 budget paid early. Book entry will occur to correct.

#### Other Property and Services ▼

Administration salary expenses	\$ 6,300	▲	Over expended offset by under expenditure in other salary areas.
Information technology salary expenses	\$ 23,000	▼	Savings offset by expenses in other salary areas.
Executive salary expenses	\$ 156,500	▲	Over expended offset by under expenditure in other salary areas. Portion related to leave accrual provisions.
Synergy Training and Assistance and Non Core Software - Information Services	\$ 16,600	▼	Savings.
Software Licencing and Internet Connectivity - Information Services	\$ 33,500	▼	Savings.
Building and Equipment Operating and Maintenance - Kununurra & Wyndham Administration	\$ 39,500	▼	Reduced maintenance activities.
Vehicle Expenses Wyndham Administration	\$ 8,600	▲	Higher running costs than anticipated.
Fuel and Oils and Repair Costs	\$ 12,400	▲	Increased fuel & repair costs on fleet.
Plant Licences and 3P Insurance	\$ 7,100	▼	Savings.

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

(continued)

### Operating (continued)

#### Other Property and Services (continued)

Loss on Sale of Assets - Plant	\$ 14,200	▼ Full program of plant disposals identified not completed.
Direct Salaries & Overhead - Public Works Overheads	\$ 296,700	▼ Savings set off against direct salaries & overhead expenses in other departments.
Minor Plant / Equipment Purchases and Kununurra Depot Maintenance	\$ 14,100	▲ Over expended offset against other savings.
Engineering Consultancy	\$ 65,200	▼ Seasonal timing. Activities continuing 2012/2013.

### Non Cash Expenditure and Revenue

#### Adjustments and Accruals

Profit / Loss on Asset Disposals	\$ 462,900	▼ Property and plant program identified not completed.
Movement in Accruals and Provisions	\$ 97,049	▲ End of financial year adjustments. Non cash item.

### Capital

#### Purchase Land Held for Resale ▲

Land Subdivision - Stage One - East Kimberley Regional Airport	\$ 18,200	▲ Prior year budget included \$45K for this project, project was put on hold pending the outcome of the Airport Master Plan. Preliminary costs were invoiced by contractor. Will be funded from transfer from Airport Reserve.
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#### Purchase Land and Buildings ▼

SES Shed Relocation - FESA	\$ 280,000	▼ Project rescoped with further funding being sourced. Activity for 2012/2013.
East Kimberley Marine Rescue Shed	\$ 40,000	▼ Project to occur 2012/2013.
Capital Improvements - Kununurra Child Care Centre	\$ 27,500	▼ Project to continue 2012/2013.
Wyndham Staff Housing - Construction	\$ 13,900	▼ Savings.
Wyndham Staff Housing Caretakers Dwelling - Construction	\$ 9,100	▲ Unbudgeted GST payable on Caretakers dwelling construction expenses as per tax consultant advise.
Building Upgrades - Kununurra Swimming Complex	\$ 54,500	▼ Project to continue 2012/2013 due to contractors availability.
Gymnasium Expansion and Single Entry - Kununurra Leisure Centre	\$ 22,900	▼ Project to continue 2012/2013 due to contractors availability.
Airport Terminal Expansion - East Kimberley Regional Airport	\$ 225,400	▼ Project to continue 2012/2013. Reserve funded.
Development Residential Accommodation - Waterlily Place	\$ 106,900	▼ Project to continue 2012/2013. Grant funded.
Wyndham Depot Upgrade	\$ 36,700	▼ Project to continue 2012/2013.
Kitchen Upgrade Wyndham Office	\$ 8,100	▲ Project completed.
Banking Security Upgrade Wyndham Office	\$ 10,000	▼ Project rescoped and will proceed 2012/2013.
Kununurra Leisure Centre Hall Upgrade	\$ 310,700	▼ Project to continue 2012/2013.
Staff Housing - Construction - Bella Lane Kununurra	\$ 15,800	▼ Savings. Project completed.
Staff Housing - Purchase 4x2 Kununurra	\$ 110,000	▼ Savings. Property purchase price less than estimates.

#### Purchase Infrastructure Assets - Roads ▼

Kalumburu Road - Re-sheet	\$ 381,300	▼ Project to continue 2012/2013.
Road Reseals - Townsites	\$ 241,500	▼ Project to continue 2012/2013.
Weaber Plain Road - RRG - Reconstruct	\$ 773,600	▼ Project to continue 2012/2013.

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

(continued)

### Capital (continued)

#### Purchase Infrastructure Assets - Roads (continued)

Research Station Road - RRG - Road Repairs	\$ 240,300	▼	Project to occur 2012/2013.
Messmate Way Roundabout - Reconstruct and Seal	\$ 13,600	▼	Project to continue 2012/2013.
Weero Road - Construct and Seal	\$ 166,400	▼	Project to continue 2012/2013.
Research Station Road - Construct and Seal	\$ 192,700	▼	Project to continue 2012/2013.
Packsaddle Road - Road Shoulder Repairs	\$ 52,200	▼	Project to continue 2012/2013.
Cato Court - Reconstruction	\$ 150,000	▼	Project rescope to occur 2012/2013.
Egret Close - Construct and Seal	\$ 214,200	▼	Project to continue 2012/2013.
Kalumburu Road - RRG Projects	\$ 350,000	▼	Project to occur 2012/2013.
Gardenia Drive Reseal - RRG	\$ 65,200	▼	Project to continue 2012/2013.
WANDRRA Monsoonal Trough and Associated Flooding (10 March 2011) - Expenditure	\$ 2,709,000	▼	Expense budgeted as capital, re-analysis of nature of expenditure has resulted in actuals being transferred to operating. Externally funded.

#### Purchase Infrastructure Assets - Drainage ▼

River Farm Road - Drainage Upgrades	\$ 120,000	▼	Project to occur 2012/2013.
Drainage Feature Survey	\$ 120,000	▼	Project to occur 2012/2013.
D2 Drain Rehabilitation	\$ 60,200	▼	Project to continue 2012/2013.
Victoria Highway Kununurra Townsite - Drainage Upgrade	\$ 176,100	▼	Project to continue 2012/2013.

#### Purchase Infrastructure Assets - Other ▼

Pool Capital Repairs - Kununurra Swimming Complex	\$ 97,900	▼	Project to continue 2012/2013.
Water Feature - Wyndham Swimming Complex	\$ 5,750	▼	Project to continue 2012/2013.
Wyndham Community Jetty - East Kimberley Development Package	\$ 203,900	▲	Overspent to annual budget. Wyndham Jetty Management Plan not included in budget estimates plus tidy up works around entry.
Community Oval	\$ 517,280	▼	EKDP Funds have been removed as asset cannot be constructed prior to 30 June deadline. Preliminary costs moved to operating expenditure covered under funding held.
Multipurpose Court Redevelopment	\$ 50,000	▼	Project to occur 2012/2013.
Reticulation Works - East Kimberley Regional Airport	\$ 20,000	▼	Project to occur 2012/2013.
Town Entry Statements Kununurra - Tourism & Area Promotion	\$ 45,000	▼	Return of grant. Budget value carried in capital, but expended as operating.
Celebrity Tree Park - Playground	\$ 12,900	▼	Savings. Project completed. Partially reserve funded.
Whitegum / Warlarring Park Power and Lighting Upgrade	\$ 39,400	▼	Project to continue 2012/2013.
Celebrity Tree Boat Ramp - Planning	\$ 13,600	▼	Project to continue 2012/2013.
Lake Kununurra Foreshore Recreational Nature Trail	\$ 42,300	▼	Project to continue 2012/2013.
Park Lighting Upgrades	\$ 75,000	▼	Project to occur 2012/2013.
Wyndham Boat Ramp Pontoon Jetty	\$ 7,960	▼	Project to continue 2012/2013.
Leichart Street - Angle Parking	\$ 100,000	▼	Project to occur 2012/2013. Reserve funded.
Security Fence Upgrade - East Kimberley Regional Airport	\$ 6,150	▼	Project to continue 2012/2013.
Car Park Repairs - Wyndham Airport	\$ 19,950	▼	Project to occur 2012/2013.

# Shire of Wyndham East Kimberley

## Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

(continued)

### Capital (continued)

#### Purchase Infrastructure Assets - Other (continued)

Runway Maintenance - Wyndham Airport	\$ 19,950	▼ Project to occur 2012/2013.
Road Repairs Erskine Hill - Wyndham Airport	\$ 11,900	▼ Savings. Project completed. Reserve funded.
Kununurra Landfill Site - Fencing Upgrades	\$ 7,390	▼ Project to continue 2012/2013.
Wyndham Dual Use Footpath - Planning	\$ 20,000	▼ Project rescope, to occur 2012/2013.
Wyndham Waste Water Treatment Facility	\$ 35,000	▼ Project to occur 2012/2013.

#### Purchase Plant and Equipment ▼

Airport Plant - Purchase Price	\$ 87,000	▼ Plant program identified not completed. Reserve funded.
Light Plant - Purchase Price	\$ 288,300	▼ Plant program identified not completed.

#### Purchase Furniture and Equipment ▼

Kununurra Landfill Site - Recycling Shelving and Setup	\$ 50,000	▼ Project reviewed, not to be undertaken. Reserve funded.
Kununurra Youth Centre - Fit out	\$ 31,600	▼ Funding reviewed and grant body, not to be undertaken.
Wyndham Youth Service - Setup	\$ 20,450	▼ Project to continue 2012/2013. Externally funded.
Furniture and Equipment - East Kimberley Regional Airport	\$ 112,200	▼ Project to continue 2012/2013. Reserve funded.
Kununurra Projector Upgrade - Information Technology	\$ 7,000	▼ Project to occur 2012/2013.
GIS - Information Technology	\$ 14,900	▼ Project to continue 2012/2013.
Printer Replacements - Information Technology	\$ 33,800	▼ Project to continue 2012/2013.

#### Grants / Contributions for Development of Assets ▼

FESA Capital Grant - SES Shed Relocation	\$ 280,000	▼ Project rescope with further funding being sourced. Activity for 2012/2013.
Royalties for Regions Infrastructure Grant	\$ 947,700	▼ 2011/2012 funding not yet received until previous allocation spent and acquitted. To occur 2012/2013.
Multi Purpose Community Courts - Grants and Contributions	\$ 23,300	▼ Project delayed to 2012/2013 affecting timing of grant payment.
Aboriginal Roads Funding - State Grants	\$ 76,500	▼ Funding received after expenditure, to occur in 2012/2013.
Regional Road Group Grants	\$ 321,200	▼ Funding received after expenditure, to occur in 2012/2013.
Road to Recovery Grants	\$ 106,100	▼ Funding received after expenditure, to occur in 2012/2013.
Road Contributions - Weero Road	\$ 30,000	▼ Due to concerns over the finances of the developer the bond was cashed in on expiration. The \$30K difference was to be topped up by the developer but this appears unlikely at this stage. Other avenues to recover this cost need to be explored.
Hall Upgrade Kununurra Leisure Centre - Grants	\$ 147,600	▼ Funding received after expenditure, to occur in 2012/2013.
Celebrity Tree Boat Ramp - Grant	\$ 16,000	▼ Funding received after expenditure, to occur in 2012/2013.
Lake Kununurra Foreshore Recreational Nature Trail - Funding	\$ 75,000	▼ Funding received after expenditure, to occur in 2012/2013.
Housing Construction Facility Project Funding - Kimberley Zone	\$ 1,940,600	▲ Unbudgeted fund received for housing construction facility project - Kimberley Zone.
WANDRRA Funding - Monsoonal Event (Kalumburu Road)	\$ 2,600,000	▼ Grant income budgeted as capital, re-analysis of nature of income has resulted in actuals being transferred to operating.
Wyndham Dual Use Footpath - Planning Grant	\$ 20,000	▼ Funding received after expenditure, to occur in 2012/2013.

## Shire of Wyndham East Kimberley

### Notes to Statement of Financial Activity

For the Period Ended 30 June 2012

*(continued)*

#### Capital *(continued)*

##### Proceeds from Disposal of Assets

	▼	
Sale of Boobiulla Housing Kununurra	\$ 400,000	▼ Staff houses identified for sale remained unsold at end of financial year.
Airport Plant - Trade Value	\$ 9,091	▼ Plant program identified not completed.
Light and Medium Plant - Trade Value	\$ 22,900	▼ Plant program identified not completed.

##### Reserves

	▼	
Reserve Interest - Transfer to Reserve	\$ 58,500	▼ Less annual interest than anticipated.
Waste Management Reserve - Transfer to Reserve	\$ 256,010	▲ Transfer defined by activity income.
Foreshore Reserve - Transfer to Reserve	\$ 5,300	▼ Lease revenue less than anticipated.
Recreation Hardcourt - Transfer to Reserve	\$ 22,000	▼ Expected court rebate not received.
Waste Management Reserve - Transfer from Reserve	\$ 40,175	▼ Transfer defined by project expense.
Airport General Reserve - Transfer from Reserve	\$ 410,400	▼ Plant and project activities delayed to 2012/2013.
Parking Reserve - Transfer from Reserve	\$ 100,000	▼ Project to occur 2012/2013.
Staff Housing Reserve - Transfer from Reserve	\$ 10,900	▲ Transfer defined by project expense.
Recreation Hardcourts - Transfer from Reserve	\$ 26,600	▼ Project delayed to 2012/2013.
Parks Reserve - Transfer from Reserve	\$ 46,000	▼ Savings on one project and one project removed at budget review.

# Shire of Wyndham East Kimberley

## Note to Statement of Financial Activity

### Budget Remaining to Collect/Spend

as at 30 June 2012

	YTD Actual 2011/12 \$	Amended Annual Budget 2011/12	Budget Remaining 2011/12 \$	%	
<b>Revenue</b>					
General Purpose Funding	6,524,888	4,280,257	(2,244,631)	-52%	▲
Governance	1,031,637	514,476	(517,161)	-101%	▲
Law, Order, Public Safety	41,177	107,936	66,759	62%	▼
Health	57,683	54,550	(3,133)	-6%	▲
Education and Welfare	171,867	143,750	(28,117)	-20%	▲
Housing	538,222	887,488	349,267	39%	▼
Community Amenities	2,179,198	2,145,118	(34,080)	-2%	▲
Recreation and Culture	569,935	569,450	(485)	0%	▲
Transport	5,376,012	4,181,751	(1,194,261)	-29%	▲
Economic Services	244,666	174,000	(70,666)	-41%	▲
Other Property and Services	296,129	307,252	11,123	4%	▼
	<u>17,031,413</u>	<u>13,366,028</u>	<u>(3,665,385)</u>	<u>-27%</u>	<u>▲</u>
<b>Expenses</b>					
General Purpose Funding	(528,409)	(508,060)	20,349	-4%	▲
Governance	(2,597,629)	(2,643,230)	(45,601)	2%	▼
Law, Order, Public Safety	(578,671)	(618,795)	(40,124)	6%	▼
Health	(348,845)	(414,222)	(65,377)	16%	▼
Education and Welfare	(432,824)	(432,887)	(63)	0%	▼
Housing	(596,421)	(607,755)	(11,334)	2%	▼
Community Amenities	(4,873,718)	(5,103,504)	(229,786)	5%	▼
Recreation & Culture	(4,135,204)	(3,997,673)	137,531	-3%	▲
Transport	(7,126,090)	(5,866,691)	1,259,399	-21%	▲
Economic Services	(922,213)	(897,133)	25,080	-3%	▲
Other Property and Services	(189,332)	(360,647)	(171,316)	48%	▼
	<u>(22,329,356)</u>	<u>(21,450,596)</u>	<u>878,760</u>	<u>-4%</u>	<u>▲</u>
<b>Adjustments for Cash Budget Requirements:</b>					
<b>Non-Cash Expenditure and Revenue</b>					
(Profit)/Loss on Asset Disposals	(297,721)	(760,643)	(462,922)	61%	▼
Movement in Accruals and Provisions	97,049	0	(97,049)	0%	▲
Depreciation on Assets	3,177,225	3,156,801	(20,424)	-1%	▲
<b>Capital Expenditure and Revenue</b>					
Purchase Land Held for Resale	(18,216)	0	18,216	0%	▲
Purchase Land and Buildings	(7,181,301)	(8,425,723)	(1,244,421)	15%	▼
Purchase Infrastructure Assets - Roads	(1,675,164)	(7,121,894)	(5,446,730)	76%	▼
Purchase Infrastructure Assets - Footpaths	0	0	0	0%	▲
Purchase Infrastructure Assets - Drainage	(63,680)	(540,000)	(476,320)	88%	▼
Purchase Infrastructure Assets - Other	(2,117,222)	(3,067,464)	(950,242)	31%	▼
Purchase Plant and Equipment	(627,360)	(1,019,500)	(392,140)	38%	▼
Purchase Furniture and Equipment	(467,895)	(752,421)	(284,526)	38%	▼
Grants / Contributions for Development of Assets	7,954,968	10,656,009	2,701,041	25%	▼
Proceeds from Disposal of Assets	707,303	1,135,002	427,699	38%	▼
Proceeds from Sale of Land Held for Resale	0	0	0	0%	▼
Repayment of Debentures	(1,114,499)	(1,114,499)	(0)	0%	▼
Proceeds from New Debentures	0	0	0	0%	▼
Transfers to Reserves (Restricted Assets)	(821,324)	(649,782)	171,542	-26%	▲
Transfers from Reserves (Restricted Assets)	2,469,942	3,083,826	613,884	20%	▼
ADD Estimated Surplus/(Deficit) July 1 B/Fwd	7,677,243	7,677,243	0	0%	▼
LESS Estimated Surplus/(Deficit) Meeting Public Works	2,009,420	714,855	(8,314,552)	-126%	▲
<b>Amount Required to be Raised from Rates</b>	<u>6,628,002</u>	<u>6,542,467</u>	<u>(85,535)</u>	<u>-1%</u>	<u>▲</u>

## MONTHLY REPORT ON INVESTMENT PORTFOLIO (CASH)

### INVESTMENT POLICY F17

#### "Overall Portfolio Limits"

S&P Long Term Rating	S&P Short Term Rating	Direct Investment Maximum %	Managed Funds Maximum %
AAA	A-1+	100%	100%
AA	A-1	100%	100%
A	A-2	60%	80%

Note: "S & P" relates to Standard & Poors credit rating agency

#### "Counterparty Credit Framework"

S&P Long Term Rating	S&P Short Term Rating	Direct Investment Maximum %	Managed Funds Maximum %
AAA	A-1+	45%	50%
AA	A-1	35%	45%
A	A-2	20%	40%

#### "Term to Maturity Framework"

##### Overall Portfolio Term to Maturity Limits

Portfolio % < 1 year	100% max 40% min
Portfolio % > 1 year	60%
Portfolio % > 3 year	35%
Portfolio % > 5 year	25%

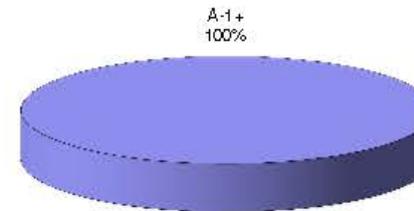
##### Individual Investment Maturity Limits

ADI	5 years
Non ADI	3 years

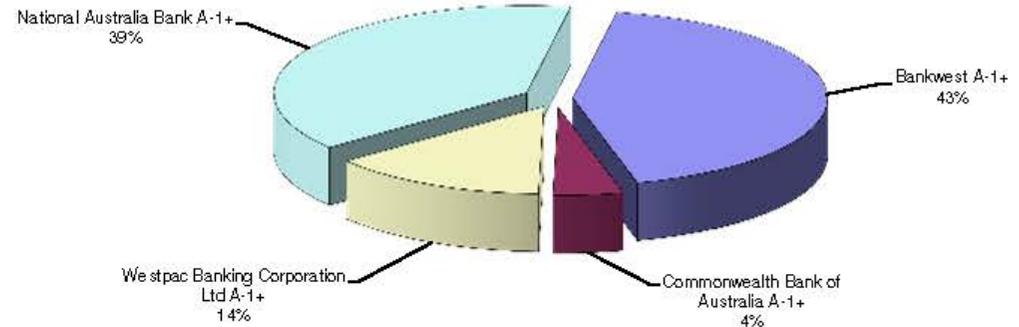
Note: "ADI" relates to an Authorised Deposit Institution (authorised under the Banking Act 1959)

### RESULTS AS AT 30 JUNE 2012

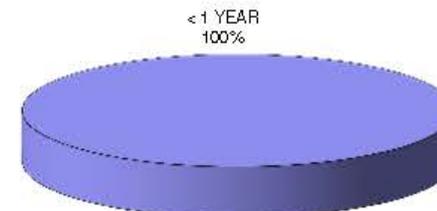
#### Overall Portfolio



#### Counterparty Credit

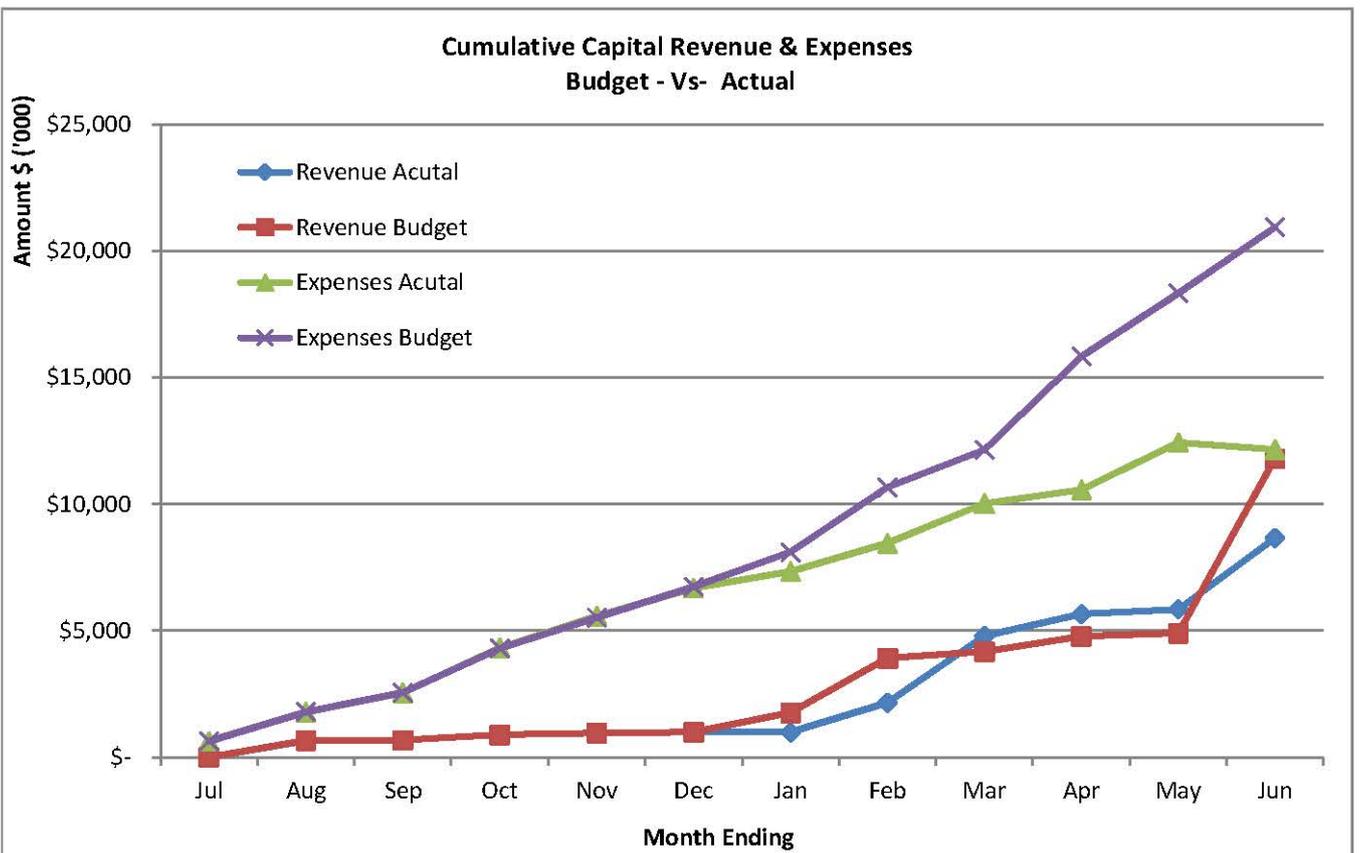
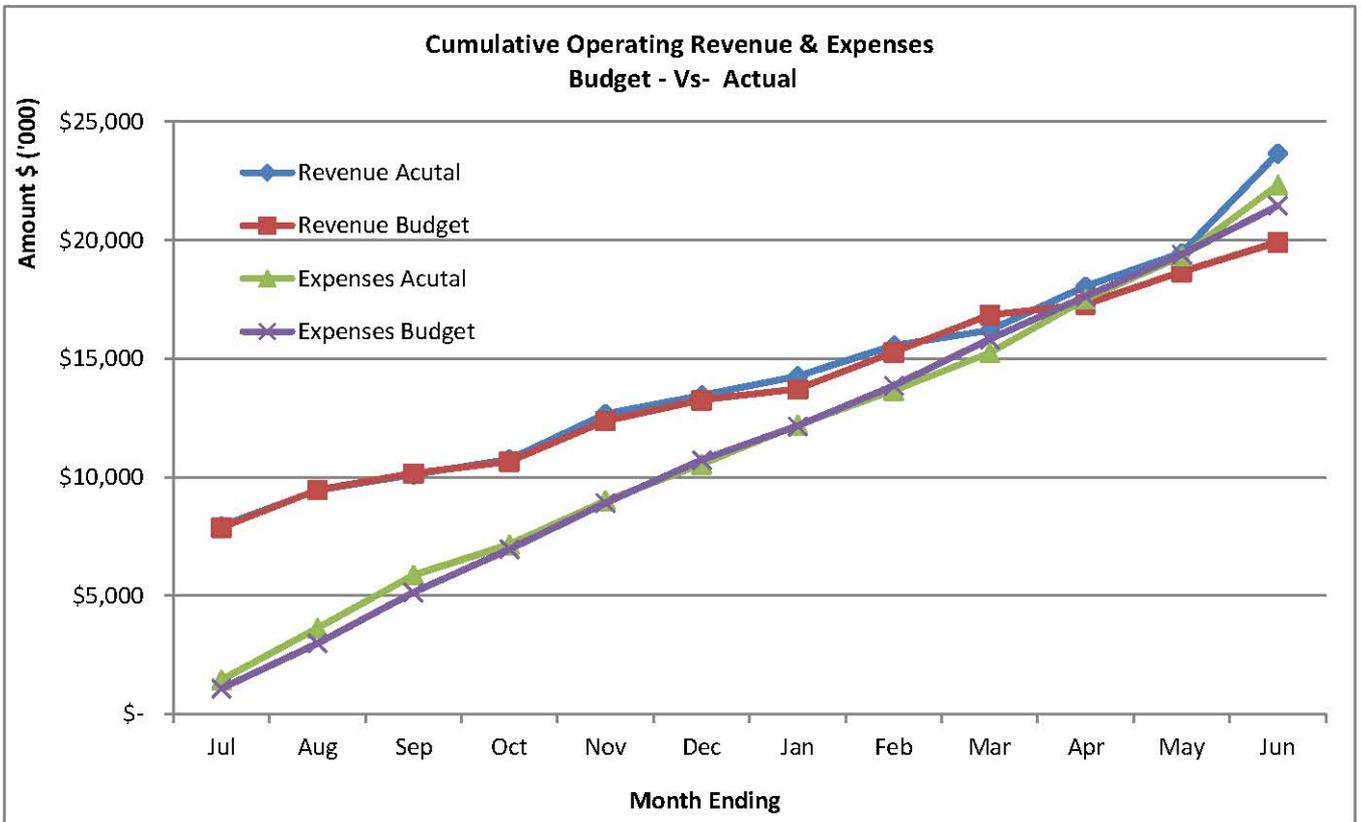


#### Term to Maturity



# Shire of Wyndham East Kimberley

Note to Statement of Financial Activity  
as at 30 June 2012



## 12.2.2 List of Accounts Paid Under Delegation

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Paulette Strongman, Finance Officer Asanka Jayakody, Team Leader Financial Services
<b>REPORTING OFFICER:</b>	Jo-Anne Ellis, Director Corporate Services
<b>FILE NO:</b>	FM.09.5

### **PURPOSE**

To present the listing of accounts paid from the Municipal Fund and Trust Fund in accordance with the requirements of the Local Government (Financial Management) Regulations 1996.

### **BACKGROUND**

Council delegated to CEO the exercise of its power under Financial Management Regulation 12 to make payments from Municipal Fund and Trust Fund at the Ordinary Council Meeting of 16<sup>th</sup> August 2011.

### **STATUTORY IMPLICATIONS**

Local Government Act 1995 – Section 5.42  
Local Government (Financial Management) Regulations 1996 – Regulations 12 and 13

### **POLICY IMPLICATIONS**

CD\GOV6113 – Payments from Municipal Fund and Trust Fund.

### **FINANCIAL IMPLICATIONS**

Ongoing management of Council funds by providing Council with sufficient information to monitor and review payments made.

### **STRATEGIC IMPLICATIONS**

*Governance, Key Result Area 5,*  
Council's financial position and forward planning is sound

### **COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

### **COMMENT**

In accordance with statutory requirements, each payment from the Municipal Fund or the Trust Fund is to be noted on a list compiled each month showing: the payee's name, amount of payment, date of payment and sufficient information to identify the transaction. The list is to be presented to Council at the next ordinary meeting of the Council following the

preparation of the list and is to be recorded in the minutes of the meeting at which it is presented.

**ATTACHMENTS**

Attachment 1 – List of Accounts Paid from Municipal Fund and Trust Fund

**VOTING REQUIREMENT**

Simple Majority

**OFFICER’S RECOMMENDATION**

That Council receives and accepts the listing of accounts paid from the Municipal and Trust fund, being:

Municipal EFT115738 – EFT116006 (05 Jul – 27 Jul 12)	\$ 1,310,569.07
Municipal cheques 41385 - 41451 (05 Jul – 02 Aug 12)	\$ 157,880.66
Trust cheques 385 - 405 (05 Jul –06 Aug 12)	\$ 528,782.21
Trust EFT 500321 - 500340 (02 Jul – 31 Jul 12)	\$ 22,234.15
Payroll (03 Jul – 25 Jul 12)	\$ 461,701.83
Direct bank debits (02 Jul – 31 Jul 12)	<u>\$ 38,835.41</u>
TOTAL	\$ 2,520,003.33

**COUNCIL DECISION**

**Minute No. 9858**

**Moved: Cr D Ausburn**

**Seconded: Cr R Dessert**

**That Council receives and accepts the listing of accounts paid from the Municipal and Trust fund, being:**

<b>Municipal EFT115738 – EFT116006 (05 Jul – 27 Jul 12)</b>	<b>\$ 1,310,569.07</b>
<b>Municipal cheques 41385 - 41451 (05 Jul – 02 Aug 12)</b>	<b>\$ 157,880.66</b>
<b>Trust cheques 385 - 405 (05 Jul –06 Aug 12)</b>	<b>\$ 528,782.21</b>
<b>Trust EFT 500321 - 500340 (02 Jul – 31 Jul 12)</b>	<b>\$ 22,234.15</b>
<b>Payroll (03 Jul – 25 Jul 12)</b>	<b>\$ 461,701.83</b>
<b>Direct bank debits (02 Jul – 31 Jul 12)</b>	<b>\$ 38,835.41</b>
<b>TOTAL</b>	<b>\$ 2,520,003.33</b>

**Carried Unanimously 6/0**

List of Accounts Submitted to Council 21 August 2012

EFT	DATE	NAME	DESCRIPTION	AMOUNT
EFT115738	05/07/2012	ABCO PRODUCTS	SCRUBBING/CLEANING MACHINE - WYNDHAM ADMIN	3,066.66
EFT115739	05/07/2012	AUSTRAL MERCANTILE COLLE. PTY LTD	LEGAL FEES – RATES/DEBTORS	3,987.03
EFT115740	05/07/2012	AVP COMMERCIAL POOLS	PAYMENT NO 1 - POOL LINER REPLACEMENT - KUNUNURRA LEISURE CENTRE	59,850.00
EFT115741	05/07/2012	ALL INTERACTIVE DISTRIBUTION - AI	LIBRARY VISUAL RESOURCES - WYNDHAM LIBRARY	1,546.51
EFT115742	05/07/2012	ARGYLE ENGINEERING	FABRICATE TOWING HITCH - P477	435.89
EFT115743	05/07/2012	ARGYLE MOTORS	SERVICE - P467	1,076.90
EFT115744	05/07/2012	ASANKA JAYAKODY	REIMBURSEMENT - APPROVED STUDY SUPPORT	815.00
EFT115745	05/07/2012	AUST LOCAL GOVERNMENT JOB DIRECTORY	ADVERTISE - SENIOR ENVIRONMENTAL HEALTH OFFICER - 11/06/12	869.00
EFT115746	05/07/2012	BOC GASES AUSTRALIAN LIMITED	WELDING GASES AND BOTTLE RENTAL - MAY 12	304.04
EFT115747	05/07/2012	BROADCAST AUSTRALIA PTY LTD	FACILITIES LEASING - SELF HELP - 01/06/12 - 31/05/13	275.00
EFT115748	05/07/2012	BUDGET RENT A CAR	CAR RENTAL - SHIRE PRESIDENT - WALGA STATE COUNCIL PERTH - 02-03/05/12	56.06
EFT115749	05/07/2012	BLACKWOODS ATKINS PTY LTD	MATTING & EARPLUGS - KUNUNURRA AIRPORT	2,263.00
EFT115750	05/07/2012	C & S JOLLY ELECTRICS PTY LTD	REPLACE CONTROL BOX ON POOL CLEANER - KUNUNURRA SWIMMING COMPLEX	121.00
EFT115751	05/07/2012	CENTURION TRANSPORT	FREIGHT - LITTLER CONTROL - KUNUNURRA DEPOT	87.45
EFT115752	05/07/2012	CARDNO (WA) PTY LTD	PROFESSIONAL SERVICES - WASTE MANAGEMENT STRATEGY - MAY 12	6,326.31
EFT115753	05/07/2012	CIVIC LEGAL	LEGAL ADVICE	1,201.20
EFT115754	05/07/2012	CROCODILE SIGNS PTY LTD	SIGNS & DECALS - AIRSIDE SECURITY WARNING - KUNUNURRA AIRPORT	1,489.40
EFT115755	05/07/2012	DEPT OF TREASURY & FINANCE - LIBRARY	FREIGHT CHARGES - INTER LIBRARY LOANS	1,054.19
EFT115756	05/07/2012	DAVEY TYRE & BATTERY SERVICE	NEW BATTERY - HAMMERHEAD - KUNUNURRA LEISURE CENTRE	196.00
EFT115757	05/07/2012	DRYSDALE RIVER STATION	DIESEL - PROJECT MANAGER	189.40
EFT115758	05/07/2012	EDUCATIONAL EXPERIENCE PTY LTD	TOYS, EQUIPMENTS - WYNDHAM CHILDCARE	1,012.22
EFT115759	05/07/2012	EAST KIMBERLEY GLASS	MIRRORS - KUNUNURRA DEPOT,	66.00
EFT115760	05/07/2012	EAST KIMBERLEY HARDWARE	VARIOUS HARDWARE ITEMS - SWIMMING COMPLEX & DEPOT - KUNUNURRA	214.85
EFT115761	05/07/2012	EAST KIMBERLEY PLUMBING	REPAIR -BBQ - SWIM BEACH KUNUNURRA	136.40
EFT115762	05/07/2012	FRONTIER FENCING	INSTALL OUTDOOR FURNITURE AND BOLLARDS - KUNUNURRA AIRPORT	3,718.00

EFT115763	05/07/2012	FUJI XEROX AUSTRALIA P/L	PRINTING COSTS - KUNUNURRA ADMIN & AIRPORT (MAIN) - MAR, APR, MAY 12	5,107.32
EFT115764	05/07/2012	FIVE RIVERS CAFE	CATERING - ANTHON'S LANDING COMMUNITY INPUT - PETER REID HALL WYN	181.50
EFT115765	05/07/2012	FIX ALL MOBILE MECHANIC	MECHANICAL REPAIRS - P357,P381. SERVICE - P216. PARTS - P357 - WYNDHAM	1,566.40
EFT115766	05/07/2012	GUERINONI & SONS	GRADING TABLE DRAINS - PARRY CREEK, 3 MILE VALLEY, MINDEROO -WYNDHAM	4,224.00
EFT115767	05/07/2012	GULLIVERS TAVERN	REFRESHMENTS - AIRPORT MANAGER FAREWELL - KNX AIRPORT	81.98
EFT115768	05/07/2012	IBAC PLUMBING PTY LTD	PLUMBING WORKS - TENNIS COURTS KNX & WYNDHAM DEPOT UPGRADE	16,613.85
EFT115769	05/07/2012	IMAGESOURCE	BUSINESS CARDS & FREIGHT	1,106.49
EFT115770	05/07/2012	IRRIBIZ	HIRE - FIRE FIGHTING PUMP, VARIOUS RETICS - KLC , IRRIGATION FITTINGS - WYN	635.64
EFT115771	05/07/2012	J & C ATKINS CONTRACTING PTY LTD	HIRE - WATER TRUCK TO ASSIST EXTINGUISH FIRE - WYNDHAM LANDFILL SITE	484.00
EFT115772	05/07/2012	JASON SIGNMAKERS LTD	ENGRAVING - NAMES PLATES - BABY TREE PROGRAMME KNX	511.50
EFT115773	05/07/2012	JSW HOLDINGS PTY LTD	CONCRETE - BARDING LOOP MAINTENANCE - KNX	330.00
EFT115774	05/07/2012	JANET TAKARANGI	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	154.95
EFT115775	05/07/2012	JORRITSMA H & CO	TUBING FOR TRANSPORT OF THE BANNER - BANNERS IN THE TERRACE	77.00
EFT115776	05/07/2012	KUNUNURRA VISITORS CENTRE	REIMBURSEMENT - PRINTED BALLOONS 50TH BIRTHDAY CIVIC FUNCTION - KNX	542.64
EFT115777	05/07/2012	KEN LOWTH	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	1,427.41
EFT115778	05/07/2012	KEVIN HANNAGAN	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	499.00
EFT115779	05/07/2012	KIM EDMEADES	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	231.95
EFT115780	05/07/2012	KIMBERLEY KOOL REFRIGE. & AIRCON.	REPLACE AC UNIT - COMMUNICATIONS HUT - KUNUNURRA DEPOT,	1,100.00
EFT115781	05/07/2012	KIMBERLEY VET CENTRE	EUTHANISE - 4 X FERAL CATS - KNX	200.00
EFT115782			CANCELLED PAYMENT	
EFT115783	05/07/2012	LEISURE INSTITUTE OF WA AQUATICS (INC)	LIWA MEMBERSHIP 12/13 - RECREATION OFFICER KNX	105.00
EFT115784	05/07/2012	MAGABALA BOOKS ABORIGINAL CORP	BOOKS - WRITERS FESTIVAL - KUNUNURRA	499.10
EFT115785	05/07/2012	MEGAN JANCZYK	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	222.25
EFT115786	05/07/2012	MERCURE INN BROOME	ACCOMM. & MEALS - CEO, PRESIDENT - RCG MEETINGS, BROOME - 02- 04/05/12	753.00
EFT115787	05/07/2012	MAXXIA	PAYROLL REDUCTIONS	2,796.61
EFT115788	05/07/2012	MCLEAN ENTERPRISES PTY LTD	VARIOUS FREIGHT - DEPOT - KUNUNURRA, WYNDHAM	71.50
EFT115789	05/07/2012	METALAND KUNUNURRA	REPAIR SHADE COVER - P362	14.09
EFT115790	05/07/2012	NETREGISTRY PTY LTD	RENEWAL - DOMAIN NAME SWEK.WA.GOV.AU FOR 2 YEARS	55.00
EFT115791	05/07/2012	NICK ALLEN	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	90.65
EFT115792	05/07/2012	ORDCO	HERBICIDES & INSECTICIDES - GENERAL MAINTENANCE KUNUNURRA	1,353.00

EFT115793	05/07/2012	ORICA AUSTRALIA PTY LTD	STORAGE - 4 X CHLORINE BOTTLES - WYNDHAM SWIMMING COMPLEX	1,434.40
EFT115794	05/07/2012	ORIA ORCHARDS	4 X WEEKLY FLOWER DELIVERY - KUNUNURRA OFFICE	60.00
EFT115795	05/07/2012	PIVOTEL	SERVICE CHARGES - 4 X SATELLITE PHONES - JUN 12	400.87
EFT115796	05/07/2012	PORTER CONSULTING ENGINEERS	ENGINEERING SERVICES - SEWER EXTENSION - WATERLILY PLACE KUNUNURRA	1,540.00
EFT115797	05/07/2012	PORTNER PRESS PTY LTD	EMPLOYMENT LAW PRACTICAL HANDBOOK - ORGANISATIONAL DEVELOP DEPT	197.00
EFT115798	05/07/2012	QBD BOOKSHOP	NEW BOOK STOCK - KUNUNURRA LIBRARY	183.63
EFT115799	05/07/2012	QK TECHNOLOGIES	QIKKIDS SOFTWARE WEB & ESTABLISHMENT FEE - WYN CHILDCARE CENTRE	1,300.33
EFT115800	05/07/2012	QUICK CORPORATE AUSTRALIA	STATIONERY, VARIOUS OFFICE SUPPLIES - KNX ADMIN	1,577.51
EFT115801	05/07/2012	SEEK LIMITED	3 STANDOUT AD FEATURE PACK WITH SEEK RECRUITMENT WEBSITE	115.50
EFT115802	05/07/2012	SJR CIVIL CONSULTING PTY LTD	CONCEPT DESIGN - BANDICOOT PARKING	660.00
EFT115803	05/07/2012	SQUASH MAGIC COMPANY	SQUASH EQUIPMENT - FOR RESELL - KUNNURRA LEISURE CENTRE	346.50
EFT115804	05/07/2012	SAFE FIRST TRAINING	FEES - TESTING,TAGGING,CORDS & PLUGS COURSE - BLD MAINT. OFFICER	729.00
EFT115805	05/07/2012	SHOP FOR SHOPS	TOWER SHOWCASE & FREIGHT - KUNUNURRA AIRPORT	969.20
EFT115806	05/07/2012	SUE GAFFNEY	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	410.47
EFT115807	05/07/2012	SURVEY NORTH	SURVEY SERVICES - WASTE MANAGEMENT FACILITIES - KNX & SURROUNDS	9,306.00
EFT115808	05/07/2012	THE CONSTABLE CARE CHILD SAFETY PRO.	QUICK GRANT - CONSTABLE CARE	550.00
EFT115809	05/07/2012	THORLEY'S STORE	2 X NEW GAS CYLINDERS - NEW STAFF HOUSING - KOOLAMA, WYNDHAM	308.00
EFT115810	05/07/2012	TOLL EXPRESS	VARIOUS FREIGHT - WYNDHAM DEPOT	576.66
EFT115811	05/07/2012	TERREL MACGREGOR	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	132.45
EFT115812	05/07/2012	THOMAS LOGISTICS PTY LTD	FREIGHT - DANGEROUS GOODS POSTERS - KUNUNURRA AIRPORT	15.00
EFT115813	05/07/2012	TOP END MOTORS	PRE REGISTRATION INSPECTION - P472	550.04
EFT115814	05/07/2012	TUCKERBOX / RETRAVISION KUNUNURRA	CATERING - AIRPORT MANAGER FAREWELL BBQ - KUNUNURRA	285.21
EFT115815	05/07/2012	WA LOCAL GOVERNMENT SUPER	SUPERANNUATION CONTRIBUTIONS	403.67
EFT115816	05/07/2012	WA LOCAL GOVERNMENT ASSOCIATION	ADVERTISING - SHIRE PROPERTIES FOR SALE - KIMBERLEY ECHO	447.67
EFT115817	05/07/2012	WYNDHAM EXCAVATIONS	CONCRETE, MACHINERY & LABOUR - FLOODWAY CONS.- THREE MILE VALLEY RD	25,000.00
EFT115818	05/07/2012	WYNDHAM SUPERMARKET	GROCERIES, CLEANING PRODUCTS - WYNDHAM CHIDCARE CENTRE	223.50
EFT115819	05/07/2012	WYNDHAM W.A GOLF CLUB	SPONSORSHIP - STARS ON THE BASTION - 7 JUL 12 - WYNDHAM	22,000.00
EFT115820			CANCELLED PAYMENT	
EFT115821	13/07/2012	AEC SYSTEMS	CIVIL 3D INTRODUCTION & ADVANCED TRAINING - ASSET MNGR OFFICER	2,200.00

EFT115822	13/07/2012	ALL PARK PRODUCTS PTY LTD	PRINTED CIRCUIT BOARD CONTROLLER - SWIM BEACH PARK BBQ	563.20
EFT115823	13/07/2012	ATO CHILD SUPPORT AGENCY	PAYROLL DEDUCTIONS	171.83
EFT115824	13/07/2012	AUSFUEL AFD AUSTRALIAN FUEL DISTRI.	FUEL CHARGES - JUN 12	1,940.40
EFT115825	13/07/2012	ALL SEASONS KUNUNURRA	FUNCTION FACILITY HIRE - COMMUNITY PLANNING SESSION - 26/6/12	1,790.00
EFT115826	13/07/2012	ALLCLEAN COMMERCIAL CLEANERS	CLEANING - AIRPORT, YOUTH CENTRE, ADMIN, TOILETS, KLC - KNX -MAY 12	28,100.50
EFT115827	13/07/2012	ALLGEAR MOTORCYCLES AND SMALL ENGI.	PA HIRE, OIL, WHIPPY SPOOLS, BUMPHEADS, BAR & CHAIN OIL - P356	958.70
EFT115828	13/07/2012	ARGYLE MOTORS	REPAIRS & PARTS - P468, P358, P314. SERVICE - P358, P213, P110, P314	6,236.10
EFT115829	13/07/2012	AUSTRALIA POST - ACCOUNTS RECEIVABLE	POSTAGE AND STATIONERY PUCHASES - JUN 12	942.11
EFT115830	13/07/2012	AUSTRALIAN SERVICES UNION	PAYROLL DEDUCTIONS	183.20
EFT115831	13/07/2012	B VISUAL MEDIA	PRODUCTION ASSISTANT MENTOR COSTS- KNX YOUTH DEVELOP. PROGRAM	2,422.00
EFT115832	13/07/2012	BOC GASES AUSTRALIAN LIMITED	WELDING GASES AND BOTTLE RENTAL - JUN 12	294.32
EFT115833	13/07/2012	BODAN CONSTRUCTIONS PTY LTD	SUPPLY - SHADE CLOTH, VENATION BLINDS, FENCE - CARETAKERS UNIT - WYN	7,317.20
EFT115834	13/07/2012	BROADCAST AUSTRALIA PTY LTD	FACILITIES LEASING - SBS TV (MT ALBANY)	74.28
EFT115835	13/07/2012	BLACKWOODS ATKINS PTY LTD	GREASE & TRUCK WASH - P356	164.75
EFT115836	13/07/2012	BUNNINGS (DWN)	BI FOLD TABLES - KUNUNURRA LEISURE CENTRE	1,096.92
EFT115837	13/07/2012	CENTURION TRANSPORT	FREIGHT - PERMAPOLE - DEPOT, PARTS - P382	8,411.26
EFT115838	13/07/2012	CPS WEAR PARTS	NEW GRADER BLADES, NUTS & BOLTS - P477	1,679.48
EFT115839	13/07/2012	CIVIC LEGAL	LEGAL FEES	731.28
EFT115840	13/07/2012	CLEVERPATCH	CRAFT MATERIALS - STORYTIME & ACTIVITIES - KUNUNURRA LIBRARY	585.97
EFT115841	13/07/2012	CR JANE PARKER	TRAVEL REIMBURSEMENT - COUNCIL MEETINGS - FEB - JUN 12	1,880.00
EFT115842	13/07/2012	CROCODILE SIGNS PTY LTD	VARIOUS SIGNS & DECALS - KUNUNURRA AIRPORT	429.00
EFT115843	13/07/2012	DEPT OF TREASURY & FINANCE - LIBRARY	RECOVERIES OF LOST BOOKS - KUNUNURRA LIBRARY	1,177.00
EFT115844	13/07/2012	DATA #3 LICENSING SOLUTIONS	MICROSOFT ANNUAL LICENCING RENEWAL - 12/13	57,237.93
EFT115845	13/07/2012	E & MJ ROSHER PTY LTD	FUEL, INNER, HYD FILTERS & INNER ELEMENT, 6 X NEW TYRES - P382, P384	1,604.60
EFT115846	13/07/2012	EAST KIMBERLEY HARDWARE	WHITE PAINT - KOOLINDA ST WYN, VARIOUS HARDWARE ITEMS - DEPOT - KNX	1,113.75
EFT115847	13/07/2012	FUJI XEROX AUSTRALIA P/L	PRINTER COSTS - DEVELOPMENT, KLC, COMMUNITY, KYC, DEPOT - KNX, WYN	4,914.19
EFT115848	13/07/2012	FARMERS FRUIT AND VEG MART	CATERING - LAUNCH OF PINDAN BOOK - 23/5/12	602.00
EFT115849	13/07/2012	FRANMOR CONSTRUCTIONS PTY LTD	REPAIR - TOILET DOOR - KLC, FIT GUNSAFE - DEPOT, AIRPORT,RANGER KNX, WYN	1,640.57
EFT115850	13/07/2012	FULLY JUICED ELECTRICS	REPLACE FLURO TUBE LIGHTS WITH DIFFUSERS - KUNUNURRA DEPOT	380.60
EFT115851	13/07/2012	GHD PTY LTD	CONSULTANCY SERVICES - VICTORIA HIGHWAY CULVERT UPGRADE	2,776.95

EFT115852	13/07/2012	GTS GAUGES TRANSMITTERS SWITCHES	NEW GPS TRIP METER - P116	842.60
EFT115853	13/07/2012	GUERINONI & SONS	SHOULDER RECONSTRUCTION - PACKSADDLE ROAD	55,163.02
EFT115854	13/07/2012	HENDRICKSE, JUDY	REIMB. - RETURN AIRFAIRS - KIMBERLEY WRITERS FESTIVAL	788.20
EFT115855	13/07/2012	HOLIDAY INN ESPLANADE DARWIN	ACCOMMODATION – WORKERS COMPENSATION RELATED	462.00
EFT115856	13/07/2012	IBAC PLUMBING PTY LTD	PLUMBING WORKS - REPLACE SILICONE IN SHOWER - RIVERFIG AVE, KNX	200.42
EFT115857	13/07/2012	IML LOGISTICS	DELIVERY CHLORINE & GAS DRUMS - KUNUNURRA LEISURE CENTRE	2,603.21
EFT115858	13/07/2012	IT VISION ITV	IMPLEMENTATION SYNERGY MODULES AND TRAINING - IT UPGRADE	14,506.25
EFT115859	13/07/2012	IMAGESOURCE	4 X 500 BUSINESS CARDS	985.49
EFT115860	13/07/2012	JAB INDUSTRIES	WATER TRUCK/ROLLER -THREE MILE, KOOLAMA, COLE AVE, CONCRETE - WYN	9,705.85
EFT115861	13/07/2012	KIMBERLEY ECHO - ALBANY ADVERTISER	ADVERTISING - SWEK NEWS - JUN 12	4,147.46
EFT115862	13/07/2012	KIMBERLEY MOTORS	FUEL - WYNDHAM - MAY 12	8,694.81
EFT115863	13/07/2012	KIMBERLEY VET CENTRE	EUTHANISE - 1 X FERAL CAT - KUNUNURRA	50.00
EFT115864	13/07/2012	KIMBERLEY WASTE SERVICES	SKIP EMPTY, STREET SWEEPING, LITTER COLLEC. - KNX & WYN - MAR -JUN 12	163,574.04
EFT115865	13/07/2012	KUNUNURRA BETTA ELECTRICAL & GAS	VISUAL EQUIPMENT - WYNDHAM LIBRARY	610.00
EFT115866	13/07/2012	KUNUNURRA COURIERS	4 X 15L WATER - KUNUNURRA LANDFILL	78.00
EFT115867	13/07/2012	KUNUNURRA DIESEL SERVICE	SERVICE - P309, CTEK CHARGER - SES KNX	1,527.80
EFT115868	13/07/2012	KUNUNURRA HOME & GARDEN	SCREWDRIVERS, SPANNERS, CONTAINERS - BUILDING MAINT - KNX	161.50
EFT115869	13/07/2012	KUNUNURRA JUNIOR FOOTBALL	KIDSPORT APPLICATION - 5 X APPLICANTS - CLUB DEVELOPMENT - KNX	150.00
EFT115870	13/07/2012	KUNUNURRA MOBILE WELDING SERVICE	RECONSTRUCTION WORKS - CHEMICAL SHED - KNX LEISURE CENTRE	4,950.00
EFT115871	13/07/2012	KUNUNURRA SECURITY SERVICE	SCREENING SERVICES - 13/6/12 - 26/6/12, MONITORING ALARMS - KLC	44,822.50
EFT115872	13/07/2012	KUN/WYN HEALTH SERVICE-KUN. HOSPITAL	WORKERS COMPENSATION - WYNDHAM OPS STAFF	177.00
EFT115873	13/07/2012	L.G.R.C.E.U	PAYROLL DEDUCTIONS	17.40
EFT115874	13/07/2012	MARTELL ROAD MAINTENANCE	ROADWORKS - SEALING SECTIONS - PACKSADDLE ROAD	19,344.00
EFT115875	13/07/2012	MAXXIA	PAYROLL DEDUCTIONS	6,014.45
EFT115876	13/07/2012	ORD RIVER ELECTRICS	REPLACE, REWIRE, REPAIR LIGHTS - KLC, UNIT 1 & 3 RIVERFIG AVE - KNX	1,254.08
EFT115877	13/07/2012	ORICA AUSTRALIA PTY LTD	STORAGE - CHLORINE CYLINDERS - WYNDHAM & KUNUNURRA	489.52
EFT115878	13/07/2012	OLLIE'S IRRIGATIONS & PLUMBING SUPP.	IRRIGATION ITEMS - FITTINGS, SOLENOIDS, COUPLINGS - RETIC MAINTEN. - KNX	732.58
EFT115879	13/07/2012	QBD BOOKSHOP	NEW BOOK STOCK - KUNUNURRA LIBRARY	627.42
EFT115880	13/07/2012	RED ELEVEN - RED 11 PTY LTD	IT EQUIPMENT - NETWROK SERVER UPDATE KUNUNURRA	5,056.63
EFT115881	13/07/2012	REHBEIN AIRPORT CONSULTING	CONSULT. - DEV. OF EK REGIONAL AIRPORT MASTER PLAN & NOISE CONTOURS	16,985.96

EFT115882	13/07/2012	SEARLES MECHANICAL REPAIRS	SOCKET & SCREWDRIVER SET - KUNUNURRA AIRPORT	298.00
EFT115883	13/07/2012	SEEK LIMITED	3 JOB AD AND STANDOUT AD FEATURE PACK WITH SEEK RECRUITMENT WEBSITE	759.00
EFT115884	13/07/2012	SHELF SUPPLY	WORKWEAR, DOG PELLETS, DOG FOOD & GUN SAFE - POUND, DEPOT KNX	695.00
EFT115885	13/07/2012	SHIRE OF WYNDHAM EAST KIMBERLEY	PAYROLL DEDUCTIONS	70.00
EFT115886	13/07/2012	SUNNY SIGN COMPANY PTY LTD	SIGNAGE - HARDCOURTS, CHESTNUT DRV - KUNUNURRA	260.11
EFT115887	13/07/2012	SURVEY NORTH	SUVEY SERVICES - WEABER PLAIN RD, KYC, HARDCOURTS, KNX DEPOT VERGE	8,573.40
EFT115888	13/07/2012	THE CANVAS SHED	WINDOW BLINDS - STAFF HOUSING - HIBISCUS DRV KNX	371.58
EFT115889	13/07/2012	TNT AUSTRALIA PTY LIMITED	FREIGHT - MONTHLY POOL SAMPLES - KUNUNURRA	86.43
EFT115890	13/07/2012	TOLL EXPRESS	FREIGHT - PLAQUES BABY TREES - KUNUNURRA	104.59
EFT115891	13/07/2012	TOP END TILT TRAY HIRE	TRANSPORT - TRACTOR - WYNDHAM TO KUNUNURRA AIRPORT DEPOT	440.00
EFT115892	13/07/2012	UDLA - URBAN DESIGN & LANDSCAPE	LANDSCAPE SERVICES - MESSMATE WAY ROUNDABOUT, KUNUNURRA	3,762.00
EFT115893	13/07/2012	URBIS PTY LTD	CONSULTANT SERVICES - TOWN PLANNING SCHEME	1,547.21
EFT115894	13/07/2012	VANDERFIELD MACHINERY PTY LTD	FIT ROOF & PARTS - P333	6,955.14
EFT115895	13/07/2012	WA LOCAL GOVERNMENT ASSOCIATION	PROPERTY ADVERTISING AND ELECTED MEMBERS TRAINING	2,938.92
EFT115896	13/07/2012	WAYNE RICHARDS	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	723.50
EFT115897	13/07/2012	WESTRAC EQUIPMENT PTY LTD	SERVICE & REPAIR - P488	1,332.20
EFT115898	13/07/2012	WYNDHAM EXCAVATIONS	SUPPLY CONCRETE, MESH, LABOUR - DEPOT UPGRADE WYNDHAM	3,520.00
EFT115899	13/07/2012	WYNDHAM SUPERMARKET	CATERING - FORESHORE PUBLIC FORUM - WYNDHAM	75.41
EFT115900	20/07/2012	ABCO PRODUCTS	CLEANING PRODUCTS - WYNDHAM	2,530.99
EFT115901	20/07/2012	ASK WASTE MANAGEMENT	ENGINEERING PLANS - DRYING BEDS, WASTE STRATEGY - KUNUNURRA LANDFILL	2,598.75
EFT115902	20/07/2012	AUSFUEL AFD AUSTRALIAN FUEL DISTRI.	DIESEL - DEPOT KUNUNURRA	10,321.31
EFT115903	20/07/2012	AUST INST OF BUILDING SURVEYORS	SUBSCRIPTION - THE AUSTRALIAN BUILDING SURVEYOR MAGAZINE - 12/13	60.00
EFT115904	20/07/2012	ACE CORPORATE APPAREL	STAFF UNIFORMS - 4 X ADMINISTRATION STAFF KUNUNURRA	392.70
EFT115905	20/07/2012	ARGYLE MOTORS	SUPPLY & FIT - WINCH, SIDE RAILS, BULL BAR - P116	8,141.84
EFT115906	20/07/2012	ASPEN PARKS - KUN. COUNTRY CLUB	ACCOMMODATION - RECRUITMENT	684.00
EFT115907	20/07/2012	B VISUAL MEDIA	PHOTOGRAPHY	110.00
EFT115908	20/07/2012	BODAN CONSTRUCTIONS PTY LTD	PLANK VINYL FLOOR -WYN DEPOT, RELOCATE METER BOX, PAINT-WYN ADMIN	2,634.00
EFT115909	20/07/2012	BUDGET RENT A CAR	HIRE CAR – STAFF TRAINING	792.36
EFT115910	20/07/2012	BLACKWOODS ATKINS PTY LTD	2FT TUBES & STARTERS - KUNUNURRA ADMIN BUILDING.	58.30
EFT115911	20/07/2012	CN BUGG & RE BUGG T/A ARTOPIA GALLERY	SERVICE GIFT - AIRPORT MANAGER – IN ACCORDANCE WITH POLICY	760.00

EFT115912	20/07/2012	CABCHARGE	CABCHARGE - KLC STAFF PERTH & WYN CHILDCARE COORDINATOR DARWIN	105.59
EFT115913	20/07/2012	CAFE CORPORATE	COFFEE MACHINE CONSUMABLES - KUNUNURRA ADMIN	250.00
EFT115914	20/07/2012	DEPT OF ENVIRONMENT & CONSERVATION	REGISTRATION- HEALTH OFFICER -NOISE REFRESHER COURSE PERTH-19-20/07/12	250.00
EFT115915	20/07/2012	DAVEY TYRE & BATTERY SERVICE	12 VOLT DEEP CYCLE - KUNUNURRA LEISURE CENTRE	315.00
EFT115916	20/07/2012	DISCOVERY HOLIDAY PARKS	STAFF ACCOMMODATION	1,175.00
EFT115917	20/07/2012	DRYSDALE RIVER STATION	REMOTE ACCOMMODATION AND FUEL - KALUMBURU RD	901.87
EFT115918	20/07/2012	EAST KIMBERLEY GLASS	REPLACE BROKEN WINDOWS - AIRPORT AND STAFF HOUSING	3,239.50
EFT115919	20/07/2012	EAST KIMBERLEY HARDWARE	VARIOUS HARDWARE ITEMS - COOLIBAH DRIVE PARKS & GARDENS	153.70
EFT115920	20/07/2012	FELTON INDUSTRIES	3 TIER SEATING - OUTDOOR COURTS - WYNDHAM	4,498.45
EFT115921	20/07/2012	FARMERS FRUIT AND VEG MART	CATERING - WIP MEETINGS, DIRECTOR FAREWELL, COMMUNITY FORUM - KNX	845.00
EFT115922	20/07/2012	FIVE RIVERS MAINTENANCE	CLEANING - CHILD CARE, REC CENTRE, PETER REID HALL, OVAL - WYN	5,685.00
EFT115923	20/07/2012	FORTEC	2 X COVERT CAMERA, 2 X 8GB SD CARDS - KUNUNURRA DEPOT	828.00
EFT115924	20/07/2012	FRANMOR CONSTRUCTIONS PTY LTD	BUILDING CONTRACTION WORKS -GYM EXPANSION -KNX	125,544.10
EFT115925	20/07/2012	FRONTIER POST & NEWS	KIMBERLEY ECHO - JUNE 12	5.00
EFT115926	20/07/2012	GRACE REMOVALS	STAFF RELOCATION	4,356.00
EFT115927	20/07/2012	GREENFIELD TECHNICAL SERVICES	ENGINEERING CONSULTANT - SEAL DESIGNS	704.00
EFT115928	20/07/2012	IT VISION ITV	SYNERGY TRAINING - IT UPGRADE	440.00
EFT115929	20/07/2012	IMAGESOURCE	STATIONERY, DL WINDOW ENVELOPES - KUNUNURRA ADMIN	1,066.89
EFT115930	20/07/2012	IRRIBIZ	VARIOUS RETICULATION REPAIRS & ITEMS, IRRIGATION PUMP - KNX & WYN	14,575.10
EFT115931	20/07/2012	JAB INDUSTRIES	WIDEN D2 DRAIN - PANEL CONTRACT T6 11	18,975.00
EFT115932	20/07/2012	KUNUNURRA MEDICAL	MEDICAL CONSULTATION - WYNDHAM DEPOT STAFF	238.10
EFT115933	20/07/2012	KIMBERLEY COMMUNICATIONS	REPAIR AC FAULT TRANSMITTERS - TRIPLE J	154.00
EFT115934	20/07/2012	KIMBERLEY KOOL REFRIGE. & AIRCON.	HIRE CHILLER TRAILER - AUSTRALIA DAY CELEBRATIONS	198.00
EFT115935	20/07/2012	KINGMAN SIGNS & GRAPHICS PTY LTD	WAYFINDING SIGNAGE AND FREIGHT - KUNUNURRA AIRPORT TERMINAL	1,723.51
EFT115936	20/07/2012	KUN. CHAMBER OF COMMERCE & INDUS.	MEMBERSHIP - 12/13	330.00
EFT115937	20/07/2012	KUNUNURRA SECURITY SERVICE	SECURITY SCREENING SERVICES - 27/6/12 - 3/7/12 - KNX AIRPORT	24,640.00
EFT115938	20/07/2012	LOCAL GOVERNMENT MANAGERS AUST.	2012/13 MEMBERSHIP – VARIOUS EMPLOYEES ACCORDING TO CONTRACT	1,672.00
EFT115939	20/07/2012	MCLEAN ENTERPRISES PTY LTD	FREIGHT - DEPOT - KNX	66.00
EFT115940	20/07/2012	NATHAN MCLELLAN	REIMBURSEMENT - CLIENT MEMBERSHIP - KUNUNURRA LEISURE CENTRE	150.00
EFT115941	20/07/2012	NOEL HAMMOND	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	316.88

EFT115942	20/07/2012	NOISE & VIBRATION MEASUREMENT SYS.	FEES - NOISE COURSE - PERTH 02-03/07/12 - HEALTH OFFICER	1,245.00
EFT115943	20/07/2012	OFFICE NATIONAL KUNUNURRA	OFFICE FURNITURE - WYNDHAM CHILDCARE, STORAGE BOXES - IT KNX	2,189.10
EFT115944	20/07/2012	ORIA ORCHARDS	WEEKLY FLOWER DELIVERY - KUNUNURRA OFFICE, AIRPORT	30.00
EFT115945	20/07/2012	P & C WELDING AND CONSTRUCTIONS	DEMOLISION OF TOILETS - LOWER BASTION PARK, CROC PARK - WYNDHAM.	20,812.00
EFT115946	20/07/2012	PERTH AIRPORT PTY LTD	ASIC CARDS PRINTED FOR 2 X STAFF - AIRPORT KNX	100.00
EFT115947	20/07/2012	PETA FULLER	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	775.59
EFT115948	20/07/2012	PORTNER PRESS PTY LTD	EMPLOYMENT HANDBOOK UPDATE BOOKLET - ORGANISATION DEVELOPM.	97.00
EFT115949	20/07/2012	SHELF SUPPLY	3 X GUNSAFES - AIRPORT KNX, SAND BAGS - KALIMBURU RD	990.00
EFT115950	20/07/2012	SHADETECH	REPLACE SHADE SAIL - KUNUNURRA SWIMMING COMPLEX	49,621.00
EFT115951	20/07/2012	ST JOHN AMBULANCE	FIRST AID SUPPLIES - VARIOUS PLACES - KUNUNURRA & WYNDHAM	1,067.98
EFT115952	20/07/2012	STITCHED UP EMBROIDERY SERVICES	EMBROIDERY - KIMBERLEY WRITERS FESTIVAL SHIRTS 2012	875.00
EFT115953	20/07/2012	THORLEY'S STORE	FILL 3 X GAS BOTTLES - BASTION LOOKOUT BBQ'S WYNDHAM	135.00
EFT115954	20/07/2012	TOTAL SAFETY & FIRE SOLUTIONS	6 MONTH SERVICE, REPAIR, RENEWAL FIRE EXTINGUISHERS - KNX AIRPORT	1,377.20
EFT115955	20/07/2012	TERRITORY TROPHIES PRECISION ENGRAV.	REPLACEMENT STAINLESS STEEL PLAQUES - WYNDHAM JETTY	2,156.00
EFT115956	20/07/2012	TOWN CARAVAN PARK	ACCOMMODATION - WANDRRA – IN ACCORDANCE WITH CONTRACT	2,000.00
EFT115957	20/07/2012	TUCKERBOX / RETRAVISION KUNUNURRA	EQUIPMENT - AIRPORT KUNUNURRA	958.86
EFT115958	20/07/2012	UHY HAINES NORTON (WA) PTY LTD	PROFESSIONAL SERVICES - PREPARATION OF MTHLY FIN. REPORTS - KNX	2,145.00
EFT115959	20/07/2012	WA LOCAL GOVERNMENT SUPER.	SUPERANNUATION CONTRIBUTIONS	27,148.01
EFT115960	20/07/2012	WA LOCAL GOVERNMENT ASSOCIATION	GST OMITTED FROM ORIGINAL EFT PAYMENT 115895	118.50
EFT115961	20/07/2012	WILD MANGO	CATERING - AGENCY DELIBERATION SESSION - ICC CONFERENCE ROOM 25/06/12	319.00
EFT115962	20/07/2012	WYNDHAM SUPERMARKET	REFRESHMENTS - COUNCIL MEETINGS - WYNDHAM	127.31
EFT115963	26/07/2012	ALEX KEARNS	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	342.45
EFT115964	26/07/2012	ASPEN PARKS - KUNUNURRA COUNTRY CLUB	ACCOMM. - GUESTS/PRESENTERS - KIMBERLEY WRITERS FESTIVAL KNX	7,161.00
EFT115965	26/07/2012	AUSTRALIAN SERVICES UNION	PAYROLL DEDUCTIONS	183.20
EFT115966	26/07/2012	BADMINGTON WA	GRANT - KNX JNR SPORTS QUICK GRANT 2012/13	500.00
EFT115967	26/07/2012	BRIDGET JOHNSTONE	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	461.42
EFT115968	26/07/2012	CHEFMASTER AUSTRALIA	10 X CTNS BIN LINERS, 4 X CTNS L300X480/50 CLEAR ICE BAGS - WYN	3,243.45
EFT115969	26/07/2012	CR CISSY GORE - BIRCH GAULT	TRAVEL REIMBURSEMENT - COUNCIL MEETING - WYN	157.40
EFT115970	26/07/2012	DAVEY TYRE & BATTERY SERVICE	PUNCTURE REPAIR - P309, P362, P491, TYRE - P104, NEW TYRE - P362	465.10
EFT115971	26/07/2012	DRYSDALE RIVER STATION	DIESEL - PROJECT MANAGER - KALUMBURU ROAD	327.63

EFT115972	26/07/2012	FIVE RIVERS CAFE	CATERING - BRIEFING SESSION - 10/07/12.	324.35
EFT115973	26/07/2012	GUERINONI & SONS	M1 SYPHON EROSION CONTROL - D2 DRAIN REHABILITATION	10,346.05
EFT115974	26/07/2012	ID WAREHOUSE	100 CUSTOM LANYARDS AND FREIGHT - KNX AIRPORT	292.27
EFT115975	26/07/2012	ICE AGE REFRIGERATION & AIRCON.	REPAIR / SERVICE ICE MACHINE - AIRPORT DEPOT KUNUNURRA	495.00
EFT115976	26/07/2012	JACK CONRICK	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	481.87
EFT115977	26/07/2012	JOSIE BOYLE	PROFESSIONAL SERVICES KIMBERLEY WRITERS FESTIVAL - 2012	2,500.00
EFT115978	26/07/2012	KUNUNURRA MEDICAL	MEDICAL CONSULTATION - WYNDHAM DEPOT STAFF - 12/07/12	119.05
EFT115979	26/07/2012	KAVISHA MAZZELLA	PROFESSIONAL FEES KIMBERLEY WRITERS FESTIVAL 2012	2,640.00
EFT115980	26/07/2012	KIMBERLEY CAFE	CATERING - PRIME MINISTER VISIT - WARINGARRI ARTS KNX	3,080.00
EFT115981	26/07/2012	KUNUNURRA BETTA ELECTRICAL & GAS	MOBILE PHONES- KNX ADMIN, DISH WASHER- STAFF HOUSING- EUGENIA ST KNX	1,035.00
EFT115982	26/07/2012	KUNUNURRA MOBILE WELDING SERVICE	MAKE & WELD POLE LOCKING LUG - WHITEGUM PARK, KUNUNURRA	88.00
EFT115983	26/07/2012	KUNUNURRA SECURITY SERVICE	AIRPORT SCREENING SERVICES - 4/7/12 - 10/7/12. CALLOUT - ADMIN KNX	19,377.00
EFT115984	26/07/2012	L.G.R.C.E.U	PAYROLL DEDUCTIONS	19.40
EFT115985			CANCELLED PAYMENT	
EFT115986	26/07/2012	MM KINNY	WINDOW TINT - P116, P388, P387	1,260.00
EFT115987	26/07/2012	MAXXIA	PAYROLL DEDUCTIONS	5,807.43
EFT115988	26/07/2012	MEAGAN LE RICHE	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	606.00
EFT115989	26/07/2012	MERCURE HOTEL PERTH	ACCOMM. - STATE COUNCIL MEETING PERTH - SHIRE PRESIDENT - 03 - 5/7/12	446.00
EFT115990	26/07/2012	MOUSES ENGRAVINGS & MORE	PLAQUES - PRIME MINISTERS VISIT	190.00
EFT115991	26/07/2012	NEXUS ARTS	BOOKING FEE & PROF. SERVICES - STORY TELLERS - KIMBERLEY WRITERS FESTIVAL	2,893.00
EFT115992	26/07/2012	NYREE GRAPHIC DESIGN	DESIGN/PRINT - POSTER AND PROGRAM - KIMBERLEY WRITERS FESTIVAL 2012	1,326.54
EFT115993	26/07/2012	ORIA ORCHARDS	2 X WEEKLY FLOWER DELIVERY - KUNUNURRA OFFICE, AIRPORT	60.00
EFT115994	26/07/2012	PIERCE POOL SUPPLIES	TODDLER SWIM ISLAND, VARIOUS CHILDREN SWIMMING EQUIPMENT - KCL	1,754.39
EFT115995	26/07/2012	PETA FULLER	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	109.50
EFT115996			CANCELLED PAYMENT	
EFT115997	26/07/2012	RECREATION SAFETY AUSTRALIA	PLAYGROUND SAFETY AUDITS & TRAINING - 4 X SWEK STAFF - WYN & KNX	3,135.00
EFT115998	26/07/2012	SHAWMAC PTY LTD	ROAD SAFETY AUDIT REPORT - KUNUNURRA HIGH SCHOOL	6,600.00
EFT115999	26/07/2012	TNT AUSTRALIA PTY LIMITED	FREIGHT - WATER SAMPLES - HEALTH DEPARTMENT	29.52
EFT116000	26/07/2012	TOURISM HOLDINGS AUSTRALIA PTY LTD	HIRE VEHICLE - PROJECT MANAGER WANDRRA - 17 - 19/08/12	457.20
EFT116001	26/07/2012	TYREPLUS KUNUNURRA	TYRE REPAIR - P314	35.00

EFT116002	26/07/2012	UHY HAINES NORTON (WA) PTY LTD	AUDIT SERVICES VARIOUS PROGRAM & ACQUITTALS	4,565.00
EFT116003	26/07/2012	WA LOCAL GOVERNMENT SUPER.	SUPERANNUATION CONTRIBUTIONS	31,696.55
EFT116004	26/07/2012	WAYNE RICHARDS	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	89.45
EFT116005	26/07/2012	WILD MANGO	CATERING - PRIME MINISTER VISIT, MUMS & BUBS BREAKFAST - KUNUNURRA	1,049.40
EFT116006	27/07/2012	PINDAN CONTRACTING PTY LTD	KNX AIRPORT EXPANSION PROJECT - PROGRESS CLAIM NO. 17	100,075.24
			TOTAL MUNI EFT PAYMENTS	<u>1,310,569.07</u>

CHQ	DATE	NAME	DESCRIPTION	AMOUNT
41385	05/07/2012	ALLTOOLS (DARWIN)	MAKITA 18 VOLT 10 TOOL COMBO KIT - KNX ADMIN	1,899.01
41386	05/07/2012	BT FINANCIAL GROUP	SUPERANNUATION CONTRIBUTIONS	193.41
41387	05/07/2012	CITY OF MELVILLE	INVOICE FOR LOST BOOKS	14.30
41388	05/07/2012	DEPARTMENT FOR PLANNING & INFRAS.	LICENCE FEE	506.40
41389	05/07/2012	HORIZON POWER	VARIOUS ELECTRICITY - KNX & WYN - APR, MAY 12	7,077.91
41390	05/07/2012	HOSTPLUS SUPERANNUATION FUND	SUPERANNUATION CONTRIBUTIONS	651.51
41391	05/07/2012	REST SUPER	SUPERANNUATION CONTRIBUTIONS	1,003.20
41392	05/07/2012	SUNSUPER	SUPERANNUATION CONTRIBUTIONS	293.37
41393	13/07/2012	AMP LIFE LTD	SUPERANNUATION CONTRIBUTIONS	178.14
41394	13/07/2012	AUST ETHICAL INVESTMENT & SUPER	SUPERANNUATION CONTRIBUTIONS	136.69
41395	13/07/2012	AUSTRALIAN SUPERANNUATION	SUPERANNUATION CONTRIBUTIONS	955.43
41396	13/07/2012	BT FINANCIAL GROUP	SUPERANNUATION CONTRIBUTIONS	204.31
41397	13/07/2012	BT LIFETIME PERSONAL SUPER	SUPERANNUATION CONTRIBUTIONS	215.75
41398	13/07/2012	CBUS	SUPERANNUATION CONTRIBUTIONS	117.81
41399	13/07/2012	FIRST CHOICE PERSONAL SUPER	SUPERANNUATION CONTRIBUTIONS	178.14
41400	13/07/2012	HORIZON POWER	VARIOUS ELECTRICITY - KNX & WYN - MAY, JUN 12	51,120.73
41401	13/07/2012	HOSTPLUS SUPERANNUATION FUND	SUPERANNUATION CONTRIBUTIONS	547.82
41402	13/07/2012	JOHN CURTIN COLLEGE OF THE ARTS	TEACHER RELIEF - 28/5/12 - 31/5/12 - KNX YOUTH PROGRAM	893.00
41403	13/07/2012	LANDGATE	ANNUAL ACCESS LICENCE - MAY 12 - APRIL 13	3,503.58
41404	13/07/2012	LOCAL GOVERNMENT SUPER	SUPERANNUATION CONTRIBUTIONS	664.76
41405	13/07/2012	MLC NOMINEES PTY LTD	SUPERANNUATION CONTRIBUTIONS	219.39

41406	13/07/2012	NKANDU BELTZ	KUNUNURRA YOUTH DEVELOPMENT PROGRAM - JUNE 12	6,826.00
41407	13/07/2012	REST SUPER	SUPERANNUATION CONTRIBUTIONS	862.58
41408	13/07/2012	SEAFARERS RETIREMENT FUND	SUPERANNUATION CONTRIBUTIONS	198.68
41409	13/07/2012	ST JOSEPHS SCHOOL	SUPPLIES FOR BANNER IN THE TERRACES COMPETITION	273.70
41410	13/07/2012	STATEWIDE SUPERANNUATION TRUST	SUPERANNUATION CONTRIBUTIONS	644.65
41411	13/07/2012	SUNSUPER	SUPERANNUATION CONTRIBUTIONS	222.44
41412	13/07/2012	TASPLAN	SUPERANNUATION CONTRIBUTIONS	223.34
41413	13/07/2012	THE TRUSTEE FOR HEADING SUPER FUND	SUPERANNUATION CONTRIBUTIONS	400.74
41414	13/07/2012	TOWER MASTER FUND	SUPERANNUATION CONTRIBUTIONS	202.05
41415	13/07/2012	UNISUPER	SUPERANNUATION CONTRIBUTIONS	165.38
41416	13/07/2012	VICSUPER	SUPERANNUATION CONTRIBUTIONS	187.90
41417	13/07/2012	VISION SUPER	SUPERANNUATION CONTRIBUTIONS	6,356.58
41418	13/07/2012	WALKABOUT SOUVENIRS	PACKS OF DRINKS COASTERS FOR COUNCIL CHAMBERS.	54.00
41419			CANCELLED PAYMENT	
41420	20/07/2012	HORIZON POWER	ELEC - STREET LIGHTING, KNX - 1/6/12 - 30/6/12	17,079.65
41421	20/07/2012	MERLEE STOLDT	REIMBURSEMENT IN ACCORDANCE WITH EMPLOYMENT CONTRACT	84.50
41422	20/07/2012	TELSTRA	LANDLINE COSTS - MAY 2012	7,927.40
41423	20/07/2012	WATER CORPORATION	ANNUAL SERVICE CHARGE - VARIOUS PLACE KNX, WYN -12/13	31,900.55
41424	26/07/2012	AUST ETHICAL INVESTMENT & SUPER	SUPERANNUATION CONTRIBUTIONS	174.61
41425	26/07/2012	AUSTRALIAN SUPERANNUATION	SUPERANNUATION CONTRIBUTIONS	881.41
41426	26/07/2012	BT FINANCIAL GROUP	SUPERANNUATION CONTRIBUTIONS	203.81
41427	26/07/2012	BT LIFETIME PERSONAL SUPER	SUPERANNUATION CONTRIBUTIONS	215.75
41428	26/07/2012	CR RALPH ADDIS	REIMBURSEMENT TRAVEL - 24/7/12	299.06
41429	26/07/2012	FIRST CHOICE PERSONAL SUPER	SUPERANNUATION CONTRIBUTIONS	71.26
41430	26/07/2012	HORIZON POWER	ELECTRICITY - WHITEGUMPARK KNX -24/5/12 - 25/6/12	103.16
41431	26/07/2012	HOSTPLUS SUPERANNUATION FUND	SUPERANNUATION CONTRIBUTIONS	600.16
41432	26/07/2012	LANA REED	PRIME MINISTERS GIFT	261.85
41433	26/07/2012	LOCAL GOVERNMENT SUPER	SUPERANNUATION CONTRIBUTIONS	454.99
41434	26/07/2012	MLC NOMINEES PTY LTD	SUPERANNUATION CONTRIBUTIONS	319.02
41435	26/07/2012	REST SUPER	SUPERANNUATION CONTRIBUTIONS	1,015.63

41436	26/07/2012	SEAFARERS RETIREMENT FUND	SUPERANNUATION CONTRIBUTIONS	197.98
41437	26/07/2012	SHIRE OF WYNDHAM EAST KIMBERLEY	PAYROLL DEDUCTIONS	70.00
41438	26/07/2012	STATEWIDE SUPERANNUATION TRUST	SUPERANNUATION CONTRIBUTIONS	644.64
41439	26/07/2012	SUNSUPER	SUPERANNUATION CONTRIBUTIONS	439.05
41440	26/07/2012	TASPLAN	SUPERANNUATION CONTRIBUTIONS	223.34
41441	26/07/2012	THE TRUSTEE FOR HEADING SUPER FUND	SUPERANNUATION CONTRIBUTIONS	400.74
41442	26/07/2012	TOWER MASTER FUND	SUPERANNUATION CONTRIBUTIONS	202.05
41443	26/07/2012	UNISUPER	SUPERANNUATION CONTRIBUTIONS	177.90
41444	26/07/2012	VICSUPER	SUPERANNUATION CONTRIBUTIONS	187.90
41445	26/07/2012	VISION SUPER	SUPERANNUATION CONTRIBUTIONS	3,259.80
41446	02/08/2012	AMP LIFE LTD	SUPERANNUATION CONTRIBUTIONS	94.86
41447	02/08/2012	AVIATION ID AUSTRALIA PTY LTD	ASIC APPLICATION	216.00
41448	02/08/2012	AXA AUSTRALIA	SUPERANNUATION CONTRIBUTIONS	77.18
41449	02/08/2012	HORIZON POWER	ELECTRICITY - IVANHOE PUMP, KNX 23/05/12 - 17/07/12	2,103.52
41450	02/08/2012	IAN MCLEAN	CONVEX MIRRORS & LONG HANDLES - KNX AIRPORT	421.09
41451	02/08/2012	STRATCO KUNUNURRA	MATERIALS FOR FOOTBALL GOALPOST - KNX OVAL	379.10
			TOTAL MUNI CHEQUE PAYMENTS	<u>157,880.66</u>

CHQ	DATE	NAME	DESCRIPTION	AMOUNT
385	05/07/2012	SHIRE OF WYNDHAM EAST KIMBERLEY	BOND CLAIM - RECOVER DAMAGES TO COSTER BUS	500.00
386	05/07/2012	TINY TOTS IMAGES	BOND REFUND - HIRE WHITEGUM PARK	1,000.00
387	05/07/2012	BUILDERS' REGISTRATION BOARD OF W.A.	BRB COLLECTION - 01/06/12 - 30/06/12	2,424.35
388	05/07/2012	CKC NOMINEES	SERVICE PROVIDED TO MG CORP - ABORIGINAL HOUSING PROJECT FUND	309,354.00
389	05/07/2012	NBC ABORIGINAL CORPORATION	SERVICE PROVIDED TO MG CORP - ABORIGINAL HOUSING PROJECT FUND	6,930.00
390	05/07/2012	SHIRE OF WYNDHAM EAST KIMBERLEY	BRB COMMISSION - 01/06/12 - 30/06/12	77.00
391	05/07/2012	M G ABORIGINAL CORPORATION	SERVICE PROVIDED TO MG CORP - ABORIGINAL HOUSING PROJECT FUND	26,892.27
392	25/07/2012	CARMEN HUNTER	BOND REFUND - PETER REID HALL	1,000.00
393	25/07/2012	CAROL BENNELL	BOND REFUND - CELEBRITY TREE PARK	1,000.00
394	25/07/2012	DEPARTMENT OF HEALTH	APPLICATION FEE	70.00

395	25/07/2012	RT & RL PASFIELD	BOND REFUND - BUS HIRE	500.00
396	25/07/2012	RYAN CAMPBELL	BOND REFUND - BUS HIRE	500.00
397	25/07/2012	M G ABORIGINAL CORPORATION	BOND REFUND - BUS HIRE	500.00
398	25/07/2012	BUILDING & CONS. IND. TRAINING FUND	BCITF COLLECTION - 01/06/12 - 30/06/12	5,336.19
399	25/07/2012	SHIRE OF WYNDHAM EAST KIMBERLEY	COMMISSION ON BCITF COLLECTION - 1/6/12 - 30/6/12	59.40
400	06/08/2012	ADAM RAY	BOND REFUND - ASIC CARD	50.00
401	06/08/2012	COLIN WILKINSON DEVELOPMENTS	BOND REFUND - FOOTPATH	500.00
402	06/08/2012	DARREN BRUCE FULCHER	BOND REFUND - FOOTPATH	400.00
403	06/08/2012	KUNUNURRA SECURITY SERVICE	BOND REFUND - ASIC CARDS	50.00
404	06/08/2012	WES JILLEY	BOND REFUND - FOOTPATH	600.00
405	06/08/2012	CKC NOMINEES	SERVICE PROVIDED TO MG CORP - ABORIGINAL HOUSING PROJECT FUND	171,039.00
			TOTAL TRUST CHEQUE PAYMENTS	<u>528,782.21</u>

CHQ/EFT	DATE	NAME	DESCRIPTION	AMOUNT
500321	02/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING -02/07/12	1,027.50
500322	04/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 04/07/12	501.05
500323	05/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 05/07/12	1,545.05
500324	06/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 06/07/12	1,179.05
500325	09/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 09/07/12	1,609.60
500326	10/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 10/07/12	1,150.20
500327	12/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 12/07/12	1,117.40
500328	13/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 13/07/12	1,675.90
500329	16/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 16/07/12	315.35
500330	17/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 17/07/12	1,128.75
500331	18/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 18/07/12	21.00
500332	19/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 19/07/12	324.30
500333	20/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 20/07/12	466.75
500334	23/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 23/07/12	2,174.70
500335	24/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 24/07/12	2,097.90

500336	25/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 25/07/12	753.80
500337	26/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 26/07/12	1,860.65
500338	27/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 27/07/12	409.10
500339	30/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 30/07/12	2,132.50
500340	31/07/2012	TRUST DPI CLEARING	TRANSPORT CLEARING - 31/07/12	743.60
			TOTAL TRUST EFT PAYMENTS	<u>22,234.15</u>

DATE	NAME	DESCRIPTION	AMOUNT
03/07/2012	PAYROLL	PAYROLL	11,871.81
09/07/2012	PAYROLL	PAYROLL	4,667.06
11/07/2012	PAYROLL	PAYROLL	193,887.46
12/07/2012	PAYROLL	PAYROLL	23,903.46
13/07/2012	PAYROLL	PAYROLL	20,780.10
19/07/2012	PAYROLL	PAYROLL	3,535.67
25/07/2012	PAYROLL	PAYROLL	203,056.27
		TOTAL PAYROLL PAYMENTS	<u>461,701.83</u>

DATE	NAME	DESCRIPTION	AMOUNT
2/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	1,381.20
2/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	1,191.67
2/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	2,258.13
2/07/2012	DIRECT DEBIT	VISACARD	4,626.88
2/07/2012	DIRECT DEBIT	FEE - BPAY	96.92
2/07/2012	DIRECT DEBIT	MASTERCARD PAYMENT	14,406.08
10/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	1,380.50
10/07/2012	DIRECT DEBIT	MESSAGES ON HOLD	507.00
16/07/2012	DIRECT DEBIT	VEHICLE LEASE - SG FLEET AUSTRAL	1,370.95

23/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	2,383.33
24/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	1,380.50
25/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	2,166.66
27/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	2,166.67
30/07/2012	DIRECT DEBIT	LEASE COSTS - STAFF HOUSING	2,258.13
JUL 2012	DIRECT DEBIT	BANK FEE	1,260.79
		TOTAL DIRECT DEBIT PAYMENTS	<u>38,835.41</u>

### 12.2.3 Write off Rates Balances of \$20 or less and Interest less than \$5

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Megan Callus, Finance Officer - Rates
<b>REPORTING OFFICER:</b>	Jo-Anne Ellis, Director Corporate Services
<b>FILE NO:</b>	FM.11.1

#### **PURPOSE**

For Council to write off to the value of \$577.18 in outstanding rates and penalties as required by Policy F12 – Rate Collection Policy

#### **BACKGROUND**

On 22 January 2008 Council reviewed and adopted the current F12 Rate Collection Policy.

The objective of this policy is “to establish a uniform approach to the collection of rates and to minimise Council’s outstanding rate debtors”.

This Policy states “That properties with outstanding rates of \$20 or less be written off and this write off reported to Council”.

#### **STATUTORY IMPLICATIONS**

##### **Local Government Act 1995**

6.12 Power to defer, grant discounts, waive or write off debts

- (1) Subject to subsection (2) and any other written law, a local government may –
  - (a) when adopting the annual budget, grant\* a discount or other incentive for the early payment of any amount of money;
  - (b) waive or grant concessions in relation to any amount of money; or
  - (c) write off any amount of money.

which is owed to the local government.

\*absolute majority required

- (2) Subsection (1)(a) and (b) do not apply to an amount of money owing in respect of rates and service charges.

#### **POLICY IMPLICATIONS**

On 22 January 2008 Council reviewed and adopted Rate Collection Policy – F12.

This Policy States:

- Penalty interest less than \$5.00 may be written off.

- That properties with outstanding rates of \$20.00 or less be written off and this write off reported to Council.
- The Chief Executive Officer be given delegated authority to initiate the necessary action under this policy (with the exception of the sale, or the reverting of property back to the Shire).

### **FINANCIAL IMPLICATIONS**

Council incurred an expense of \$577.18 as a write off.

### **STRATEGIC IMPLICATIONS**

There are no strategic implications associated with this report.

### **COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

### **COMMENT**

A report generated on 20 July 2012 indicated that there were a total of 406 properties with an outstanding rates balance of \$20.00 or less and / or interest of less than \$5.

The total value of \$577.18 that has been written off consists of rates of \$133.37, interest of \$424.77, emergency services levy penalties of \$19.04.

The highest value written off against any individual property was \$13.75.

### **ATTACHMENTS**

There are no attachments associated with this report.

### **VOTING REQUIREMENT**

Simple Majority

### **OFFICER'S RECOMMENDATION**

That Council note that the total value of \$577.18 has been written off in accordance with Policy F12 Rate Collection Policy.

### **COUNCIL DECISION**

**Minute No. 9859**

**Moved: Cr D Ausburn  
Seconded: Cr J McCoy**

**That Council note that the total value of \$577.18 has been written off in accordance with Policy F12 Rate Collection Policy.**

**Carried Unanimously 6/0**

## 12.2.4 Tender (DP)01-12/13 Lease of Retail Kiosk at East Kimberley Regional Airport

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Jo-Anne Ellis, Director Corporate Services
<b>REPORTING OFFICER:</b>	Jo-Anne Ellis, Director Corporate Services
<b>FILE NO:</b>	CM.16.45
<b>ASSESSMENT NO:</b>	N/A

### PURPOSE

For Council to issue Tender T(DP)01-12/13 Lease of Retail Kiosk at the East Kimberley Regional Airport.

### BACKGROUND

The East Kimberley Regional Airport Terminal has undergoing a significant upgrade. Part of this upgrade was the addition of a retail shop. Tender 14-10/11 Lease of Café and Shop at the East Kimberley Regional Airport was issued on 2 June 2011 with a closing date of 17 June 2011. Four persons requested the document and 1 tender was received for the Café only. The café has been leased to Lizard Lounge Pty Ltd

In disposing of the café to Lizard Lounge Pty Ltd Council resolved for the CEO to approach existing retail outlet(s) that cater to the tourist souvenir market to negotiate a suitable outcome for the retail shop at the East Kimberley Regional Airport and bring a recommendation back to Council.

Since this time several persons have made enquiries regarding the retail kiosk lease and the most equitable method of identifying a suitable lessee is considered to be public tender.

A lease of property is considered a “disposal” under the Local Government Act 1995 and as such subject to provisions of section 3.58 of the Act.

### STATUTORY IMPLICATIONS

#### **3.58.** Disposing of property

(1) In this section —

**dispose** includes to sell, lease, or otherwise dispose of, whether absolutely or not; **property** includes the whole or any part of the interest of a local government in property, but does not include money.

(2) Except as stated in this section, a local government can only dispose of property to —

(a) the highest bidder at public auction; or

(b) the person who at public tender called by the local government makes what is, in the opinion of the local government, the most acceptable tender, whether or not it is the highest tender.

(3) A local government can dispose of property other than under subsection (2) if, before agreeing to dispose of the property —

- (a) it gives local public notice of the proposed disposition —
  - (i) describing the property concerned; and
  - (ii) giving details of the proposed disposition; and
  - (iii) inviting submissions to be made to the local government before a date to be specified in the notice, being a date not less than 2 weeks after the notice is first given;

and

- (b) it considers any submissions made to it before the date specified in the notice and, if its decision is made by the council or a committee, the decision and the reasons for it are recorded in the minutes of the meeting at which the decision was made.
- (4) The details of a proposed disposition that are required by subsection (3)(a)(ii) include —
  - (a) the names of all other parties concerned; and
  - (b) the consideration to be received by the local government for the disposition; and
  - (c) the market value of the disposition —
    - (i) as ascertained by a valuation carried out not more than 6 months before the proposed disposition; or
    - (ii) as declared by a resolution of the local government on the basis of a valuation carried out more than 6 months before the proposed disposition that the local government believes to be a true indication of the value at the time of the proposed disposition.

### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

### **FINANCIAL IMPLICATIONS**

The revenue from the lease will go into the Airport General Reserve.

### **STRATEGIC IMPLICATIONS**

The East Kimberley Regional Airport is many times the first and last impression of visitors to our region. The retail kiosk will provide an additional service to the travelling public and the surrounding businesses located in the airport precinct.

### **COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

### **COMMENT**

The retail kiosk falls under the retail shops legislation and as such a five year lease is required.

### **ATTACHMENTS**

Nil

### **VOTING REQUIREMENT**

Simple Majority

### **OFFICER'S RECOMMENDATION**

That Council issues tender T(DP)01-12/13 Lease of Retail Kiosk at the East Kimberley Regional Airport .

**COUNCIL DECISION**

**Minute No. 9860**

**Moved: Cr J Parker**

**Seconded: Cr J McCoy**

**That Council issues tender T(DP)01-12/13 Lease of Retail Kiosk at the East Kimberley Regional Airport .**

**Carried Unanimously 6/0**

## 12.2.5 Tender T(DP)02-12/13 Lease of Car Hire Booth and Car Parking Spaces at East Kimberley Regional Airport

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Jo-Anne Ellis, Director Corporate Services
<b>REPORTING OFFICER:</b>	Jo-Anne Ellis, Director Corporate Services
<b>FILE NO:</b>	CM.16.46
<b>ASSESSMENT NO:</b>	N/A

### **PURPOSE**

For Council to issue Tender T(DP)02 12/13 Lease of Car Hire Booth and Car Parking Spaces at East Kimberley Regional Airport.

### **BACKGROUND**

In April 2009 Council resolved to offer new leases to the existing car hire franchise companies that occupied the car hire booths at the East Kimberley Regional Airport

**Minute No. 8618**

**Moved:Cr J Parker**

**Seconded:Cr P Caley**

**That Council offer new leases at the East Kimberley Regional Airport to:**

- a) **Avis Australia / W.T.H. Pty Ltd**
- b) **Thrifty / Territory Rent a Car Pty Ltd**
- c) **Budget / Busby Investments Pty Ltd**
- d) **Hertz / Tuffys Pty Ltd T/A East Kimberley Hirings**
- e) **Eurpocar**

**On the following conditions**

1. **Leases commence on 1 July 2009, or at the expiration of the current lease, for a term of five years.**
2. **The use of the premises is for Hire Car Franchise purposes.**
3. **Cubicle rent in the Airport Terminal is set at \$500 and is subject to review following the Terminal Upgrade.**
4. **Car bays are offered in rows 2 and 3 of the car park at an annual rental fee of \$2,000 per bay with a maximum of six bays to be allocated to each lessee.**
5. **Car bays are offered in rows 7 and 8 of the car park at an annual rental fee of \$1,000 per bay with a maximum of six bays to be allocated to each lessee.**
6. **The number of car park bays required by each lessee is to be confirmed in writing prior to the commencement of the lease and may only be altered once annually on the lease rent review date.**
7. **Lease fees will be subject to annual CPI increase on the lease rent review date.**

**8. An additional charge for lighting and security may be initiated by agreement between parties if the need to increase lighting and/or security at the East Kimberley Regional Airport car park arises.**

**CARRIED: (7/1)**

The leases were taken up by all but Europcar. This has resulted in an empty car hire booth.

Enquiries regarding this car hire booth have been made and the most equitable method of identifying a suitable lessee is considered to be public tender.

A lease of property is considered a “disposal” under the Local Government Act 1995 and as such subject to provisions of section 3.58 of the Act.

### **STATUTORY IMPLICATIONS**

#### **3.58. Disposing of property**

- (1) In this section —  
**dispose** includes to sell, lease, or otherwise dispose of, whether absolutely or not;  
**property** includes the whole or any part of the interest of a local government in property, but does not include money.
- (2) Except as stated in this section, a local government can only dispose of property to —
  - (a) the highest bidder at public auction; or
  - (b) the person who at public tender called by the local government makes what is, in the opinion of the local government, the most acceptable tender, whether or not it is the highest tender.
- (3) A local government can dispose of property other than under subsection (2) if, before agreeing to dispose of the property —
  - (a) it gives local public notice of the proposed disposition —
    - (i) describing the property concerned; and
    - (ii) giving details of the proposed disposition; and
    - (iii) inviting submissions to be made to the local government before a date to be specified in the notice, being a date not less than 2 weeks after the notice is first given;and
  - (b) it considers any submissions made to it before the date specified in the notice and, if its decision is made by the council or a committee, the decision and the reasons for it are recorded in the minutes of the meeting at which the decision was made.
- (4) The details of a proposed disposition that are required by subsection (3)(a)(ii) include —
  - (a) the names of all other parties concerned; and
  - (b) the consideration to be received by the local government for the disposition; and
  - (c) the market value of the disposition —
    - (i) as ascertained by a valuation carried out not more than 6 months before the proposed disposition; or
    - (ii) as declared by a resolution of the local government on the basis of a valuation carried out more than 6 months before the proposed disposition that the local government believes to be a true indication of the value at the time of the proposed disposition.

### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

## **FINANCIAL IMPLICATIONS**

The revenue from the lease will go into the Airport General Reserve.

## **STRATEGIC IMPLICATIONS**

The East Kimberley Regional Airport is many times the first and last impression of visitors to our region. An additional car hire company will provide greater choice to the travelling public and the surrounding businesses located in the airport precinct.

## **COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

## **COMMENT**

The existing four leases expire on 30 June 2014. It is recommended that the new lease offered expires at the same time so that a full review of lease fees and the location of parking can be reviewed.

The current value of the car hire bays is higher than the original value due to CPI increases (and GST)

- \$586 for the booth
- \$2,345 each bay in rows 2-3 and
- \$1,173 each bays in rows 7-8.

The valuation of the booth at the time of the original leases took into account that the booths would be unavailable for some time due to the terminal upgrade.

It is recommended that the following GST inclusive values be offered.

- car hire booth (booth 1) at an annual cost of \$2,000;
- bays in rows 2-3 at an annual cost of \$2,500 per bay;
- bays in rows 7-8 at an annual cost of \$1,200 per bay.

## **ATTACHMENTS**

Attachment 1 - East Kimberly Regional Airport floor plan identifying car hire booth 1

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council issues tender T(DP)02-12/13 Lease of Car Hire Booth and Car Parking Spaces at East Kimberley Regional Airport for lease of property being:

Lease expiring 30 June 2014 for

- Car hire booth (booth 1) at an annual cost of \$2,000;
- Up to six bays in rows 2-3 at an annual cost of \$2,500 per bay;
- Up to six bays in rows 7-8 at an annual cost of \$1,200 per bay.

## POINT OF CLARIFICATION

**Cr Di Ausburn**

Requested clarification on fees for other bays.

**Jo-Anne Ellis**

Booths 2,3,4,5 are priced at a rate of \$586 per bay and they have an additional 18 months to run.

### COUNCIL DECISION

Minute No. 9861

Moved: Cr D Ausburn

Seconded: Cr J McCoy

That Council issues tender T(DP)02-12/13 Lease of Car Hire Booth and Car Parking Spaces at East Kimberley Regional Airport for lease of property being:

Lease expiring 30 June 2014 for

- Car hire booth (booth 1) at an annual cost of \$2,000;
- Up to six bays in rows 2-3 at an annual cost of \$2,500 per bay;
- Up to six bays in rows 7-8 at an annual cost of \$1,200 per bay.

Carried 5/1

For: Cr John Moulden, Cr Di Ausburn, Cr Raymond Dessert, Cr Ralph Addis, Cr Jackie McCoy

Against: Cr Jane Parker

### MOTION

Moved: Cr J Parker

Advise all Lessees, the terms of the Tender document are for the period of lease, and from 2014 the arrangements for all leases will be reviewed.

### COUNCIL DECISION

Moved: Cr J Parker

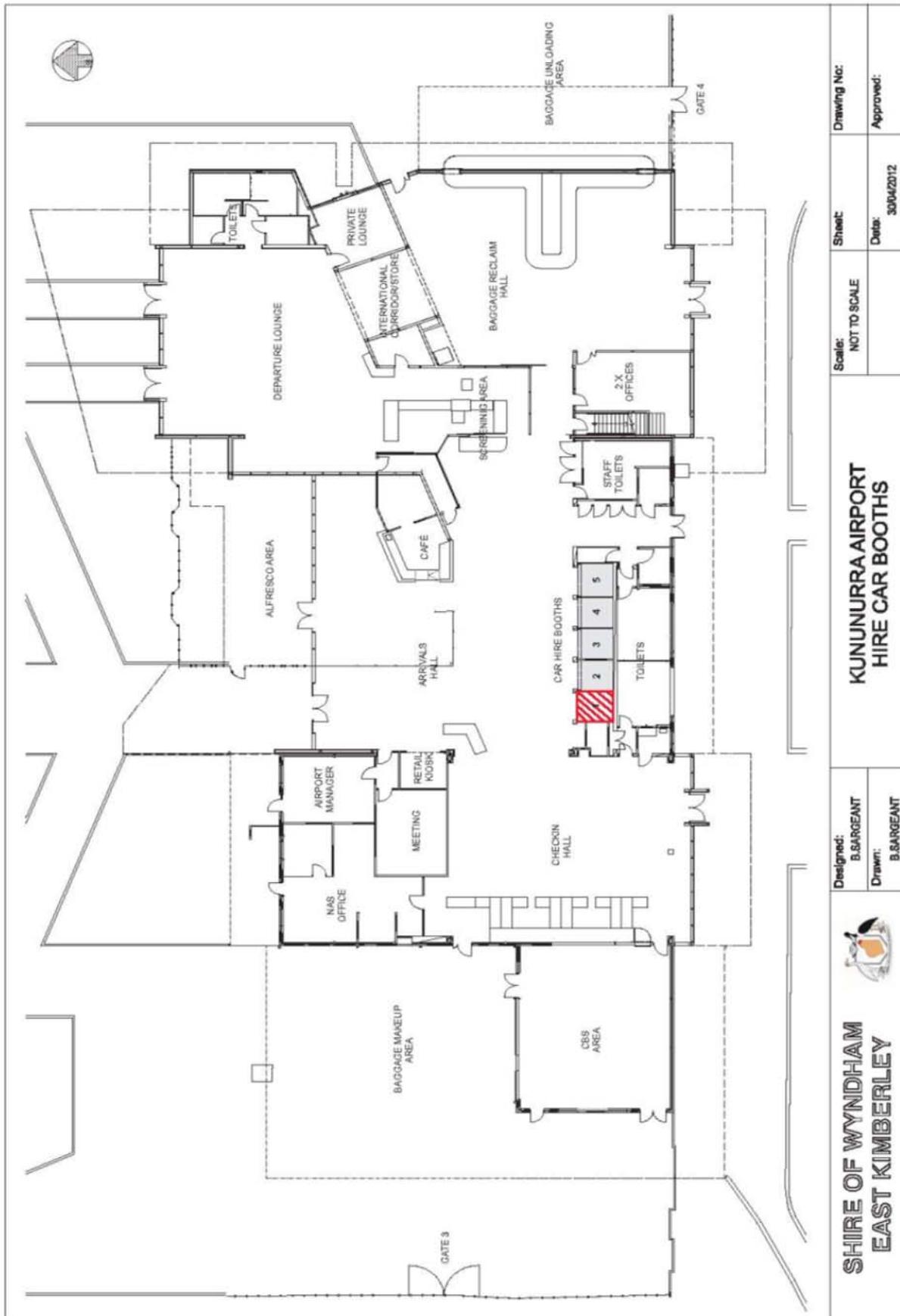
Seconded: Cr R Dessert

Advise all Lessees, the terms of the Tender document are for the period of lease, and from 2014 the arrangements for all leases will be reviewed.

Carried Unanimously 6/0



**Attachment 1 - East Kimberly Regional Airport floor plan identifying car hire booth 1**



### **12.3 INFRASTRUCTURE SERVICES**

No reports

## 12.4 COMMUNITY DEVELOPMENT

### 12.4.1 Adoption of Amendment 40 to Town Planning Scheme No. 7 – Kununurra and Environs

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	LandCorp/Urbis Consultants
<b>LOCATION:</b>	Ord East Kimberley Expansion Project, Weaber Plain, East Kimberley
<b>AUTHOR:</b>	Jennifer Ninyette, Senior Planning Officer
<b>REPORTING OFFICER:</b>	Director Community Development
<b>FILE NO:</b>	LP.04.59

#### **PURPOSE**

For Council to consider adopting Amendment 40 to the Shire of Wyndham East Kimberley Town Planning Scheme No. 7 – Kununurra and Environs, to rezone minor portions of land within the Ord East Kimberley Expansion Project, being portion of Lot 372, Lot 373 and Lot 394, to ensure that the proposed cadastral boundaries accurately correlate with the Scheme zone and reserve boundaries.

#### **BACKGROUND**

Council initiated Amendment 40 at its 20 March 2012 Ordinary Council Meeting. Specifically, at this meeting, Council resolved as follows:

**Minute No. 9716**

**Moved: Cr K Torres**

**Seconded: Cr J McCoy**

- 1) ***In pursuance of Section 75 of the Planning and Development Act (2005) (as amended), amends the Shire of Wyndham East Kimberley Town Planning Scheme No. 7 – Kununurra and Environs:***
  - a) ***to rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including, Rural Agriculture 1 Zone, No Zone, Conservation/Environmental protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.***
- 2) ***Forward Amendment No. 40 to Town Planning Scheme No. 7 – Kununurra and Environs to the Environmental Protection Authority for review, pursuant to Section 81 of the Planning and Development Act (2005) and the Western Australian Planning Commission for information, and subject to no environmental review being required by the Environmental Protection Authority and acknowledgment of the Shire's intention to advertise the Scheme Amendment being received from the Western Australian Planning Commission, the Amendment be advertised for public comment pursuant to Regulation 25(2) of the Town Planning Regulations (1967) for a period of 42 days.***

***Carried unanimously 8/0***

Amendment 40 specifically relates to the Ord East Kimberley Expansion Project, and seeks to correct minor inconsistencies of zones and reservations due to changes to the overall proposed cadastral layout that have resulted from further detailed design, environmental investigations, and the proposal to Survey Strata the development.

The Amendment therefore seeks to ensure that the Scheme zoning is aligned with the updated proposed cadastral layout. The current Scheme map, the proposed Scheme map, and map showing only the areas of proposed zoning changes are provided at Attachments 1, 2 and 3 respectively. A copy of the Amendment documentation is provided at Attachment 4.

A survey strata subdivision application which reflects the revised lot layout has been lodged with the Western Australian Planning Commission, and approved (on the 11 April 2012) subject to conditions, to run concurrently with this proposed Scheme Amendment.

This subdivision approval for the creation a total of 24 lots, consisting of 22 survey strata lots (21 are agricultural lots and 1 lot is for a conservation buffer) and 2 common property lots.

It is intended to finalise the survey strata subdivision in conjunction with this proposed Scheme Amendment to ensure that the amended zoning for the site is consistent with the final plan, reducing the risk of creating further inconsistencies and the need for additional Scheme Amendments.

#### Explanation of Proposed Changes

There are several factors which have dictated the revised shape and design of the agricultural lots subject to the Scheme Amendment, which include:

1. Drainage, Water Management and Irrigation considerations associated with the M2 Channel.
2. Environmental Constraints associated with State and Federal environmental approvals.
3. Discussions with Traditional Owners.

The current zoning of the Ord East Kimberley Expansion area is shown at Attachment 1, with the proposed zoning shown at Attachment 2.

Attachment 3 shows the extent of the proposed zoning changes, which are explained in more detail below.

#### Drainage and Irrigation Channels

The proposed shared drainage and irrigation channels required for the agricultural lots are currently designated as 'Public Purposes – Drainage' reserves under the Scheme.

However, as part of the detailed design of the development, a number of issues regarding the management of these channels have become apparent and it was determined that the most effective and efficient approach to manage the common infrastructure and environmental obligations would be via a survey strata subdivision with a Body Corporate as the management entity. Therefore, the irrigation and drainage channels are to be created as common property lots to be managed by the Body Corporate. In these circumstances it is not considered appropriate for the channels to be a Public Purpose Reserve and they are therefore proposed to be zoned 'Rural Agriculture 1' as part of this Amendment.

### Federal Environmental Conditions and Conservation/Environmental Protection Reserve

As a result of the Federal *Environment Protection and Biodiversity Protection (EPBC) Act 1999* Assessment, several conditions were placed on the approval, which has resulted in an increase to the final 'Conservation' buffer area of at least 3 hectares.

Approximately 11,500 hectares of vegetation will be set aside and managed as an environmental buffer area. Much of this area is currently a 'Conservation/Environmental Protection Reserve' and this amendment seeks to adjust the boundaries to reflect the Ord Final Agreement and environmental approvals.

As part of these environmental conditions, 4 lots are proposed to be withheld from agricultural development within the initial release of land. This includes 3 lots withheld for Deferred Clearing and 1 lot for Vegetation Management. These lots are zoned 'Rural Agriculture 1' as they have the potential to be used for agriculture, subject to environmental approvals.

Environmental conditions are included in Appendix A of the Scheme Amendment documentation at Attachment 4.

### No Zone and Waterway Reserve

The current Scheme zoning depicts areas of 'No Zone' which relate to the proposed extension to the M2 Channel and road reserves.

The Scheme Amendment proposes to retain the 'No Zone' designation for road reserves however, as the existing M1 Channel and portion of the proposed M2 Channel (outside the subject area) are shown as 'Waterway' Reserve, it is proposed to amend the area subject to the proposed M2 Channel to 'Waterway' Reserve for consistency.

## **STATUTORY IMPLICATIONS**

The Scheme Amendment intends to correct a number of minor discrepancies between the cadastral boundaries of the survey strata lots and the Shire's zoning boundaries in order to ensure that the proposed agricultural lots can be developed in accordance with the Scheme and that conservation areas are adequately reserved/protected under the Scheme.

Minor changes are also proposed to address management issues associated with common areas/infrastructure and to be consistent with zoning/reservation intents of the existing Scheme.

## **POLICY IMPLICATIONS**

There are no policy implications associated with this item.

## **FINANCIAL IMPLICATIONS**

There are no financial implications associated with this item.

## **STRATEGIC IMPLICATIONS**

The Ord East Kimberley Expansion project will release approximately 7500 hectares of irrigated agricultural land within the East Kimberley area and is considered to be a project of State and regional significance.

## **COMMUNITY CONSULTATION**

The amendment was advertised for public comment and referred to relevant government agencies for a period of 42 days with the advertising period closing on 20 July 2012. The advertising of the amendment was undertaken in the following manner:

- Relevant government agencies were advised of the scheme amendment in writing.
- Notices were placed, and documentation was available, at Council's Administration Building.
- An advertisement was placed in the Kimberley Echo Newspaper.

The Shire received two submissions of which both were from Government agencies. A schedule of the submissions received is at Attachment 5, and copies of the submissions are shown at Attachment 6.

The planning for the Ord East Kimberley Expansion has involved a number of community, local government and State Government stakeholders. This has primarily been facilitated through the establishment of the Community Reference Group (CRG). The CRG consists of a number of local farmers and business operators and is chaired by a representative of the State Government.

## **COMMENT**

The Scheme Amendment is intended to ensure that the cadastral boundaries of the survey strata lots will accurately correspond with the Shire's zoning boundaries, and as such is relatively minor in nature.

In regards to the Shire's draft Local Planning Scheme No. 8, the Amendment has been discussed between the relevant planning consultants to ensure this proposal is consistent with the objectives of the Review.

However, given the timeframe(s) still required to finalise and implement the new Scheme, it is considered appropriate to revise the Town Planning Scheme No. 7 prior to this, and as such it is intended that Council resolve to adopt the amendment.

It is noted that final adoption/approval of the Scheme Amendment by the Western Australian Planning Commission will not occur until subdivision works are complete and have been surveyed, to ensure that no further discrepancies occur.

## **ATTACHMENTS**

- Attachment 1 – Current Scheme Map
- Attachment 2 – Proposed Scheme Map
- Attachment 3 – Proposed Zoning Changes
- Attachment 4 – Amendment Documentation
- Attachment 5 – Schedule of Submissions
- Attachment 6 – Copies of Submissions

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council:

1. Notes the submissions received and the associated officer comments in respect of Amendment No. 40 to the Shire of Wyndham- East Kimberley Town Planning Scheme No.7.
2. In pursuance of Section 75 of the Planning and Development Act (2005) (as amended), amends the Shire of Wyndham East Kimberley Town Planning Scheme No. 7 – Kununurra and Environs:
  - a. to rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including, Rural Agriculture 1 Zone, No Zone, Conservation/Environmental protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.
3. Authorise the Shire President and the Chief Executive Officer to execute the relevant documentation pursuant to Regulation 22(1) of the Town Planning Regulations, 1967 (as amended).
4. Forward the Schedule of Submissions and a copy of each submission to the Western Australian Planning Commission together with three copies of the signed and sealed Amendment documents for endorsement pursuant to Regulation 22(2) of the Town Planning Regulation, 1967 (as amended).
5. Request the Honourable Minister for Planning to grant final approval to Amendment No. 40 to Town Planning Scheme No. 7 – Kununurra and Environs.
6. Inform those agencies and persons who made a submission on Amendment No. 40 to Town Planning Scheme No. 7 – Kununurra and Environs of its decision.

## **COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr R Addis**

**That Council:**

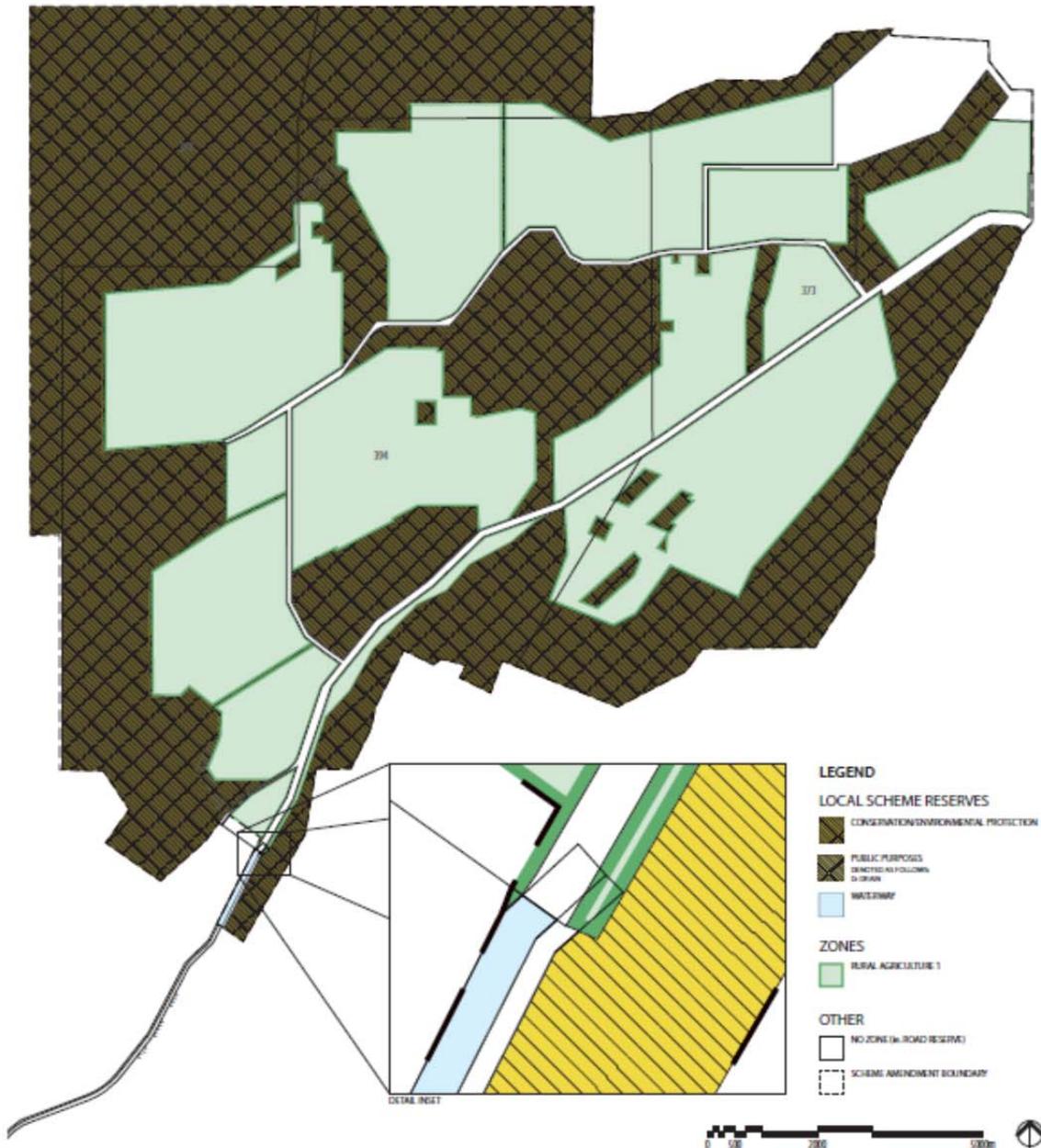
- 1. Notes the submissions received and the associated officer comments in respect of Amendment No. 40 to the Shire of Wyndham- East Kimberley Town Planning Scheme No.7.**
- 2. In pursuance of Section 75 of the Planning and Development Act (2005) (as amended), amends the Shire of Wyndham East Kimberley Town Planning Scheme No. 7 – Kununurra and Environs:**
  - a. to rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including, Rural Agriculture 1 Zone, No Zone, Conservation/Environmental protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.**
- 3. Authorise the Shire President and the Chief Executive Officer to execute the relevant documentation pursuant to Regulation 22(1) of the Town Planning Regulations, 1967 (as amended).**
- 4. Forward the Schedule of Submissions and a copy of each submission to the Western Australian Planning Commission together with three copies of the signed and sealed Amendment documents for endorsement pursuant to Regulation 22(2) of the Town Planning Regulation, 1967 (as amended).**
- 5. Request the Honourable Minister for Planning to grant final approval to Amendment No. 40 to Town Planning Scheme No. 7 – Kununurra and Environs.**
- 6. Inform those agencies and persons who made a submission on Amendment No. 40 to Town Planning Scheme No. 7 – Kununurra and Environs of its decision.**

**Carried Unanimously 6/0**

**Attachment 1 – Current Scheme Map**

**SHIRE OF WYNDHAM-EAST KIMBERLY  
TOWN PLANNING SCHEME NO. 7 (DISTRICT SCHEME)  
PROPOSED AMENDMENT NO. 40**

**PLAN 1 OF 3 CURRENT ZONING**



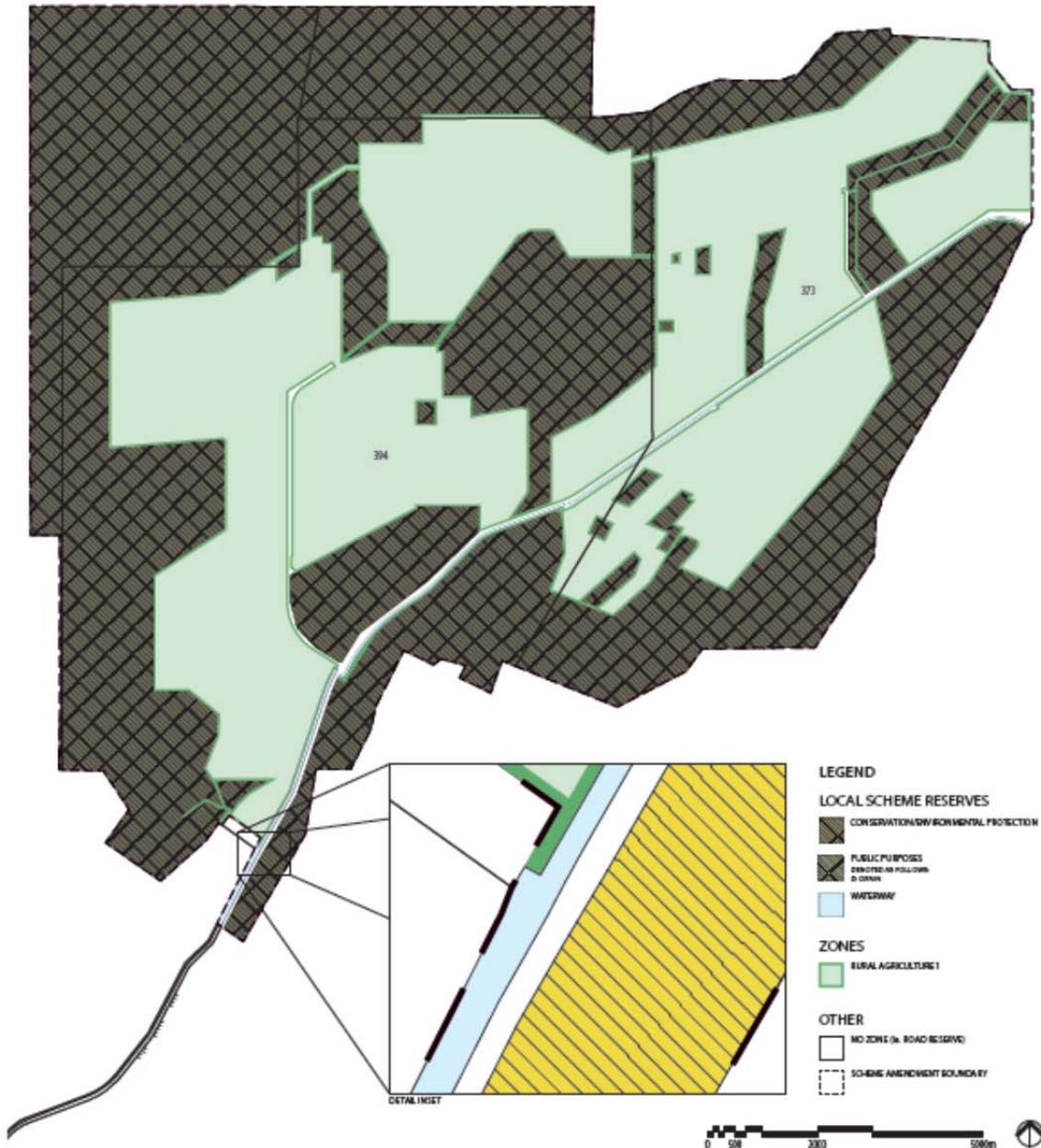
JOB NO. PA0770      DATE 06.11.2011      SCALE 1:175  
 Level 1, 55 St Georges Ter.      Tel: +61(0) 8246 0500      info@urbis.com.au  
 Perth, WA 6000 Australia      Fax: +61(0) 8221 1779      www.urbis.com.au



**Attachment 2 – Proposed Scheme Map**

**SHIRE OF WYNDHAM-EAST KIMBERLY  
TOWN PLANNING SCHEME NO. 7 (DISTRICT SCHEME)  
PROPOSED AMENDMENT NO. 40**

**PLAN 2 OF 3 PROPOSED ZONING**

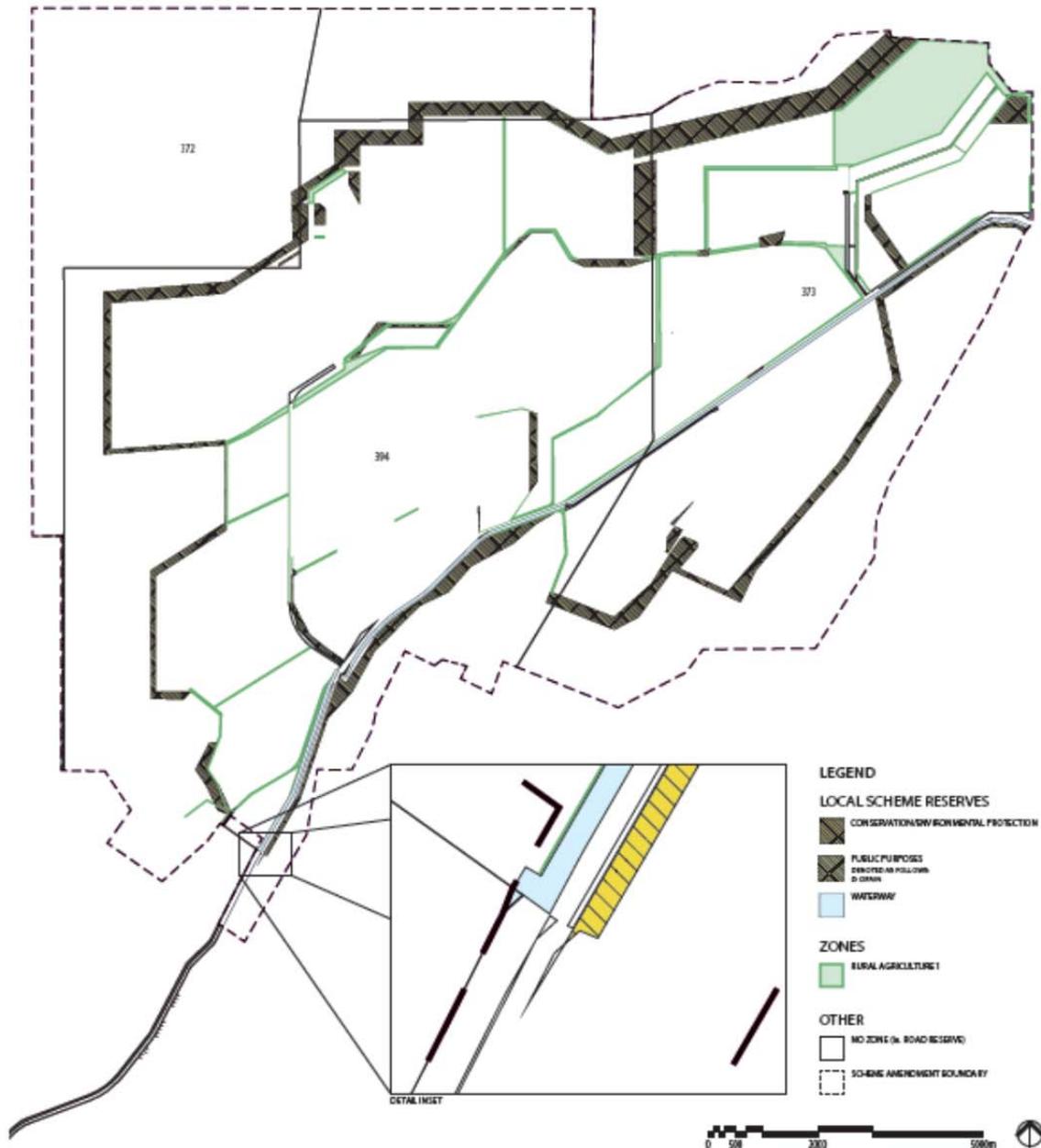


JOB NO. PA6770	DATE 08.11.2011	SCALE 1:10	
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**Attachment 3 – Proposed Zoning Changes**

**SHIRE OF WYNDHAM-EAST KIMBERLY  
TOWN PLANNING SCHEME NO. 7 (DISTRICT SCHEME)  
PROPOSED AMENDMENT NO. 40**

**PLAN 3 OF 3 PROPOSED ZONING CHANGED AREAS**



JOB NO. PA6770      DATE 08.11.2011      SCALE 1:175  
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Proposed Amendment No. \_\_\_ to  
Shire of Wyndham East  
Kimberley Town Planning  
Scheme No.7 – Kununurra and  
Environs

Ord East Kimberley Expansion Project

November 2011

**URBIS STAFF RESPONSIBLE FOR THIS REPORT WERE:**

Director	Ray Haeren
Associate Director	Maureen Hegarty
Consultant	Megan Gammon
Job Code	PA0770
Report Number	1

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*Planning and Development Act 2005 (as amended)*

**RESOLUTION DECIDING TO AMEND  
A TOWN PLANNING SCHEME**

Shire of Wyndham East Kimberley Town Planning Scheme No. 7 – Kununurra and Environs

RESOLVED that the local government, in pursuance of section 75 of the *Planning and Development Act 2005* (as amended), amend the above Town Planning Scheme:

- a) To rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including, Rural Agriculture 1 Zone, No Zone, Conservation/Environmental Protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.

Dated this \_\_\_\_\_ day of \_\_\_\_\_ 20\_\_\_\_\_.

---

Chief Executive Officer

**REPORT – AMENDMENT NO.**

FILE NO: \_\_\_\_\_

PART OF AGENDA

MINISTER FOR PLANNING

**PROPOSAL TO AMEND A TOWN PLANNING SCHEME**

1. Local Government: Shire of Wyndham East Kimberley
2. Description of Scheme: Shire of Wyndham East Kimberley Town Planning Scheme No. 7
3. Type of Scheme: Kununurra and Environs
4. Serial Number: Amendment No. \_\_
5. Proposal:
  1. To rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including the Rural Agriculture 1 Zone, No Zone, Conservation/Environmental Protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.
6. Report by: Urbis Pty Ltd on behalf of LandCorp for the Shire of Wyndham East Kimberley.

# 1 Introduction

LandCorp seeks the support of the Western Australian Planning Commission (WAPC) and the approval of the Hon. Minister for Planning to the proposed Scheme Amendment.

## 1.1 OVERVIEW OF PROPOSAL

The proposed Scheme Amendment seeks to:

1. Rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including, Rural Agriculture 1 Zone, No Zone, Conservation/Environmental Protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.

This Amendment will ensure the proposed cadastral boundaries of the survey-strata lot are aligned with the Shire's zoning boundaries. This will facilitate the development of agricultural land within the Ord East Kimberley Expansion project area.

## 1.2 CONTEXT

In 2009, the Shire of Wyndham-East Kimberley (SWEK) Town Planning Scheme No.7 (TPS7) was amended to zone/reserve the Ord East Kimberley Expansion project. This project will release approximately 7,500 hectares of irrigated agricultural land within the East Kimberley area and is considered to be a project of state and regional significance.

The zones and reservations under the previous Scheme Amendment reflected the subdivision layout proposed for the land at that point in time. A green title subdivision application was subsequently approved (dated 12 January 2010, WAPC #140788) for the creation of 22 lots and was generally consistent with the zoning of the land.

Since the Scheme Amendment and subdivision approvals were sought, further detailed design and environmental investigation has resulted in refinements and changes to the overall cadastral layout. The current zoning of the subject land, however, results in the prescriptive delineation of zones and reserves which do not accommodate the changes that typically occur during the detailed design phase.

As a result of the detailed design phase, there are now a number of minor discrepancies between the cadastral boundaries and the TPS7 zones and reserves, with the final 'Conservation' buffer area increasing by at least 3 hectares. Some of the lots created will include minor portions of land that are not zoned or are reserved under TPS7. Conversely, some of the infrastructure and 'Conservation' areas will now be located on land that is currently zoned 'Rural Agriculture 1' as opposed to being reserved for 'Conservation' or a 'No Zone'.

To correct these discrepancies, there is now a need to lodge a Scheme Amendment to ensure the TPS7 zoning is aligned with the updated survey strata cadastral layout. A subdivision application which reflects the new lot layout is being prepared concurrently with this Scheme Amendment request.

### 1.2.1 FEDERAL ENVIRONMENTAL APPROVAL

The Ord East Kimberley Expansion Project was referred to the Federal Government for environmental approval, where it was assessed against the *Environment Protection and Biodiversity Conservation (EPBC) Act 1999*. The application was approved in September 2011, subject to a range of various environmental conditions. A copy of the approval is provided at **Appendix A**.

## 1.3 PROCESS TO DATE

Discussions have been held between LandCorp, the Department of Planning and the Shire of Wyndham East Kimberley in regards to the significance of the inconsistencies and the effects of these on the end development of the lots. As a result, it has been agreed that a Town Planning Scheme Amendment will be progressed in parallel with a new survey strata subdivision application that will ensure the zoning and cadastral boundaries are aligned.

### 1.3.1 TIMEFRAMES

LandCorp envisage the completion of the servicing and infrastructure works relating to the subdivision application (currently with the Commission) will take approximately two years, at which point the survey plan shall be deposited and titles issues, provided all conditions of approval have been met.

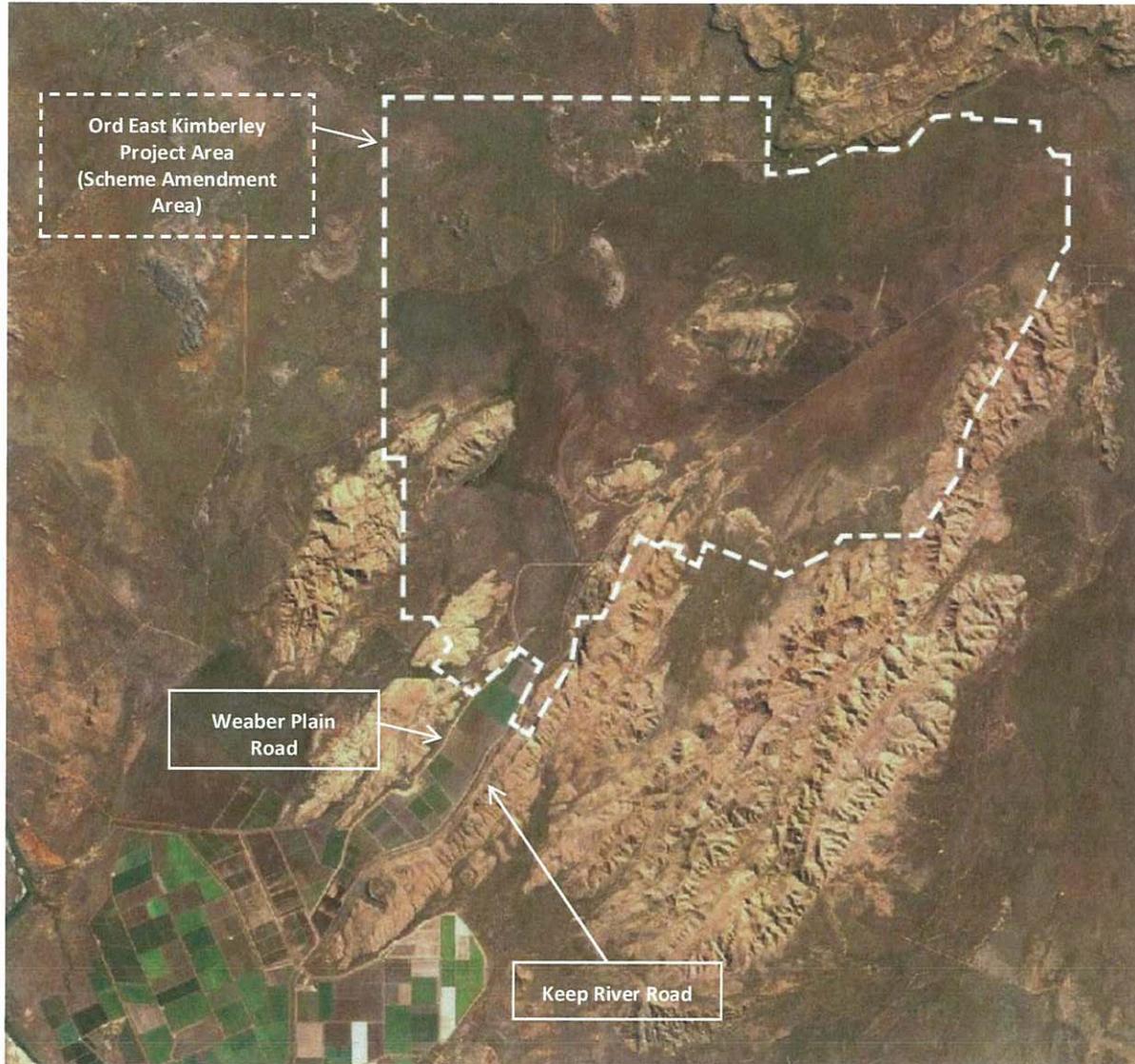
In the interim, LandCorp has offered the land for Request for Proposals on the 8 November 2011, and will enter into development leases with successful proponents. Whilst the clearing works are estimated to be completed within six to nine months, it is expected proponents will be able to commence agricultural activities on the sites once development agreements have been finalised (pending the proponents obtaining local government development approvals where necessary). Additionally, it is acknowledged that a Body Corporate will be established in order to provide for the management of the strata lots and associated common property lots. Details of the Body Corporate arrangement have not been finalised at this stage.

## 2 Site Description

### 2.1 LOCATION

The Ord East Kimberley Expansion Project, also referred to as 'Goomig Farm', is located within East Kimberley in the Shire of Wyndham East Kimberley. The site is accessed by Weaber Plain Road and Keep River Road from the south, as shown in Figure 1, below:

FIGURE 1 – LOCATION PLAN



(SOURCE: NEARMAP 2011)

## 2.2 SITE DETAILS

The site currently consists of Lot 372, Part Lot 373 and Lot 394 Weaber Plains Road, Kununurra. The total Scheme Amendment area is approximately 20,750 hectares in size. The cadastre and existing zoning of the subject area is shown in Figure 2, below:

## 2.3 LAND TENURE

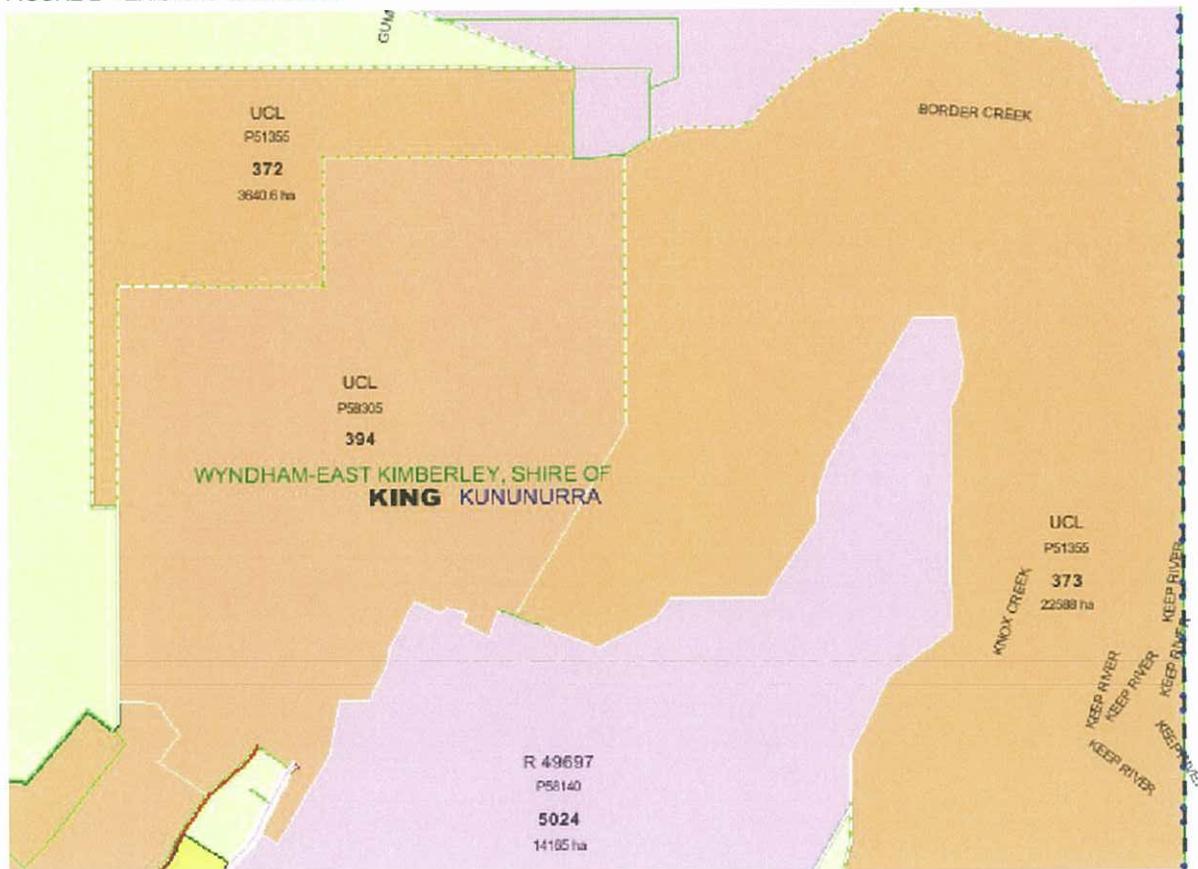
The three (3) lots subject to this Scheme Amendment request are outlined in Table 1 and Figure 3, below:

TABLE 1 – SITE PARTICULARS

LOT NO.	STREET	DIAGRAM	AREA	REGISTERED PROPIETER
Lot 372	Weaber Plain Road	51355	3,640.6 ha	Unallocated Crown Land
Part Lot 373	Weaber Plain Road	51355	22,588 ha	Unallocated Crown Land
Lot 394	Weaber Plain Road	58305	10,705 ha	Unallocated Crown Land

Relevant Certificates of Title and Diagrams are included at **Appendix B**.

FIGURE 2 – EXISTING CADASTRE



(SOURCE: LANDGATE 2011)

A Survey Strata subdivision application is being prepared concurrently with this request. The application proposes to create a total of twenty four (24) lots, consisting of twenty two (22) survey strata lots and two (2) common property lots. The proposed subdivision of the survey strata lots is generally consistent with the previous subdivision approval.

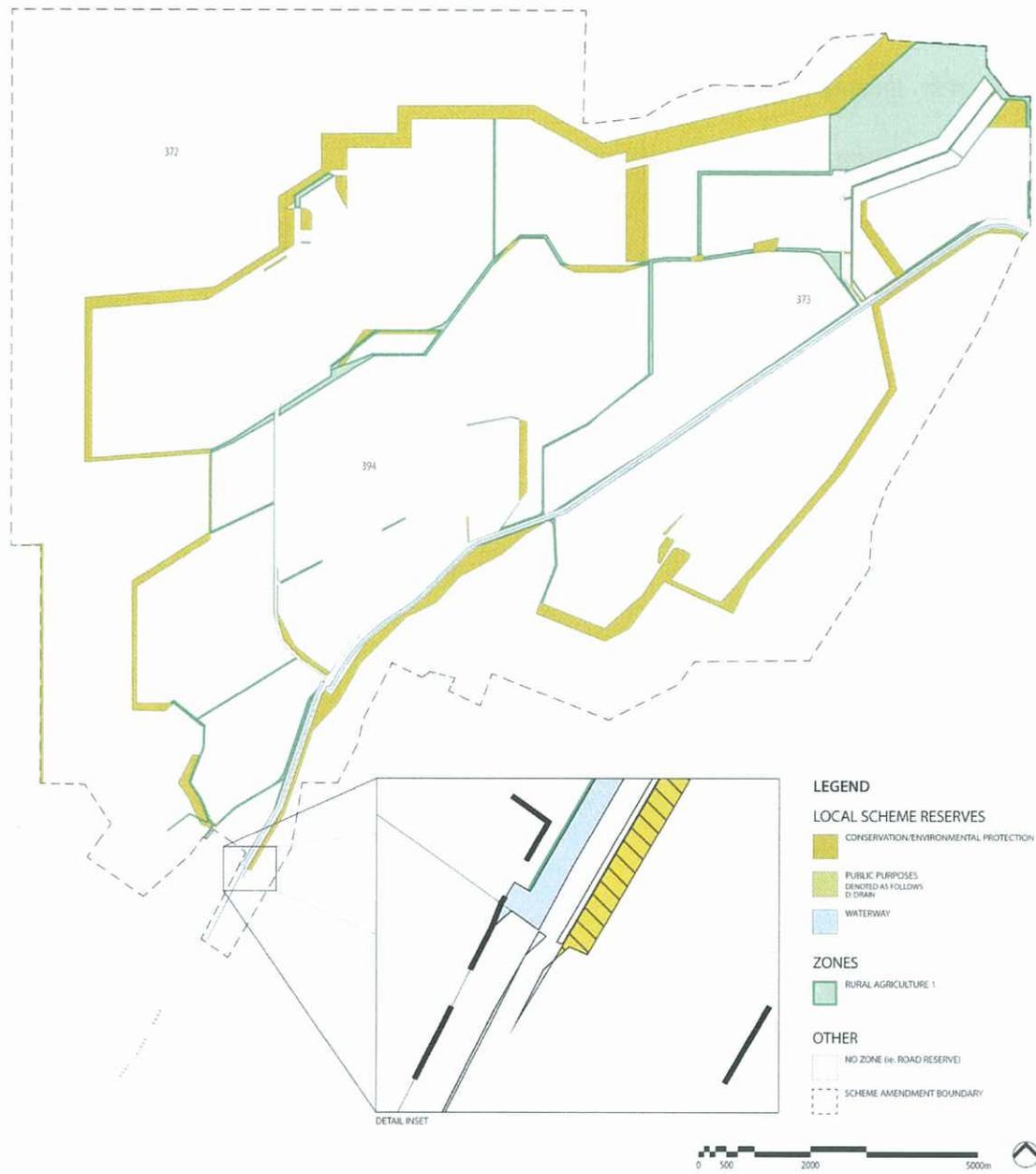
Management of common property/infrastructure and environmental obligations is a critical element of this development. The most effective approach was determined to be a Survey Strata subdivision with a Body Corporate as the management entity. A fresh subdivision application was therefore required to enable this change. The subdivision application was lodged with the Commission on the 23 November 2011.

Undertaking the subdivision in parallel with this Scheme Amendment ensures that the amended zoning for the site is consistent with the final plan, reducing the risk of creating further inconsistencies and the need for additional Scheme Amendments.



Figure 4, below, demonstrates the extent of inconsistencies between the proposed cadastral layout and the Shire's existing TPS7 zoning plan (the areas subject to this Scheme Amendment request):

FIGURE 4 – AREAS SUBJECT TO SCHEME AMENDMENT APPLICATION



JOB NO PA0770      DATE 08 11 2011      SCALE NTS  
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There are several factors which have dictated the revised shape and design of the agricultural lots subject to this Scheme Amendment. These factors include:

- Drainage, Water Management and Irrigation considerations associated with the M2 Channel.
- Environmental Constraints associated with State and Federal environmental approvals.
- Discussions with Traditional Owners.

There are four (4) zones/reserves subject to this Scheme Amendment request. Table 1 provides an outline of each zone/reserve and a summary of their intended land use:

TABLE 2 – INTENDED LAND USES

ZONE OR RESERVE	INTENDED LAND USE
Rural Agriculture 1 (zone)	Agricultural lots, Common Property lots, Deferred Clearing and Vegetation Management lots, water supply and drainage infrastructure.
No Zone (zone)	Roads
Conservation /Environmental Protection (reserve)	Buffer and conservation
Waterway (reserve)	M2 Channel

Each proposed zone/reserve is discussed briefly below, with respect to the above factors:

### 3.1 RURAL AGRICULTURE 1 ZONE

#### 3.1.1 DRAINAGE AND IRRIGATION CHANNELS

The proposed shared drainage and irrigation channels required for the agricultural lots are designated as ‘Public Purpose – Drainage’ reserves under the existing TPS7 zoning plan. Management of this infrastructure is a crucial element of the development. As part of the detailed design of the development, a number of issues regarding the management of these channels became apparent.

The most effective and efficient approach to manage common infrastructure and environmental obligations is a survey strata subdivision with a Body Corporate as the management entity. In the Survey Strata proposal the irrigation and drainage channels will be created as common property lots and the Body Corporate will manage these lots. In these circumstances it is not appropriate for the channels to be a Public Purpose Reserve and they are therefore proposed to be zoned ‘Rural Agriculture 1’ as part of this Amendment.

#### 3.1.2 LOTS SUBJECT TO FEDERAL ENVIRONMENTAL CONDITIONS

As a result of the Federal *Environment Protection and Biodiversity Protection (EPBC) Act 1999* Assessment, several conditions were placed on the approval. As part of these conditions, four (4) lots are proposed to be withheld from agricultural development within the initial release of land. This includes three (3) lots withheld for Deferred Clearing and one (1) lot for Vegetation Management. These lots are zoned ‘Rural Agriculture 1’ as they have the potential to be used for agriculture, subject to environmental approvals.

### 3.2 NO ZONE

The current Scheme zoning depicts areas of 'No Zone', which relates to the current proposed extension of the M2 Channel and road reserves. Whilst it is proposed to amend the area subject to the proposed M2 Channel to 'Waterway Reserve' (refer to Section 3.4), the Scheme Amendment proposes to retain the 'No Zone' designation for road reserves. This is demonstrated on the Scheme Amendment Map.

### 3.3 CONSERVATION/ENVIRONMENTAL PROTECTION RESERVE

As part of the Ord 2 (Stage 2) Project, approximately 11,500 hectares of vegetation will be set aside and managed as an environmental buffer area. Much of this area is currently a "Conservation/Environmental Protection Reserve" and this amendment seeks to adjust the boundaries of the area to reflect the Ord Final Agreement and environmental approvals.

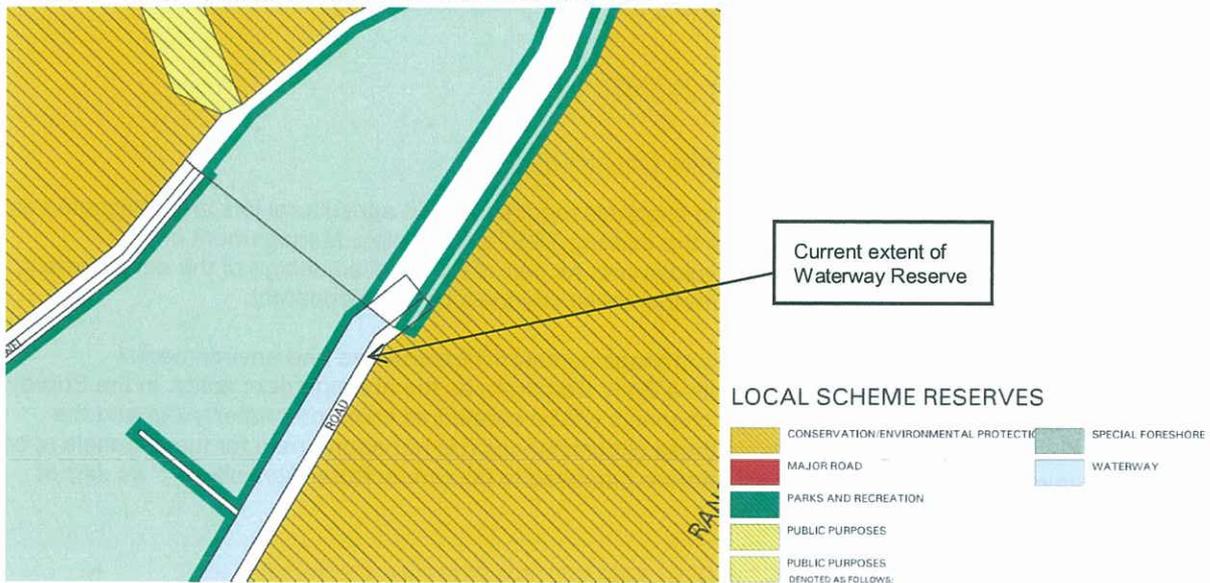
The area will be managed to retain and improve the quality of habitat suitable for threatened and migratory species. There are also corridors throughout the area which will maintain ecological linkages between different habitats in the buffer, allowing wildlife movement. Accordingly, the design and layout of the development subject to this Scheme Amendment has been determined through acknowledgement of this buffer, as well as existing topography and natural features on the subject site.

The extent of the revised 'Conservation/Environmental Protection' Reserve is shown on the Scheme Amendment Map.

### 3.4 WATERWAY RESERVE

An area currently required for the purposes of the proposed extension to the M2 Drainage Channel traverses the subject area from the south. The existing M2 Channel, which sits outside of the subject area, is currently reserved for 'Waterway', however the proposed extension of the Channel through the subject area is currently designated as a 'No Zone'. Figure 5, below, illustrates this zoning inconsistency:

FIGURE 5 – TPS7 – EXTENT OF CURRENT WATERWAY RESERVE



As part of this Scheme Amendment request, it is proposed to zone the land required for the proposed extension of the M2 channel (within the subject site) as 'Waterway' Reserve to achieve a level of consistency with the existing M2 Channel.

## 4 Planning Context and Rationale

This section provides an explanation of the strategic and statutory context for the subject site in the State and Local Government Context. Of particular relevance are the State Planning Policies 2 Environmental and Natural Resources Policy and 2.5 Agricultural and Rural Land Use Planning and the provisions of the Shire of Wyndham East Kimberley TPS 7.

### 4.1 STATE PLANNING FRAMEWORK

#### 4.1.1 STATE PLANNING POLICY NO 2 – ENVIRONMENTAL AND NATURAL RESOURCES POLICY (SPP 2)

The proposed Scheme Amendment is considered to provide an appropriate response to the environmental context of the site and surrounds. The design has taken into account the existing natural features and Ord Irrigation Scheme M2 Buffers and can demonstrate accordance with SPP 2. The proposal also received Federal Environmental approval on 14 September 2011.

#### 4.1.2 STATE PLANNING POLICY NO 2.5 – AGRICULTURAL AND RURAL LAND USE PLANNING (SPP 2.5)

The proposed Scheme Amendment provides an appropriate use and development of rural and agricultural land in accordance with SPP 2.5. The proposal provides for additional agricultural land which will provide significant benefits to the surrounding community, whilst allowing for the careful management of natural resources within the unique environment in which the site is located.

### 4.2 LOCAL PLANNING FRAMEWORK

#### 4.2.1 SHIRE OF WYNDHAM EAST KIMBERLEY TOWN PLANNING SCHEME NO.7

The subject area is located within the Shire of Wyndham East Kimberley and is subject to the provisions of Town Planning Scheme No.7 – Kununurra and Environs (TPS7). TPS7 provides the statutory basis for planning of Kununurra and its surrounds. A summary of the key objectives of each zone/reserve subject to this Scheme Amendment is provided below:

- Rural Agriculture 1 – *‘that the land be retained for extensive agriculture and/or horticulture. The land shall be retained in viable farm sized lots to prevent subdivision and subsequent loss of this limited resource’.*
- Conservation/Environmental Protection Reserve – *‘to identify and protect those areas of conservation significance particularly within existing National Parks and other conservation areas’.*
- Waterway Reserve – *‘to identify and protect significant lakes, rivers and irrigation channels and ensuring that environmental values are paramount in any consideration by the Council for applications for use of a waterway reserve’.*
- No Zone – there are no objectives for the ‘No Zone’.

The rezonings which form part of this Scheme Amendment request are consistent with the objectives outlined in TPS7, for the following reasons:

- The area of land subject to this Scheme Amendment is zoned Rural Agriculture 1, No Zone, Conservation /Environmental Protection Reserve and Waterway Reserve under the Shire’s TPS7.
- The proposed Scheme Amendment aims to rectify a number of minor discrepancies to ensure all lots can appropriately comply with the applicable objectives and land use permissibility’s set out for the relevant zones and reserves of TPS7.

## 5 Conclusion

The proposed Scheme Amendment:

- Will correct a number of minor discrepancies between the proposed cadastral boundaries of the survey strata lot and the Shire's zoning boundaries.
- Implements the principles of the Local and State Government Planning framework.
- Enables development of a significant portion of agricultural land within the East Kimberley area as part of the Ord East Kimberley Expansion project.

On this basis, it is respectfully requested that the Shire of Wyndham East Kimberley and the Western Australian Planning Commission support and Minister for Planning approve the Amendment.

**SCHEME AMENDMENT**

**PLANNING AND DEVELOPMENT ACT 2005**

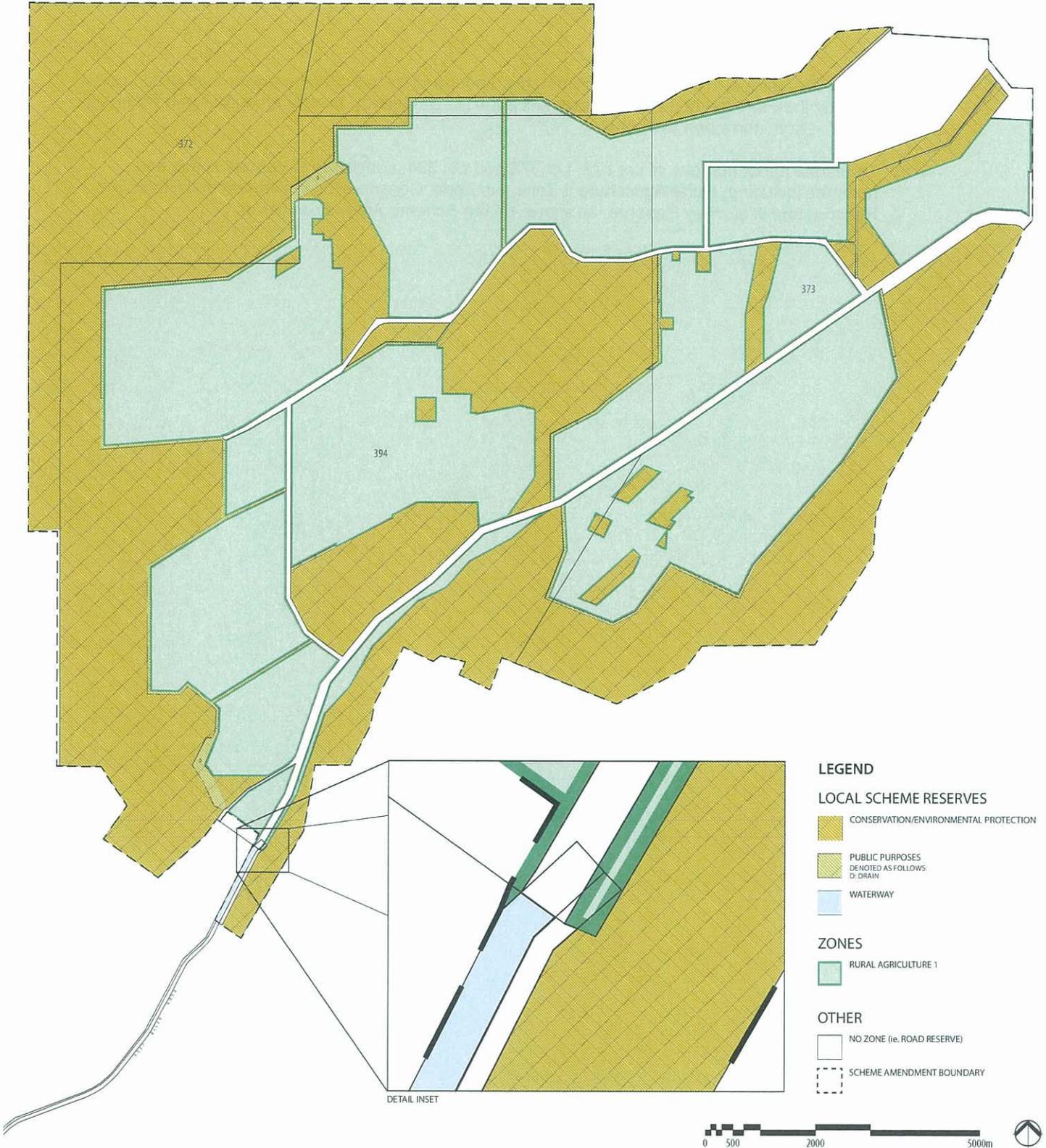
**SHIRE OF WYNDHAM EAST KIMBERLEY**

The Council of the Shire of Wyndham East Kimberley, under and by virtue of the power conferred upon it in that behalf by the Planning and Development Act 2005 (as amended), hereby amends Town Planning Scheme No. 7 – Kununurra and Environs:

- a) To rezone minor portions of Lot 372, Lot 373 and Lot 394, consisting of various zones and reserves including, Rural Agriculture 1 Zone, No Zone, Conservation/Environmental Protection Reserve and Waterway Reserve, as shown on the Scheme Amendment Map.

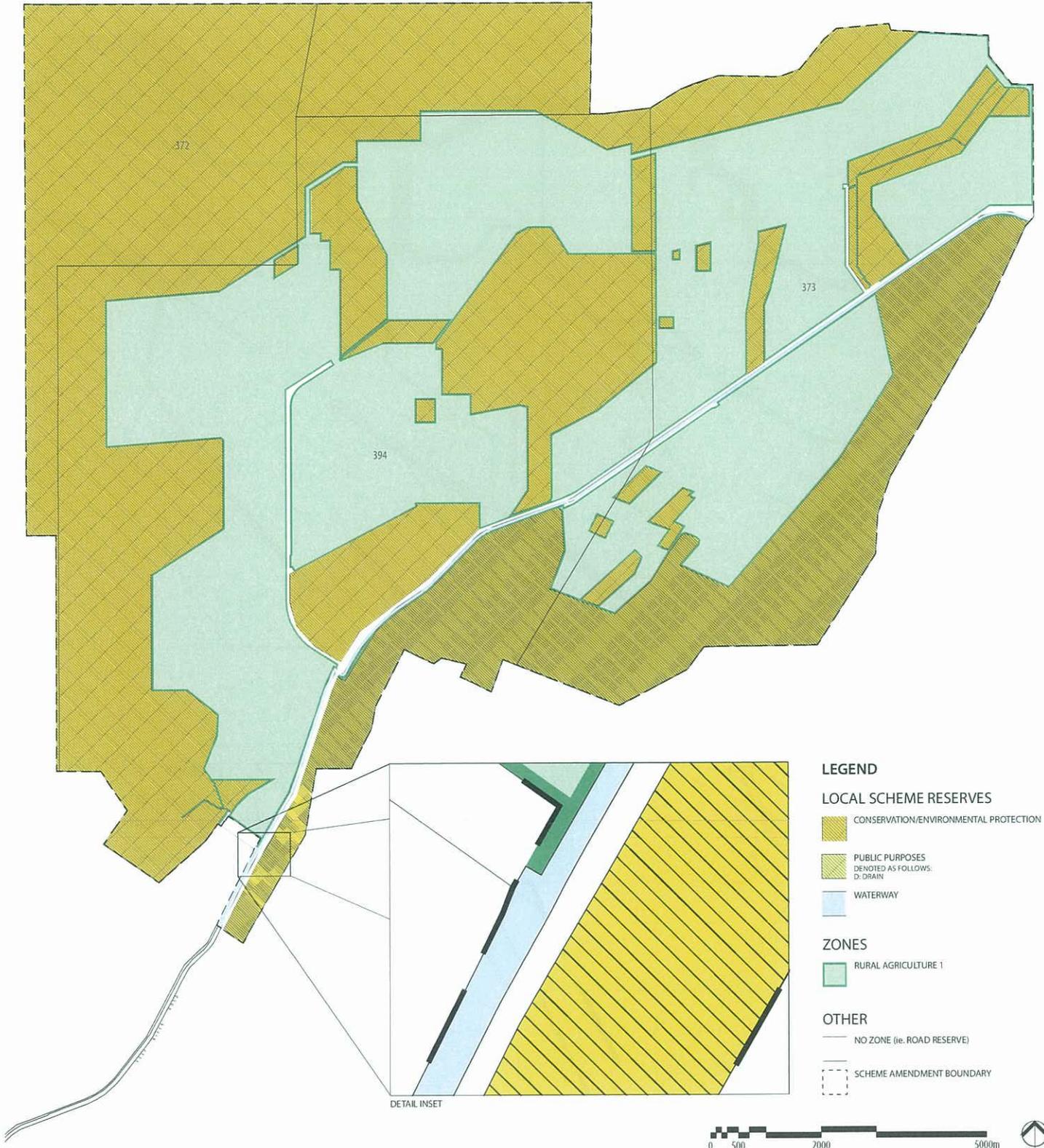
**SHIRE OF WYNDHAM-EAST KIMBERLY  
TOWN PLANNING SCHEME NO. 7 (DISTRICT SCHEME)  
PROPOSED AMENDMENT NO. XXX**

**PLAN 1 OF 3 CURRENT ZONING**



**SHIRE OF WYNDHAM-EAST KIMBERLY  
TOWN PLANNING SCHEME NO. 7 (DISTRICT SCHEME)  
PROPOSED AMENDMENT NO. XXX**

**PLAN 2 OF 3 PROPOSED ZONING**



**LEGEND**

**LOCAL SCHEME RESERVES**

- CONSERVATION/ENVIRONMENTAL PROTECTION
- PUBLIC PURPOSES DENOTED AS FOLLOWS:  
D: DRAIN
- WATERWAY

**ZONES**

- RURAL AGRICULTURE 1

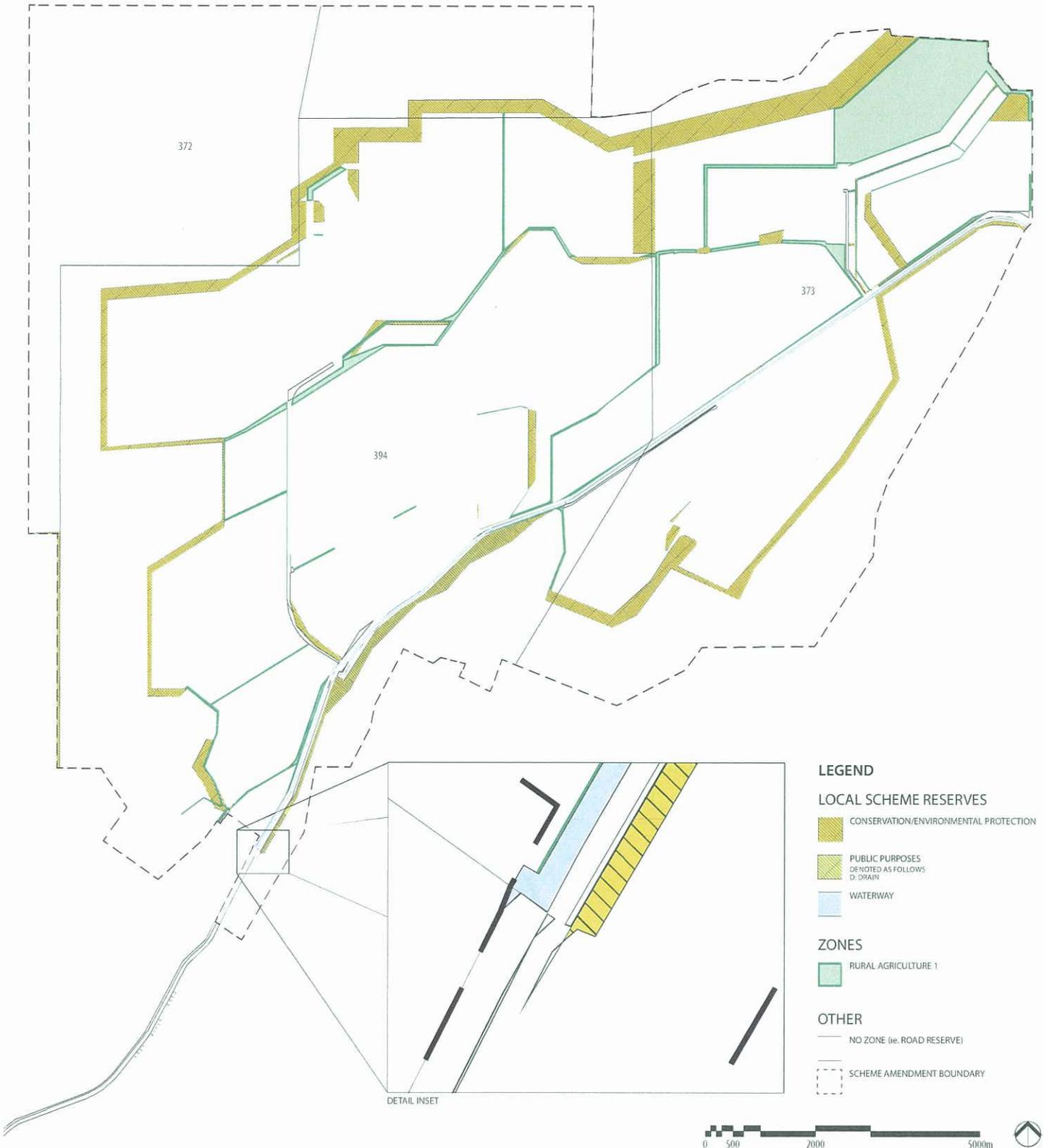
**OTHER**

- NO ZONE (i.e. ROAD RESERVE)
- SCHEME AMENDMENT BOUNDARY



**SHIRE OF WYNDHAM-EAST KIMBERLY  
TOWN PLANNING SCHEME NO. 7 (DISTRICT SCHEME)  
PROPOSED AMENDMENT NO. XXX**

**PLAN 3 OF 3 PROPOSED ZONING CHANGED AREAS**



**Initiate Scheme Amendment**

Adopted by resolution of the Council of the Shire of Wyndham East Kimberley at the Ordinary Meeting of the Council held on \_\_\_\_\_ 20\_\_\_\_\_.

\_\_\_\_\_  
PRESIDENT

\_\_\_\_\_  
CHIEF EXECUTIVE OFFICER

**Final Approval**

Adopted for final approval by resolution of the Shire of Wyndham East Kimberley at the Ordinary Meeting of the Council held on \_\_\_\_\_ 20\_\_\_\_\_.

The Common Seal of the Shire of Wyndham East Kimberley was hereunto affixed by authority of a resolution of the Council in the presence of:

\_\_\_\_\_  
PRESIDENT

\_\_\_\_\_  
CHIEF EXECUTIVE OFFICER

RECOMMENDED/SUBMITTED  
FOR FINAL APPROVAL

\_\_\_\_\_  
DELEGATED UNDER S.16 OF THE  
PLANNING AND DEVELOPMENT ACT  
2005

\_\_\_\_\_  
DATE

FINAL APPROVAL GRANTED

\_\_\_\_\_  
MINISTER FOR PLANNING

## Appendix A

## Federal Environmental Protection and Biodiversity Conservation Act (EPBC) – Approval 2011



**Approval**

**Weaber Plain Development Project (EPBC 2010/5491)**

This decision is made under sections 130(1) and 133 of the *Environment Protection and Biodiversity Conservation Act 1999*.

**Proposed action**

<b>person to whom the approval is granted</b>	Department of State Development
<b>proponent's ABN</b>	90 199 516 864
<b>proposed action</b>	The proposed action is to clear for irrigation and infrastructure an area up to 9,375ha on the Weaber Plain, 30km north-northeast of Kununurra, WA. The project infrastructure includes a main drainage channel, smaller distribution channels, stormwater and groundwater drainage, flood protection levees, access roads and power supply for each farm lot. (See EPBC Act referral 2010/5491).

**Approval decision**

Controlling Provision	Decision
Wetlands of international importance (sections 16 & 17B)	Approved
Listed threatened species and communities (sections 18 & 18A)	Approved
Listed migratory species (sections 20 & 20A)	Approved

**conditions of approval**

This approval is subject to the conditions specified below.

**expiry date of approval**

This approval has effect until 31 December 2061

**Decision-maker**

<b>name and position</b>	The Hon Tony Burke MP Minister for Sustainability, Environment, Water, Population and Communities
--------------------------	--

**signature**

**date of decision**

13. 9. 11

## **Conditions attached to the approval**

---

1. Within 30 days after the **commencement of the action**, the person taking the action must advise the **Department** in writing of the actual date of commencement.
2. The person taking the action must maintain accurate records substantiating all activities associated with or relevant to the conditions of approval, and make them available upon request to the **Department**. Such records may be subject to audit by the **Department** or an independent auditor in accordance with section 458 of the **EPBC Act**, or used to verify compliance with the conditions of approval. Summaries of audits will be posted on the **Department's** website. The results of audits may also be publicised through the general media.
3. Within three months of every 12 month anniversary of the **commencement of the action**, the person taking the action must publish a report on their website addressing compliance with each of the conditions of this approval, including implementation of any management plans as specified in the conditions. Documentary evidence providing proof of the date of publication and non-compliance with any of the conditions of this approval must be provided to the **Department** at the same time as the compliance report is published.
4. The person taking the action must provide a schedule of works to the **Department** prior to the **commencement of the action**.
5. To avoid and/or to minimise impacts on listed threatened and migratory species, the person taking the action must:
  - A. Not clear more than 9,375 hectares of vegetation (as described in the Supplementary Environmental Impact Statement);
  - B. Establish a **Buffer Area** of at least 11,470 hectares (as shown in Figure 2 of the Supplementary Environmental Impact Statement), to be managed for conservation in perpetuity;
  - C. Not clear any Gouldian Finch breeding habitat that is known to have been utilised by the Gouldian Finch;
  - D. Use no more than 120 GL of water per **water year** from the Ord River System for irrigation in the development area; and
  - E. Discharge groundwater only in the K1 pool or downstream in the Keep River estuary (as identified in Figure 5 of the Supplementary Environmental Impact Statement).

### **6. Gouldian Finch Conservation Plan**

In order to protect the endangered Gouldian Finch (*Erythrura gouldiae*), the person taking the action must prepare a Gouldian Finch Conservation Plan (GFCP), in consultation with the **WA DEC** and a **Gouldian Finch Expert**. The GFCP must be submitted for approval by the **Minister**. **Clearance of farm lots** must not be undertaken until the GFCP is approved. The GFCP must include the following:

- A. A monitoring program that includes:
  - i. Baseline surveys of the quality and distribution of Gouldian Finch feeding habitat in the **Buffer Area**;
  - ii. Annual monitoring of breeding populations, including timing and reproductive outputs (i.e. clutch size and fledging success);
  - iii. Annual wet-season monitoring of foraging activity in critical wet-season feeding areas in close proximity to breeding areas;

- iv. Mapping, and annual monitoring of the **phenology** and productivity of wet season feeding habitat, and assessment of their use by Gouldian Finches;
- B. A Fire Management Program developed and implemented to protect and enhance Gouldian Finch feeding and breeding habitat. The Fire Management Program must incorporate relevant findings from fire management projects such as, but not limited to, the Ecofire project conducted in the northern and central Kimberley (Rangelands NRM 2011, <http://www.rangelandswa.com.au/pages/150/ecofire>) and must be developed in close consultation with a **Gouldian Finch expert**;
- C. Widening of all vegetation corridors indicated in Figure 2 of the Supplementary Environmental Impact Statement (including between Lots 5 and 18 and Lots 9 and 14) to a minimum width of 400m;
- D. Avoidance of clearing any breeding habitat that has been utilised by the Gouldian Finch, as identified in Figure 1 of the Gouldian Finch Management Plan;
- E. Salvaging of breeding hollows that are cleared for relocation in the **Buffer Area** and results of their use recorded as part of the monitoring program;
- F. Performance standards in relation to the Gouldian Finch population;
- G. Adaptive management triggers should performance standards not be met and contingency measures to be implemented if this occurs;
- H. An annual audit and review of the effectiveness of management measures, operating controls and implementation of any required improvements to management conditions;
- I. Protocols and timelines for review and reporting to the **Department**.

The approved Gouldian Finch Conservation Plan must be implemented.

#### 7. **Buffer Management Plan**

In order to protect listed threatened species, the person taking the action must prepare a Buffer Management Plan (BMP) in consultation with the **WA DEC**. The BMP must be submitted for approval by the **Minister**. **Clearance of farm lots** must not be undertaken until the BMP is approved. The BMP must include:

- A. Vegetation and fauna surveys and mapping of the **Buffer Area** (shown in Figure 2 of the supplementary Environmental Impact Statement). Fauna surveys must be targeted for **EPBC Act** listed threatened species that are likely to occur in the **Buffer Area**. The program must be developed in consultation with **WA DEC**, with methodologies approved by the **Department**. The person taking the action must provide results of the survey program to the **Department**, including maps showing the location of any breeding, nesting or denning habitat identified in the **Buffer Area**. The survey program must include the endangered Northern Quoll (*Dasyurus hallucatus*), the vulnerable Red Goshawk (*Erythrorchis radiates*) and the vulnerable Northern Shrike-tit (*Falcunculus frontatus whitei*). Surveys must be completed prior to 31 December 2012.
- B. Details of tenure and management arrangements of the **Buffer Area** that provides assurance that the area will be conserved and managed in perpetuity;
- C. Ongoing management practices that will be applied to the **Buffer Area** to maximise benefits to listed threatened species;

- D. Methods to control human disturbance of the **Buffer Area**, including restriction of vehicular access;
- E. Regular and ongoing inspection of the **Buffer Area** for weeds, plant pathogens and pest animals and methods to prevent the introduction and spread and provide for quick control of weeds, plant pathogens and pest animals in the **Buffer Area**;
- F. Fire management of the **Buffer Area** to maximise benefits to listed threatened species;
- G. Methods to minimise the impacts of construction activities on the **Buffer Area**;
- H. Rehabilitation of disturbed portions of the **Buffer Area** to benefit listed threatened species;
- I. Responsibilities and provision of resources for the ongoing management of the **Buffer Area**;
- J. Protocols and timing of review and reporting to the **Department**.

The approved Buffer Management Plan must be implemented.

Note: To avoid doubt, if a condition of another approval held by the proponent requires a Buffer Management Plan, the proponent may simultaneously meet the relevant requirements of both conditions by submitting a single plan.

#### 8. **Weed, Plant pathogen and Pest Management Plan**

In order to protect listed threatened species, the person taking the action must undertake the action in accordance with the Weed, Plant pathogen and Pest Management Plan approved under the *Environmental Protection Act 1986*, and any amendments to that plan. The person taking the action must provide an annual report to the **Department** on compliance with the plan, with the first report submitted not later than 12 months after commencement of the action.

#### 9. **Independent Review Group**

The person taking the action must appoint an **Independent Review Group** to review hydrological aspects of the action and associated impacts on **EPBC Act** listed threatened species. The **Independent Review Group** must be established prior to the submission of the Aquatic Fauna Management Plan, Stormwater and Groundwater Discharge Management Plan and Groundwater Management Plan (referred to in Conditions 10, 11 and 12) to the **Minister** for approval. The **Independent Review Group** must be established according to the following requirements:

- A. The group must be funded, resourced and managed by the person taking the action;
- B. The group must consist of independent scientific and technical experts, of whom at least one must be a **Glyphis and Pristis expert** and two must be technical experts with at least 5 years experience in northern Australian surface water and groundwater hydrology. The members of the group and any subsequent changes must be approved by the **Minister**;
- C. Terms of Reference for the group must be prepared by the person taking the action and submitted for approval by the **Minister**. The Terms of Reference must include the frequency of proposed meetings and chairing and quorum arrangements. The Terms of Reference must be approved by the **Minister** in writing prior to the submission of the Aquatic Fauna Management Plan, Stormwater and Groundwater Discharge Management Plan and Groundwater Management Plan, to the **Minister** for approval;

- D. The group must provide advice on any substantive changes to, or reviews of the Aquatic Fauna Management Plan, Stormwater and Groundwater Discharge Management Plan and Groundwater Management Plan (referred to in Conditions 10, 11 and 12);
- E. The group must assess any exceedance of trigger values and advise changes as required;
- F. The **Minister** may seek advice from the review group at any time. Specific matters identified through such advice may need to be addressed in the Management Plans. Where such advice is sought the proponent would be provided with opportunity to submit information and respond to the specific matters identified, in order to ensure the Management Plans are based on the best available information;

#### 10. Aquatic Fauna Management Plan

In order to protect listed threatened species in the Keep River, the person taking the action must prepare an Aquatic Fauna Management Plan (AFMP), in consultation with the **WA DEC** and the **Independent Review Group**. The AFMP must be submitted for approval by the **Minister**. **Clearance of farm lots** must not be undertaken until the AFMP is approved. The AFMP must include:

- A. A targeted, non-lethal baseline surveying program for listed threatened species that are likely to occur in the Keep River. This must include the critically endangered Speartooth Shark (*Glyphis glyphis*), the endangered Northern River Shark (*Glyphis garricki*), the vulnerable Dwarf Sawfish (*Pristis clavata*) and the vulnerable Freshwater Sawfish (*Pristis Microdon*). The methodology of the baseline surveying program must be developed in consultation with the **Independent Review Group**. Surveys must be conducted over a period of 3 years and must be undertaken in the four Keep River pools (K1, K2, K3 and K4) and at least 3 sites in the Keep River estuary;
- B. Details of water quality and flow requirements, including relevant downstream environmental quality parameters, in accordance with **ANZECC guidelines**;
- C. A monitoring program in the Keep River pools to be undertaken to ensure water quality and flow does not exceed trigger values;
- D. Details of an outcome based risk assessment which utilises data collected during the baseline monitoring program to determine the potential for risk to listed species at an individual and local population level;
- E. Details of management objectives, management actions, performance standards and contingency measures to mitigate impacts on listed aquatic fauna species in the Keep River;
- F. Regular and ongoing inspection of the Border Creek and Keep River for weeds, plant pathogens and pest animals and methods to prevent the introduction and provide for quick control of weeds, plant pathogens and pest animals in the Border Creek and Keep River as a result of the action;
- G. A targeted aquatic fauna monitoring program to be undertaken to measure the success of management measures to inform an adaptive management approach;
- H. Protocols and timelines for review and reporting to the **Department**.

The approved Aquatic Fauna Management Plan must be implemented.

## 11. Stormwater and Groundwater Discharge Management Plan

In order to protect listed threatened species in the Keep River, the person taking the action must prepare a Stormwater and Groundwater Discharge Management Plan (SGDMP) in consultation with the **Independent Review Group**. The SGDMP must be submitted for approval by the **Minister**. **Clearance of farm lots** must not be undertaken until the SGDMP is approved. The SGDMP must include:

- A. Details of a **Tailwater Management System** to be established on each farm to manage runoff and minimise the discharge of pollutants into the Border Creek and Keep River. The **Tailwater Management System** must be actively managed to minimise the discharge of stormwater into the Border Creek and Keep River. The **Tailwater Management Systems** must be constructed and operational prior to **commencement of irrigation**;
- B. Management actions to prevent runoff transporting pollutants downstream should the agreed tailwater retention capacity be reached. This must include diversion of on-farm stormwater to irrigation channels in periods of low flow, where there is capacity, as identified by Conditions 11.G and 11.H, to ensure pollutants are not transported into the Border Creek and Keep River in low flow periods;
- C. A baseline monitoring program for water quality and hydrology in the Border Creek and Keep River. This must be completed prior to **commencement of irrigation** and prior to any release of stormwater or groundwater from farms. Sampling sites must include the Keep River estuary and the four Keep River pools (K4, K3, K2 and K1). Methodologies and sampling locations must be established in consultation with the **Independent Review Group**;
- D. Installation of water quality and flow gauging stations capable of sampling first flush discharges at: the stormwater outlet from the **Development Area**; Border Creek; and in the Keep River, in consultation with the **Independent Review Group**. Sampling must include analytes identified in Condition 11.I and must have the required accuracy to measure low flow rates. Gauging stations must be established prior to the **commencement of irrigation**. For any release of first flush water, monitoring must be conducted more than once a day and for any other stormwater flows monitoring must be conducted at least once per day. Automated sampling techniques may be utilised.
- E. **Seasonal** baseline water quality trigger values for the Keep River must be determined in accordance with **ANZECC guidelines** and agreed by the **Independent Review Group**. Until these trigger values are agreed by the **Independent Review Group**, **ANZECC guidelines** trigger values for systems with high conservation/ecological value (as defined in the **ANZECC guidelines**) must be used. Sample analytes must be agreed to by the **Independent Review Group** and in accordance with Condition 11.I;
- F. Details of **AUSRIVAS** trigger levels for aquatic macro-invertebrates. **AUSRIVAS** assessment must be undertaken in consultation with the **Independent Review Group** and prior to the **commencement of irrigation**;
- G. Updating of the discharge dilution and release timing model (based on Keep River and Border Creek flow monitoring data and water quality characteristics of stormwater from the **Development Area** and the Keep River system). This must be conducted prior to **commencement of irrigation** and annually during operation.
- H. An adaptive groundwater and stormwater discharge program to provide for adaptive management of the discharge of stormwater and surplus groundwater that includes:
  - i. discharge rules and rates and contingency actions; and

- ii. monitoring locations and requirements including infrastructure and setup;
  - iii. design and location of dewatering infrastructure;
  - iv. design and location of discharge infrastructure;
  - v. written evidence of any Northern Territory Government permits that are required for discharge of groundwater; and
  - vi. management measures that ensure discharge of water will not impact on water quality in Border Creek and Keep River, including erosion protection measures.
- I. Establishment of a list of key analytes to be sampled as part of ongoing water quality monitoring in consultation with the **Independent Review Group**. The list must be updated annually based on monitoring results.
- J. Discharge of groundwater to the Keep River to occur only if all other strategies have been undertaken and there is sufficient flow as determined by Condition 11.H. Discharge must be in the K1 pool or downstream in the Keep River estuary (as identified in Figure 5 of the Supplementary Environmental Impact Statement), with discharge timings and rules developed with consideration of ebb tides and in consultation with the **Independent Review Group**.
- K. Contingency actions to dispose of excess groundwater should monitoring results from Condition 10.C and 10.G indicate there are likely to be adverse impacts on listed threatened species as a result of the action.
- L. An Operational Surface Water Model (OSWM) (that incorporates the outcomes of Conditions 11.A, 11.G and 11.H, and the requirements of 11.J and 11.K) to minimise discharges of stormwater and groundwater into the Border Creek and Keep River and ensure that all flow rules are complied with. A framework of the OSWM must be provided prior to **commencement of irrigation** and a full model, which includes updated monitoring results, provided within 12 months of the **commencement of irrigation**. The OSWM must be updated on a **seasonal** basis.
- M. Contingency measures should water quality and flow trigger values be exceeded or there are impacts on the health of threatened species as identified in aquatic fauna monitoring results in Condition 10.G. This must include the ceasing of discharge of stormwater and groundwater to the Border Creek and Keep River, implementation of a high intensity (at least daily) water quality sampling program, release of fresh irrigation water to flush the system and changes to farm practices such as reducing or ceasing the use of fertilisers and chemicals.
- N. Protocols and timelines for review and reporting to the **Department**.

The approved Stormwater and Groundwater Discharge Management Plan must be implemented.

Note: To avoid doubt, if a condition of another approval held by the proponent requires a Stormwater and Groundwater Discharge Management Plan (or a similar plan), the proponent may simultaneously meet the relevant requirements of both conditions by submitting a single plan.

## 12. Groundwater Management Plan

In order to protect listed threatened species in the Keep River, the person taking the action must prepare a Groundwater Management Plan (GMP) in consultation with the **Independent Review Group**. The GMP must be submitted for approval by the **Minister**. **Clearance of farm lots** must not be undertaken until the GMP is approved. The GMP must include:

- A. Expansion of the existing groundwater monitoring bore network for the collection of baseline and ongoing groundwater data. The expanded bore network must be

installed prior to commencing **clearance of farm lots** and at least 18 months before the **commencement of irrigation** and must include:

- i. At least 20 high intensity regional bores, and
- ii. At least 30 low intensity regional bores.

The management plan must indicate the locations for the expanded bore network;

- B. Monitoring of the bores established under Condition 12.A to collect baseline and ongoing groundwater data. Baseline monitoring must commence at least 18 months prior to **commencement of irrigation**.

Sampling parameters must be determined in consultation with the **Independent Review Group** and must include:

- i. High intensity bores:  
Daily groundwater levels and temperature monitoring;  
**Seasonal** monitoring of Electrical Conductivity (EC), pH, Total Dissolved Solids (TDS), major cations and anions, nutrients and pesticides;
- ii. Low intensity bores:  
**Seasonal** monitoring of EC, pH, groundwater levels, TDS, nutrients and pesticides;

- C. The establishment of at least one on-farm bore per farm. The on-farm bore network must be installed prior to **commencement of irrigation**;
- D. Monitoring of the on-farm bores established under Condition 12.C to collect baseline and ongoing groundwater data. Parameters for monitoring must be determined in consultation with the **Independent Review Group** and must include **seasonal** monitoring of groundwater levels, EC and pH;
- E. Updates of the groundwater model and operation of the groundwater management system with monitoring data derived from Conditions 12.B and 12.D to assist in determining an optimal dewatering strategy. Numerical groundwater modelling must be updated prior to **commencement of irrigation** and in consultation with the **Independent Review Group**. Subsequent updates must be conducted every 2-4 years depending on monitoring in Condition 12.D (if worse case scenario indicates a breach in trigger levels, modelling must be updated every 2 years, otherwise every 4 years);
- F. Monitoring of the bores established under Condition 12.C for physical, chemical and nutrient parameters, if high or low intensity bores exceed groundwater quality or groundwater level triggers. Sampling must include groundwater levels, EC, TDS, major cations and anions, nutrients, pesticides and pH and must be undertaken on a **seasonal** basis for five years following the exceedance of trigger levels.
- G. Establishment of groundwater quality trigger levels for chemicals and nutrients through the use of baseline groundwater quality monitoring in accordance with **ANZECC guidelines (2000)**. **ANZECC guidelines** trigger values for a 'high conservation/ecological value system' must be adopted for the initial 3 year period. Site specific trigger levels may be determined following this period based on **ANZECC guidelines** protocols.
- H. Establishment of groundwater management infrastructure, including a network of groundwater abstraction bores in the **Development Area** and **Buffer Area** and discharge infrastructure at the K1 pool or downstream in the Keep River estuary designed in consultation with the **Independent Review Group**. Forecasting of trigger level exceedance must be projected 10 years into the future. Abstraction wells and

groundwater discharge infrastructure must be installed and operational prior to any expected breach of trigger levels based on forecasting (incorporating the accuracy of the model into installation timings).

- I. Establishment of a series of high intensity reference bores, at locations agreed to by the **Independent Review Group**, to define a groundwater reference condition. The reference bores must be installed at least 18 months prior to **commencement of irrigation**.
- J. Monitoring of the bores established under Condition 12.1 to collect reference baseline and ongoing groundwater data. Sampling must include daily groundwater levels and temperature and **seasonal** EC and pH levels.
- K. Details of contingency measures should groundwater levels, soil salinity, chemicals or nutrients exceed trigger levels. This must include details of increased monitoring, implementation of a groundwater control program and changes to farm practices such as reducing or ceasing the use of fertilisers and chemicals.
- L. Details of contingency measures to be implemented should trend analysis of groundwater levels exceed the trend at reference bores by a rate determined in consultation with the **Independent Review Group**. This must include details of increased monitoring and implementation of a groundwater control program.
- M. Protocols and timelines for review and reporting to the **Department**.

The approved Groundwater Management Plan must be implemented.

Note: To avoid doubt, if a condition of another approval held by the proponent requires a Groundwater Management Plan, the proponent may simultaneously meet the relevant requirements of both conditions by submitting a single plan.

### 13. Decommissioning Plan

In order to protect listed threatened species, the person taking the action must prepare a Decommissioning Plan (DP), in consultation with the **WA DEC**. A preliminary DP must be submitted for approval by the **Minister** not more than 5 years after **commencement of the action** and a final DP submitted at least 6 months prior to the anticipated date of decommissioning. The DP must include:

- A. The progressive removal or reuse of infrastructure where operations cease;
- B. Establishment of management practices and safeguards to minimise environmental disturbance;
- C. Measures to ensure **Matters of National Environmental Significance** are not impacted by progressive decommissioning, or final decommissioning of infrastructure;
- D. Rehabilitation actions for the infrastructure sites following decommissioning including for:
  - i. optimising habitat and habitat connectivity for **Matters of National Environmental Significance**;
  - ii. enhancing pre-construction environmental quality; and
  - iii. ongoing management during rehabilitation.

The approved Decommissioning Plan must be implemented.

Note: To avoid doubt, if a condition of another approval held by the proponent requires a Decommissioning Plan, the proponent may simultaneously meet the relevant requirements of both conditions by submitting a single plan.

#### 14. Offset Management Plan

In order to offset the potential impacts on listed threatened species, including the endangered Gouldian Finch (*Erythrura gouldiae*), the endangered Northern Quoll (*Dasyurus hallucatus*), the vulnerable Red Goshawk (*Erythrorchis radiates*), the vulnerable Crested Shrike-tit (*Falcunculus frontatus whitei*), the critically endangered Speartooth Shark (*Glyphis glyphis*), the endangered Northern River Shark (*Glyphis garricki*), the vulnerable Freshwater Sawfish (*Pristis Microdon*) and the vulnerable Dwarf Sawfish (*Pristis clavata*), the person taking the action must prepare an Offset Management Plan (OMP) in consultation with the **WA DEC**. The OMP must be submitted for approval by the **Minister**. The OMP must be submitted to the **Department** for approval by the **Minister** no later than 12 months after the date of this approval decision.

The OMP must include, but should not be limited to:

- A. Details of the direct offsets proposed in the draft Environmental Impact Statement and how these will deliver long-term conservation benefits for relevant terrestrial listed threatened species that would not otherwise be achieved. This must include:
  - i. Mapping of the native vegetation habitat suitable for listed threatened species;
  - ii. Details of the area and characteristics of suitable habitat for listed threatened species;
  - iii. Details of whether the offset site provides the same landscape function and habitat type for the listed species as the habitat cleared or impacted by the proposal;
  - iv. Details of whether the offset site delivers a real conservation outcome that would not have otherwise been achieved (i.e. whether it was to be protected regardless of the action);
  - v. Steps that will be taken to ensure that any direct offset site will be protected in perpetuity for conservation purposes and details of evidence that will be provided to the **Department** that conservation covenants have been entered into;
  - vi. Provision of ongoing management of the offset site, including details of funding mechanisms.
- B. Details of alternative direct or indirect offsets if the proposed offsets do not satisfy the requirements listed in Condition 14.A;
- C. Funding of research activities, agreed by the **Department**, to an amount of no less than \$150,000 per year for 10 years, for the management, monitoring and/or improved protection of the critically endangered Speartooth Shark (*Glyphis glyphis*), the endangered Northern River Shark (*Glyphis garricki*), the vulnerable Freshwater Sawfish (*Pristis Microdon*) and the vulnerable Dwarf Sawfish (*Pristis clavata*). The proposed research activities must be developed in consultation with the **Sawfish and Glyphis Recovery Team**. Payments must be made to a trust fund agreed to by the **Department**. Research activities must be approved and the first yearly payment must be provided within 18 months of the date of this approval decision.

The approved Offset Management Plan must be implemented.

15. If the person taking the action wishes to carry out any activity otherwise than in accordance with any of the management plans as specified in the conditions, the person taking the action must submit to the **Department** for the **Minister's** written approval a revised version of that management plan. The varied activity shall not commence until the **Minister** has approved the varied management plan in writing. The **Minister** will not approve a varied management plan unless the revised management plan would result in an equivalent or improved environmental outcome over time. If the **Minister** approves the

revised management plan, that management plan, must be implemented in place of the management plan originally approved.

16. If the **Minister** believes that it is necessary or convenient for the better protection of the listed threatened and migratory species to do so, the **Minister** may request that the person taking the action make specified revisions to the management plans specified in the conditions and submit the revised management plan for the **Minister's** written approval. The person taking the action must comply with any such request. The revised approved management plan must be implemented. Unless the **Minister** has approved the revised management plan, then the person taking the action must continue to implement the management plan originally approved, as specified in the conditions.
17. Unless otherwise agreed to in writing by the **Minister**, the person taking the action must publish all management plans referred to in these conditions of approval and any baseline information and monitoring results required by these plans on their website. Each management plan must be published on the website within 1 month of being approved and all baseline information and monitoring results must be published on the website annually, beginning twelve months after the **commencement of the action**.
18. Prior to the sale of any land the person taking the action must provide evidence to the **Department** that any relevant conditions (including, but not limited to the requirements of Conditions 6, 7, 8, 10, 11, 12 and 13) have been registered on the title.
19. Annually, beginning twelve months after the **commencement of the action**, the person taking the action must ensure that an independent audit of compliance with the conditions of approval is conducted and a report submitted to the **Minister**. The independent auditor must be approved by the **Minister** prior to the commencement of the audit. Audit criteria must be agreed to by the **Minister** and the audit report must address the criteria to the satisfaction of the **Minister**.
20. If, at any time after five years from the date of this approval, the person taking the action has not substantially commenced the action, then the person taking the action must not substantially commence the action without the written agreement of the **Minister**.

**Definitions:**

**ANZECC guidelines** are the Australian and New Zealand Guidelines for Fresh and Marine Water Quality (2000).

**AUSRIVAS** is the Australian River Assessment System.

The **Buffer Area** is the area surrounding the **development area**, as shown in Figure 2 of the Supplementary Environmental Impact Statement.

**Clearance of farm lots** is the removal of any vegetation on the farm lots identified in the Supplementary Environmental Impact Statement.

**Commencement of the action** is the construction of any infrastructure associated with the proposed action.

**Commencement of irrigation** is the commencement of any irrigation of farmland in the development area.

The **Department** is the Australian Government Department administering the *Environment Protection and Biodiversity Conservation Act 1999*.

The **Development Area** is the area to be developed, including farm lots and associated infrastructure, not including the **Buffer Area**, as shown in Figure 2 of the Supplementary Environmental Impact Statement.

***Glyphis and pristis expert*** is a person with at least five years experience in ecology and experience in *Pristis* and *Glyphis* species ecology and management.

The **EPBC Act** is the *Environment Protection and Biodiversity Conservation Act 1999*.

**Gouldian Finch Expert** is a person with at least five years experience in ecology and experience in Gouldian Finch (*Erythrura gouldiae*) ecology and management.

The **Minister** is the Minister administering the *Environment Protection and Biodiversity Conservation Act 1999* and includes a delegate of the Minister.

**Phenology** is the study of periodic life cycle events and how these are influenced by seasonal and interannual variations in climate.

The **Project Area** is the whole footprint of the proposal, including the **development area** and the **Buffer Area**, as shown in Figure 2 of the Supplementary Environmental Impact Statement.

The **Sawfish and Glyphis Recovery Team** is the team working on the *Draft Recovery Plan for Sawfish and Glyphis*, being led by the **Department**.

**Seasonal** means the start of the dry season prior to irrigation and the start of the wet season prior to wet season rains.

**Tailwater Management System** is a system to retain first-flush rainfall and irrigation runoff from farms, as outlined in section 3.2.2 of the Draft Environmental Impact Statement.

**WA DEC** means the Western Australian Department of Environment and Conservation or its successor organisation.

A **water year** is the year beginning 1 November and ending 31 October.

## Appendix B

## Certificates of Title and Diagrams

WESTERN



AUSTRALIA

REGISTER NUMBER <b>372/DP51355</b>	
DUPLICATE EDITION <b>N/A</b>	DATE DUPLICATE ISSUED <b>N/A</b>

**RECORD OF CERTIFICATE  
OF  
CROWN LAND TITLE**

VOLUME **LR3157** FOLIO **310**

UNDER THE TRANSFER OF LAND ACT 1893  
AND THE LAND ADMINISTRATION ACT 1997

**NO DUPLICATE CREATED**

The undermentioned land is Crown land in the name of the STATE of WESTERN AUSTRALIA, subject to the interests and Status Orders shown in the first schedule which are in turn subject to the limitations, interests, encumbrances and notifications shown in the second schedule.

*J.R. Roberts*

REGISTRAR OF TITLES



**LAND DESCRIPTION:**

LOT 372 ON DEPOSITED PLAN 51355

**STATUS ORDER AND PRIMARY INTEREST HOLDER:  
(FIRST SCHEDULE)**

**STATUS ORDER/INTEREST:** UNALLOCATED CROWN LAND

**PRIMARY INTEREST HOLDER:** STATE OF WESTERN AUSTRALIA

**LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS:  
(SECOND SCHEDULE)**

1. I798304      AUTHORISATION ORDER FOR THE TAKING OF INTERESTS. REGISTERED 23.2.2004.  
K896104      AMENDMENT OF AUTHORISATION ORDER. AUTHORISATION ORDER AMENDED.  
REGISTERED 31.3.2009.
2. I798305      NOTICE OF INTENTION TO TAKE FOR THE DESIGNATED PURPOSE OF IRRIGATED  
AGRICULTURAL, HORTICULTURAL & ANCILLARY & ASSOCIATED PURPOSES  
INCLUDING IRRIGATION, DRAINAGE, ROAD & POWER INFRASTRUCTURE, FLOOD  
PROTECTION LEVEES, PROVISION OF CONSTRUCTION MATERIALS & LAND FOR  
MAINTENANCE OF BIO DIVERSITY & PROTECTION OF ABORIGINAL HERITAGE.  
REGISTERED 23.2.2004.  
J100450      NOTICE OF INTENTION TO TAKE AMENDED REGISTERED 29.11.2004.  
K896105      NOTICE OF INTENTION TO TAKE AMENDED REGISTERED 31.3.2009.  
L239069      PERIOD OF CURRENCY IS NOW INCREASED TO 8 YEARS FROM 23.2.2004  
REGISTERED 22.2.2010.
3. K896170      MEMORIAL. LAND ADMINISTRATION ACT 1997. SECTION 17. LODGED 31.3.2009.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.  
Lot as described in the land description may be a lot or location.

-----END OF CERTIFICATE OF CROWN LAND TITLE-----

**STATEMENTS:**

The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents or for local government, legal, surveying or other professional advice.

END OF PAGE 1 - CONTINUED OVER

ORIGINAL CERTIFICATE OF CROWN LAND TITLE

REGISTER NUMBER: 372/DP51355

VOLUME/FOLIO: LR3157-310

PAGE 2

SKETCH OF LAND: DP51355 [SHEET 1].  
PREVIOUS TITLE: LR3073-344.  
PROPERTY STREET ADDRESS: NO STREET ADDRESS INFORMATION AVAILABLE.  
LOCAL GOVERNMENT AREA: SHIRE OF WYNDHAM-EAST KIMBERLEY.  
RESPONSIBLE AGENCY: DEPARTMENT OF REGIONAL DEVELOPMENT AND LANDS (SLSD).

NOTE 1: K904987 CORRESPONDENCE FILE 51160-2005-03RO  
NOTE 2: SUBJECT TO SURVEY - NOT FOR ALIENATION PURPOSES

WESTERN



AUSTRALIA

REGISTER NUMBER <b>373/DP51355</b>	
DUPLICATE EDITION <b>N/A</b>	DATE DUPLICATE ISSUED <b>N/A</b>

**RECORD OF CERTIFICATE  
OF  
CROWN LAND TITLE**

VOLUME **LR3157** FOLIO **311**

UNDER THE TRANSFER OF LAND ACT 1893  
AND THE LAND ADMINISTRATION ACT 1997

**NO DUPLICATE CREATED**

The undermentioned land is Crown land in the name of the STATE of WESTERN AUSTRALIA, subject to the interests and Status Orders shown in the first schedule which are in turn subject to the limitations, interests, encumbrances and notifications shown in the second schedule.

*R. Roberts*

REGISTRAR OF TITLES



**LAND DESCRIPTION:**

LOT 373 ON DEPOSITED PLAN 51355

**STATUS ORDER AND PRIMARY INTEREST HOLDER:  
(FIRST SCHEDULE)**

**STATUS ORDER/INTEREST:** UNALLOCATED CROWN LAND

**PRIMARY INTEREST HOLDER:** STATE OF WESTERN AUSTRALIA

**LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS:  
(SECOND SCHEDULE)**

1. I798304 AUTHORITY ORDER FOR THE TAKING OF INTERESTS. REGISTERED 23.2.2004.  
K896104 AMENDMENT OF AUTHORITY ORDER. AUTHORITY ORDER AMENDED.  
REGISTERED 31.3.2009.
2. I798305 NOTICE OF INTENTION TO TAKE FOR THE DESIGNATED PURPOSE OF IRRIGATED  
AGRICULTURAL, HORTICULTURAL & ANCILLARY & ASSOCIATED PURPOSES  
INCLUDING IRRIGATION, DRAINAGE, ROAD & POWER INFRASTRUCTURE, FLOOD  
PROTECTION LEVEES, PROVISION OF CONSTRUCTION MATERIALS & LAND FOR  
MAINTENANCE OF BIO DIVERSITY & PROTECTION OF ABORIGINAL HERITAGE.  
REGISTERED 23.2.2004.  
J100450 NOTICE OF INTENTION TO TAKE AMENDED REGISTERED 29.11.2004.  
K896105 NOTICE OF INTENTION TO TAKE AMENDED REGISTERED 31.3.2009.  
L239069 PERIOD OF CURRENCY IS NOW INCREASED TO 8 YEARS FROM 23.2.2004  
REGISTERED 22.2.2010.
3. K896170 MEMORIAL. LAND ADMINISTRATION ACT 1997. SECTION 17. LODGED 31.3.2009.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.  
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-----END OF CERTIFICATE OF CROWN LAND TITLE-----

**STATEMENTS:**

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END OF PAGE 1 - CONTINUED OVER

ORIGINAL CERTIFICATE OF CROWN LAND TITLE

REGISTER NUMBER: 373/DP51355

VOLUME/FOLIO: LR3157-311

PAGE 2

SKETCH OF LAND: DP51355 [SHEET 1].  
PREVIOUS TITLE: LR3073-344.  
PROPERTY STREET ADDRESS: NO STREET ADDRESS INFORMATION AVAILABLE.  
LOCAL GOVERNMENT AREA: SHIRE OF WYNDHAM-EAST KIMBERLEY.  
RESPONSIBLE AGENCY: DEPARTMENT OF REGIONAL DEVELOPMENT AND LANDS (SLSD).

NOTE 1: K904988 CORRESPONDENCE FILE 51160-2005-03RO  
NOTE 2: SUBJECT TO SURVEY - NOT FOR ALIENATION PURPOSES  
NOTE 3: L461615 DEPOSITED PLAN (INTEREST ONLY) 69374 LODGED.

WESTERN



AUSTRALIA

REGISTER NUMBER <b>394/DP58305</b>	
DUPLICATE EDITION <b>N/A</b>	DATE DUPLICATE ISSUED <b>N/A</b>

**RECORD OF CERTIFICATE  
OF  
CROWN LAND TITLE**

VOLUME **LR3157** FOLIO **357**

UNDER THE TRANSFER OF LAND ACT 1893  
AND THE LAND ADMINISTRATION ACT 1997

**NO DUPLICATE CREATED**

The undermentioned land is Crown land in the name of the STATE of WESTERN AUSTRALIA, subject to the interests and Status Orders shown in the first schedule which are in turn subject to the limitations, interests, encumbrances and notifications shown in the second schedule.

*R. Roberts*

REGISTRAR OF TITLES



**LAND DESCRIPTION:**

LOT 394 ON DEPOSITED PLAN 58305

**STATUS ORDER AND PRIMARY INTEREST HOLDER:  
(FIRST SCHEDULE)**

**STATUS ORDER/INTEREST:** UNALLOCATED CROWN LAND

**PRIMARY INTEREST HOLDER:** STATE OF WESTERN AUSTRALIA

**LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS:  
(SECOND SCHEDULE)**

1. I798304 AUTHORISATION ORDER FOR THE TAKING OF INTERESTS. AS TO PORTION ONLY. REGISTERED 23.2.2004.
2. I798305 NOTICE OF INTENTION TO TAKE FOR THE DESIGNATED PURPOSE OF IRRIGATED AGRICULTURAL, HORTICULTURAL & ANCILLARY & ASSOCIATED PURPOSES INCLUDING IRRIGATION, DRAINAGE, ROAD & POWER INFRASTRUCTURE, FLOOD PROTECTION LEVEES, PROVISION OF CONSTRUCTION MATERIALS & LAND FOR MAINTENANCE OF BIO DIVERSITY & PROTECTION OF ABORIGINAL HERITAGE. AS TO PORTION ONLY - SEE DP40686. REGISTERED 23.2.2004.
- J100450 NOTICE OF INTENTION TO TAKE AMENDED REGISTERED 29.11.2004.
- L239069 PERIOD OF CURRENCY IS NOW INCREASED TO 8 YEARS FROM 23.2.2004 REGISTERED 22.2.2010.
3. K854721 NOTIFICATION. THE ASSETS OF THE WATER CORPORATION AS SET OUT IN THE WITHIN TRANSFER ORDER THAT IS UPON, IN, OVER OR UNDER THE WITHIN LAND ARE VESTED IN ORD IRRIGATION ASSET MUTUAL CO-OPERATIVE LTD PURSUANT TO SECTION 46(C)(1)(a) OF THE WATER SERVICES LICENSING ACT 1995. AS TO PORTION ONLY - SEE DEPOSITED PLAN 56625. REGISTERED 16.2.2009.
4. K896170 MEMORIAL. LAND ADMINISTRATION ACT 1997. SECTION 17. LODGED 31.3.2009.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.  
Lot as described in the land description may be a lot or location.

-----END OF CERTIFICATE OF CROWN LAND TITLE-----

END OF PAGE 1 - CONTINUED OVER

ORIGINAL CERTIFICATE OF CROWN LAND TITLE

REGISTER NUMBER: 394/DP58305

VOLUME/FOLIO: LR3157-357

PAGE 2

**STATEMENTS:**

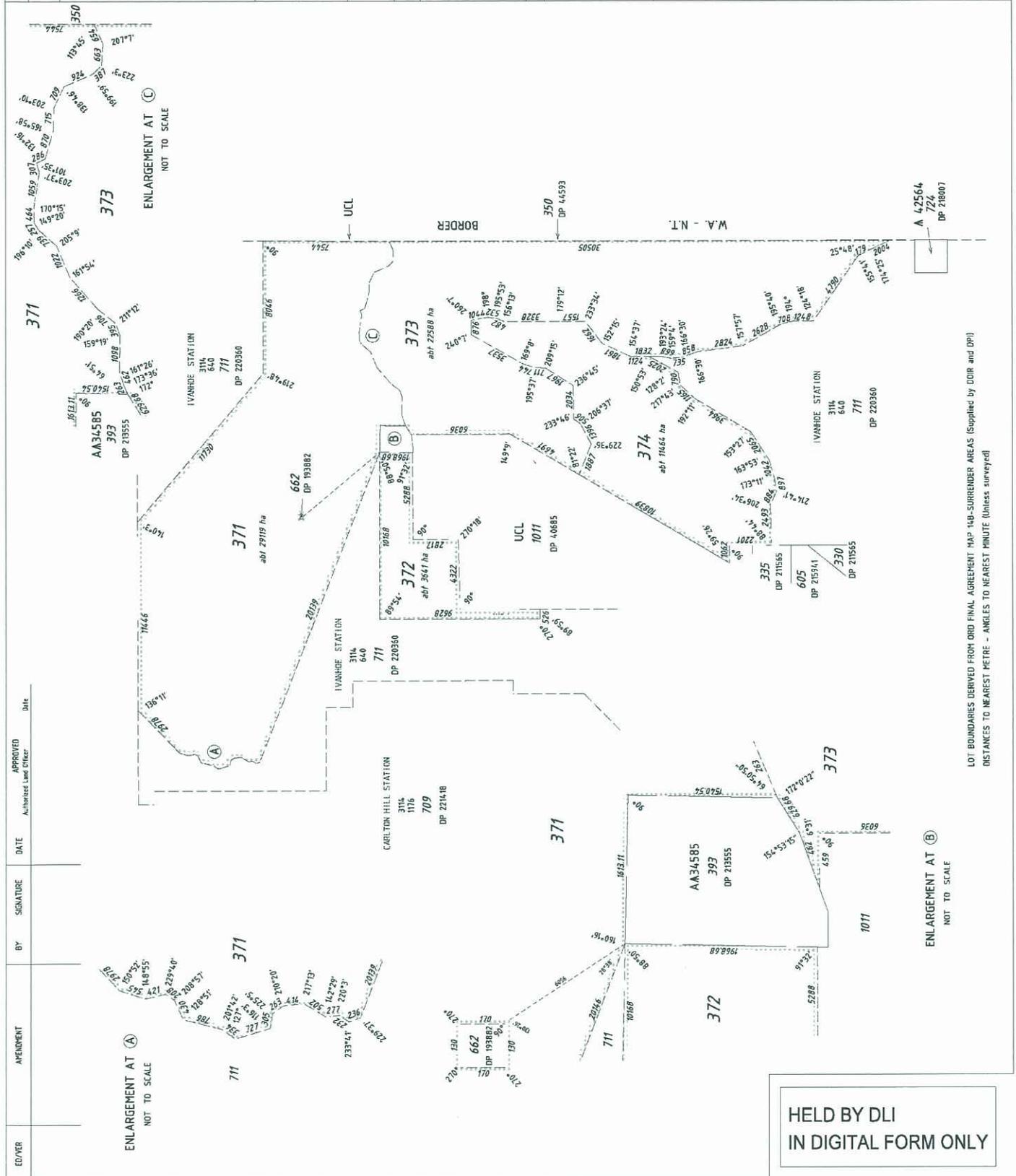
The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents or for local government, legal, surveying or other professional advice.

SKETCH OF LAND: DP58305 [SHEET 1].  
PREVIOUS TITLE: LR3133-953.  
PROPERTY STREET ADDRESS: NO STREET ADDRESS INFORMATION AVAILABLE.  
LOCAL GOVERNMENT AREA: SHIRE OF WYNDHAM-EAST KIMBERLEY.  
RESPONSIBLE AGENCY: DEPARTMENT OF REGIONAL DEVELOPMENT AND LANDS (SLSD).

NOTE 1: K905003 CORRESPONDENCE FILE 51160-2005-03RO  
NOTE 2: SUBJECT TO SURVEY - NOT FOR ALIENATION PURPOSES  
NOTE 3: L461615 DEPOSITED PLAN (INTEREST ONLY) 69374 LODGED.



EDDVER	AMENDMENT	BY	SIGNATURE	DATE	APPROVED Authorized Land Officer	Date
TYPE		CROWN				
PURPOSE		SUBDIVISION				
PLAN OF		LOTS 371 - 374				
DISTRICT	KING	FORMED TENURE				
TOWNSHIP	0850-193/17	PT. LOT 711 ON DP 220360				
FILE	0850-193/17	CL 3070/244				
LOCAL AUTHORITY	SHIRE OF WINDHAM - EAST	LOCAL AUTHORITY				
LOCALITY	KUMBUKRA	LOCALITY				
ON	CANBERRA GOLF (250)	REFERENCE				
SCALE	1:150000	0 500 1000 1500 2000 2500				
ALL DISTANCES ARE IN METRES						
SUBJECT TO SURVEY						
NOT FOR ALIENATION PURPOSES						
SURVEY FROM	DLI - M. BAKER	23/5/2006				
LOADED	DATE	24/1/2008				
TYPE OF VALIDATION	DATE	24/1/2008				
LEGAL OPINION	DATE	4/2/2008				
REGISTERED	DATE	4/2/2008				
COMMIT	DATE	4/2/2008				
IN ORDER FOR DEALINGS	Surrender from P/L 374/640					
FOR AUTHORIZED LAND OFFICER	DATE	4/2/2008				
APPROVED	DATE	31-Mar-2009				
AUTHORIZED LAND OFFICER	DATE	31-Mar-2009				



LOT BOUNDARIES DERIVED FROM ORD FINAL AGREEMENT MAP 148-SURRENDER AREAS (Supplied by DDR and DPI)  
DISTANCES TO NEAREST METRE - ANGLES TO NEAREST MINUTE (Unless surveyed)

HELD BY DLI  
IN DIGITAL FORM ONLY

DEPOSITED PLAN  
**51355**

SHEET 1 OF 1  
EDITION 1 VERSION 1



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**Attachment 5 – Schedule of Submissions**

**SHIRE OF WYNDHAM - EAST KIMBERLEY  
TOWN PLANNING SCHEME NO. 7 – KUNUNURRA AND ENVIRONS**

**Amendment No. 40: Ord East Kimberley Expansion Project  
Schedule of Submissions**

<b>SUBMISSION NO. NAME &amp; ADDRESS</b>	<b>OBJECTION OR SUPPORT</b>	<b>SUMMARY OF RESPONSES</b>	<b>COMMENTS</b>
<p><b>RESPONSE 1</b> Department of Agriculture and Food WA PO Box 19 Kununurra WA 6743</p>	<p>No Comment</p>	<p>The Department of Agriculture and Food WA has been involved in the development of the Ord East Kimberley expansion project, and has no comments on these proposed amendments.</p>	<p>Noted.</p>
<p><b>RESPONSE 2</b> Department of Water PO Box 625 Kununurra WA 6743</p>	<p>No Objection</p>	<p>The Department of Water has no objection to the proposed amendment. The Department has the following comment:</p> <ol style="list-style-type: none"> <li>1. The proposed area is located within the Canning-Kimberley Groundwater area, the Ord River and Tributaries surface area and the Ord Irrigation District , all of which are proclaimed under the <i>Rights in Water Irrigation Act 1914</i> (RIWI Act). Therefore if the proponent requires groundwater or surface water they will be required a 5C licence to take water.</li> <li>2. The construction of production bores within proclaimed areas, and construction of all artesian wells, remain subject to licensing requirements under the RIWI Act.</li> <li>3. Proponents undertaking any works that interfere with the bed and banks of a watercourse may require a permit. These works may include constructing tracks and watercourse crossings.</li> </ol>	<p>Noted. These comments relate to subdivisional works currently underway, and potential purchasers should be made aware of the Departments licensing requirements.</p>

## Attachment 6 – Copies of Submissions

**Jennifer Ninyette**

---

**From:** Wilson, Noel <noel.wilson@agric.wa.gov.au>  
**Sent:** Wednesday, 11 July 2012 9:56 AM  
**To:** Jennifer Ninyette  
**Subject:** Proposed amendment No. 40 to Shire of Wyndham East Kimberley Town Planning Scheme No. 7 - Kununurra and Environs - Ord East Kimberley Expansion Project

Hi Jennifer,

**Re: Proposed amendment No. 40 to Shire of Wyndham East Kimberley Town Planning Scheme No. 7 - Kununurra and Environs - Ord East Kimberley Expansion Project**

As the Department of Agriculture and Food WA has been involved in the development of the Ord East Kimberley expansion project I have no comments on these proposed amendments.

Regards,

*Noel Wilson  
Kimberley Manager  
Department of Agriculture and Food WA  
PO Box 19 Kununurra 6743  
Phone - 0891664001  
Mobile - 0829105336*

---

**This e-mail and files transmitted with it are privileged and confidential information intended for the use of the addressee. The confidentiality and/or privilege in this e-mail is not waived, lost or destroyed if it has been transmitted to you in error. If you received this e-mail in error you must (a) not disseminate, copy or take any action in reliance on it; (b) please notify the Department of Agriculture and Food, WA immediately by return e-mail to the sender; (c) please delete the original e-mail.**  
Department of Agriculture and Food WA



Government of Western Australia  
Department of Water

12 JUL 2012

Your ref: LP.04.59: TP2385  
Our ref: RF3126-02  
SRS31398 looking after all our water needs  
Enquiries: Amber Briggs, 9166 4114

I-14452  
LP.04.59

Jennifer Ninyette  
Senior Planning Officer  
Shire of Wyndham – East Kimberley  
PO Box 614  
Kununurra WA 6743

Dear Jennifer

**PROPOSED AMENDMENT NO 40 TO SHIRE OF WYNDHAM EAST KIMBERLEY  
TOWN PLANNING SCHEME NO. 7 – KUNUNURRA AND ENVIRONS – ORD EAST  
KIMBERLEY EXPANSION PROJECT**

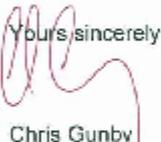
Thank you for the above referral, received in our office on 8 June 2012. The Department of Water (DoW) has assessed the referral and has no objection to the proposed amendment. The DoW has the following comments.

The proposed area is located within the Canning-Kimberley groundwater area, the Ord River and Tributaries surface water area and the Ord Irrigation District, all of which are proclaimed under the *Rights in Water and Irrigation Act 1914* (RIWI Act). Therefore, if the proponent requires groundwater or surface water they will need to apply for a 5C licence to take water.

Please note that the DoW's advice in relation to the requirements for a 26D licence to construct/alter a well for some activities has recently changed. Under the *Rights in Water and Irrigation Exemption (Section 26C) Order 2012*, gazetted on 16 March 2012, a 26D licence is no longer required for the construction or alteration of non-artesian wells that are used solely to monitor water levels and/or water quality. The construction of production bores within proclaimed areas, and the construction of all artesian wells, remain subject to licensing requirements under the RIWI Act.

Proponents undertaking any works that interfere with the bed and banks of a watercourse may require a permit. These works may include constructing tracks and watercourse crossings.

Should you have any further queries regarding this matter please contact Amber Briggs in the Kununurra office on 9166 4114.

Yours sincerely  


Chris Gunby  
A/Regional Manager  
Kimberley Region

5 July 2012

Kimberley Region  
27 Victoria Highway Kununurra Western Australia 6743  
PO Box 625 Kununurra Western Australia 6743  
Telephone (08) 9166 4100 Facsimile (08) 9166 3174  
www.water.wa.gov.au  
wa.gov.au

3/2012/001

## 12.4.2 East Kimberley Regional Airport Master Plan

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Victoria Highway, Kununurra
<b>AUTHOR:</b>	Jennifer Ninnette, Senior Planning Officer
<b>REPORTING OFFICER:</b>	Nick Kearns, Director Community Development
<b>FILE NO:</b>	LP.02.10

### **PURPOSE**

For Council to consider receiving the East Kimberley Regional Master Plan.

### **BACKGROUND**

In late 2003, the Shire of Wyndham East Kimberley commissioned the preparation of a business plan to improve the operational effectiveness of the East Kimberley Regional Airport. The report, which was finalised in January 2003, identified a number of strategies to:

- Attract (new) major air service provider(s);
- Identify appropriate complementary business functions within the airport site;
- Strengthen tourism, such as accommodation and other tourist services; and
- Establish an Airport Development Committee to oversee the direction of the airport.

Since that time the Shire's Airport Committee has reviewed a number of options to strengthen the financial performance and function of the airport – particularly with respect to providing opportunities for new development within the airport site.

To this end, a number of conceptual master plans were prepared, including a proposal to subdivide land for private development, future internal roads (non airside) as well an expansion to the airport terminal infrastructure including the main car park. However, these plans lacked a focus on airside aviation requirements and an association with the Airport Business Plan.

In August 2010 the Shire undertook a significant upgrade and expansion to the airport terminal. This \$12 million dollar project includes a baggage carousel, café, new toilets, kiosk, private lounge, internal and external waiting areas, offices and landscaping. The airport terminal upgrade and expansion was completed in December 2011, delivering to the region a terminal that provides a level of functionality and comfort never before experienced.

The Shire has since sought to 'import' relevant expertise in aviation – to recommend a range of actions and measures which will build upon the previous work undertaken, and for this to be presented as an East Kimberley Regional Airport Master Plan. The Master Plan is intended to provide recommendations to achieve the following objective to:

- Provide for long term sustainability;
- Cater to current and future growth;
- Balance between airport functions and surrounding land use;
- Facilitate additional aviation business and commercial development; and
- Protect land for aviation growth and expansion.

In September 2011 the Shire invited quotes for the preparation of the East Kimberley Regional Airport (EKRA) Master Plan, to provide a strategic planning framework for the next

10 years to guide the future growth and development of the EKRA, appointing Rehbein Airport Consulting in late October 2011.

Since this time, Rehbein Airport Consulting has consulted with the Shire and other relevant stakeholders to prepare the draft East Kimberley Regional Airport Master Plan which was presented to Councillors on the 17<sup>th</sup> July 2012, which includes:

- Opportunities and constraints;
- Concept drawings;
- Consideration of a change of demographic and business profiles in light of anticipated growth within the region.
- Development of options and growth scenario's
- Aeronautical and Non- Aeronautical development concepts and recommendations, including infrastructure utility services;
- Staged development pan and indicative development costs; and
- Implementation plan.

### **STATUTORY IMPLICATIONS**

The draft master plan has been prepared by a suitably qualified airport consultant, who has liaised with relevant agencies and ensured that the proposals are in accordance with legislative requirements.

### **POLICY IMPLICATIONS**

There are no policy implications associated with this item.

### **FINANCIAL IMPLICATIONS**

The master plan identifies key infrastructure developments, along with indicative development costs, however identifies that each element needs to go through a development process including stages of planning, design and procurement.

### **STRATEGIC IMPLICATIONS**

The EKRA Master Plan provides a strategic framework for the future development of the airport and sets out the required infrastructure likely to be required to support the forecast demand of passenger and aircraft movements at the airport over the next 10 to 25 years.

The master plan is essentially a land use planning tool that identifies the need to safeguard land for future growth and operation of the airport, and further stages of planning, design and procurement will be required for individual elements and infrastructure development proposed. It is also recommended that the master plan be reviewed every 5 years or following any major change in key drivers for airport growth.

### **COMMUNITY CONSULTATION**

The methodology for completing the Master Plan included stakeholder consultation to solicit the views, issues and concerns of Shire representatives, airlines, airport users, airport landowners/lessees and local businesses and organisations.

Following the initial stakeholder consultation, the draft master plan report was forwarded to relevant stakeholders for comment. A table of submissions received during this consultation period is provided at Attachment 2.

Minor changes have been made to the draft report following stakeholder consultation and the presentation to Councillors.

## **COMMENT**

The draft East Kimberley Regional Airport Master Plan has been drafted over a period of 9 months, and will provide the overarching framework for future provision of detailed development and operational plans for the airport.

Staging of the proposed infrastructure development is subject to a range of external factors as well as demand. The master plan has set out the proposed airport development over the next 25 years in two stages, Stage 1, the next 10 years (to 2021/22), and Stage 2, to 2036/37. Appropriate trigger points have also been identified for each of the developments.

Implementation of the master plan requires that each infrastructure element will need to go through a development process including stages of planning, design and procurement.

The master plan has highlighted an initial stage of planning is required due to the need for specific infrastructure developments. The table at Attachment 3 highlights the infrastructure developments that are considered to be most pertinent, and which should immediately continue through to detailed planning/feasibility studies being undertaken.

The key recommendations of the master plan report are summarised below:

### **Runway Development**

A key item is the development of a longer runway to support future increases in aircraft size and range of destinations. The draft Master Plan safeguards for the development of a runway to support operations to all domestic destinations on Regular Public Transport (RPT) services, and possibly to destinations in South East Asia, by providing for two runway development options:

- Runway Option 1 – involves the extension, widening and strengthening of the existing runway.
- Runway Option 2 – involves the development of a new runway located north of, and parallel to, the existing runway.

While Option 2 is preferred, runway development is subject to further investigation, planning and feasibility assessments, and land acquisition.

### **Passenger Terminal**

With the recent expansion and upgrade of the passenger terminal it is estimated that the current terminal is likely to accommodate potential demand up to around 2021-22, however that minor configuration of the terminal may be required during this time to accommodate the needs of terminal users.

It is expected that terminal require expansion will be required when two or more large aircraft (Boeing B737-800/Airbus A320-200) require simultaneous use of the terminal, and it is anticipated that between 4,000m<sup>2</sup> and 4,500m<sup>2</sup> of terminal footprint will be required by 2036/37.

### Aircraft Parking

The master plan identifies the need to expand the apron to allow for increased volumes RPT and charter aircraft, with a maximum of 4 aircraft parking positions required for RPT aircraft by 2036/37, however that the existing RPT apron will also require strengthening to accommodate larger jet aircraft

The master plan also proposes that a dedicated private jet apron be developed, to allow it to be completely separate from the RPT apron, excluding it from security screening requirements and allowing for convenient landside access.

The West General Aviation (GA) apron is intended to be expanded to accommodate visiting aircraft parking as well as any additional GA parking, and can be developed in sub-stages according to demand.

### Taxiway System

Proposes the upgrade and extension of the parallel taxiway (Taxiway F) to provide access to a number of airport facilities and precincts, and remove the current issue of GA traffic crossing the RPT apron, and which will require significant strengthening and even reconstruction of sections to accommodate larger aircraft.

Additional Taxiways are proposed to be constructed to provide practical and safe access to aprons and proposed aviation-related precinct development.

### Other Airfield Facilities

The Bureau of Meteorology Weather station may be required to be relocated north of the runway in later stages of the master plan to allow for extension of the West GA apron.

Runway Option 2 will require the relocation of the Doppler VOR/DME, and the master plan proposes that this facility be relocated north of the runway.

While there is no requirement to relocate the Non-Direction Beacon (NDB) for the runway development, there may be some benefit in the future to relocate the NBD north of the runway for the maximum and most effective use of land south of the runway available for aviation-related commercial purposes.

Both runway options will require the relocation of the wind indicators.

The master plan identifies that an additional (aviation) fuel facility may be required to be developed on the West GA apron to reduce the requirement for aircraft to taxi between the two GA aprons to access fuel.

### Aircraft Noise

An Australian Noise Exposure Forecast (ANEF) has been prepared for the 25 year planning horizon (2036/2037) of the Master Plan. An ANEF contour map has been produced, and has been submitted to Airservices Australia for technical endorsement, which is recommended to be incorporated in the Shire's Local Planning Scheme to ensure compatible development is proposed in the vicinity of the airport and future operations are protected.

N60 and N70 'Noise above', or 'N' contours have also been produced as part of the master plan to assist the understanding of the noise impact of the airport.

### Non-Aeronautical Concept

The master plan recommends that Council leases land, as opposed to selling freehold land, at the airport to allow for the Shire to maintain long-term control of the land which will facilitate staged development and implementation of the master plan.

A range of aviation-related activities have also been identified as having potential for development at EKRA, including private hangar storage, light aircraft maintenance, heavy aircraft maintenance, air cargo, support businesses for the mining or oil and gas industries and hotel accommodation among others.

Five potential land-use development types were identified and a number of precincts are located on the airport site concept plans for commercial development for these purposes, as listed below:

1. Aviation-related commercial development with airside access
2. Aviation-related commercial development without airside access
3. Aircraft maintenance/engineering
4. Air Cargo/Freight
5. Private Hangars

It is also proposed, if required, to relocate the Kununurra town depot to the south east corner of the existing airport site.

It is acknowledged in the EKRA master plan that the development of these land uses will require the significant upgrade of engineering services at the airport.

### Landside Access

The master plan recommends a detailed review of the existing airport access points and access road markings to ensure that landside access is efficient and not confusing to the driver, and should be subject to a more specific traffic study to be undertaken by the Shire.

In order to reduce crossover traffic at the passenger terminal forecourt, all non-passenger terminal related traffic should use alternative accesses including Dusty Rankin and Cyril Kleinig Drives. The EKRA master plan also recommends upgrading/construction of the existing Speedway access track to provide a new access from the highway located to the west of existing access points to provide access to the proposed private hangar precinct.

The master plan sets out the anticipated car parking development required to support the forecast growth in traffic, and proposes parking restrictions for short-stay parking, along with the development of secure and non-secure long stay parking, and that a fee be charged for the use of long-stay car parks

It is recommended that Council receive the draft East Kimberley Regional Master Plan, to provide a strategic land use planning tool for future growth and development of the airport.

### **ATTACHMENTS**

- Attachment 1 – East Kimberley Regional Airport Master Plan
- Attachment 2 – Schedule of Stakeholder Feedback
- Attachment 3 – Table II: EKRA 2012/13 Planning Studies

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council:

- 1) Receive the East Kimberley Regional Airport Master Plan.
- 2) Directs Shire Officers to develop an Action Plan based on the key recommendations of the East Kimberley Regional Airport Master Plan.

## **COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr D Ausburn**

**That Council:**

- 1) Receive the East Kimberley Regional Airport Master Plan.**
- 2) Directs Shire Officers to develop an Action Plan based on the key recommendations of the East Kimberley Regional Airport Master Plan.**

**Carried Unanimously 6/0**

## REHBEIN AIRPORT CONSULTING

DATE 13 August, 2012

CONTACT BEN HARGREAVES

Draft

# East Kimberley Regional Airport Master Plan For Shire of Wyndham East Kimberley

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**APPENDIX A**

STAKEHOLDER CONSULTATION SCHEDULE

**APPENDIX B**

DRAWINGS

Draft

*Document Control Page*

Revision	Date	Description	Author	Signature	Verifier	Signature	Approver	Signature
0	8/05/2012	DRAFT	JSS		BJH		BJH	
1	31/05/2012	FINAL DRAFT	JSS		BJH		BJH	
2	25/06/2012	FINAL DRAFT	JSS		BJH		BJH	
3	20/07/2012	FINAL DRAFT	JSS		BJH		BJH	
4	15/08/12	FINAL DRAFT	JSS		BJH		BJH	

## GLOSSARY OF TERMS AND ABBREVIATIONS

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ACN (Aircraft Classification Number)	A number expressing the relative effect of an aircraft on a pavement for a specified standard subgrade category.
ASV	Annual Service Volume
Aerodrome	A defined area on land or water (including any buildings, installations and equipment) intended to be used either wholly or in part for the arrival, departure and surface movement of aircraft.
AFRU (Aerodrome Frequency Response Unit)	The AFRU is an electronic, ground based, aviation safety enhancement device, intended for use on the CTAF or MBZ frequency at non-controlled aerodromes.
AIP ERSAs	Airservices Australia Aeronautical Information Package En-Route Supplement Australia
ANEF	Australian Noise Exposure Forecast
ARC (Aerodrome Reference Code)	A code used to specify the standards for individual aerodrome facilities which are suitable for use by aeroplanes within a range of performances and sizes. The code is composed of two elements: the first is a number (from 1 to 4) related to the aeroplane reference field length and the second is a letter (from A to F) related to the aeroplane wingspan and outer main gear wheel span.
ARP	Aerodrome Reference Point
ATC	Air Traffic Control
AWIS	Automatic Weather Information Service
BoM	Bureau of Meteorology
CAGR	Compound Annual Growth Rate
CASA (Civil Aviation Safety Authority)	The Australian federal government department responsible for setting and maintaining safety standards for civil aviation. CASA is responsible for the codification of international standards and recommended practices into Australian legislation and for the issue of licences for aviation personnel including pilots, amongst other responsibilities.
CASR (Civil Aviation Safety Regulation)	CASRs establish the regulatory framework ( <i>Regulations</i> ) within which all service providers must operate.
CTAF	Common Traffic Advisory Frequency
EKRA	East Kimberley Regional Airport
EOC	Emergency Operations Centre
FAA	Federal Aviation Administration (United States Department of Transportation)
General Aviation (GA)	The sector of the aviation industry that does not include regular public transport (RPT) airlines and military aviation.
GPS	Global Positioning System
IATA	International Air Transport Association
ICAO	International Civil Aviation Organisation

IFR/IMC (Instrument Flight Rules/ Instrument Meteorological Conditions)	Refers to rules under which flight involving navigation requiring reference to radio navigational aids or instruments is carried out. Weather conditions below a certain minima are referred to as instrument meteorological conditions (IMC). IFR flight requires pilots to be qualified in the use of instrument navigation and to use radio navigational aids provided at airports.
INM	Integrated Noise Model
IWI	Illuminated Wind Indicator
LIRL	Low Intensity Runway Lighting
LoS	Level of Service – a range of values or assessments of the ability of the terminal to meet demand
MOS	Manual of Standards
MTOW	Maximum Take-off Weight
Navaid	Commonly-used abbreviation for 'radio navigational aid'
NDB (Non Directional Beacon)	A simple and common type of radio navigational aid which allows pilots to track to or from its location.
Non-precision instrument approach	An instrument approach and landing that uses lateral guidance but does not use vertical guidance.
OLS	Obstacle Limitation Surfaces
PAL	Pilot Activated Lighting
Pavement Classification Number (PCN)	A number expressing the bearing strength of a pavement for unrestricted operations by aircraft with ACN value less than or equal to PCN.
Payload	The total weight of passengers and cargo that an aircraft can carry.
PSI	Unit of pressure or stress (pounds per square inch)
RESA (Runway End Safety Area)	Area provided at the end of a runway strip, to protect the aeroplane in the event of undershooting or overrunning the runway.
RFDS	Royal Flying Doctors Service
RNAV/GNSS Approach	Area Navigation/Global Navigation Satellite System Approach. A form of instrument approach procedure using signals from orbiting satellites to determine an aircraft's precise position at a point in time.
RPT (Regular Public Transport)	Air services operated by airlines that are scheduled to occur on a regular basis at fixed times or frequencies and on fixed routes.
Runway Strip	A defined area including the runway and stopway, intended to reduce risk of damage to aircraft running off a runway and to protect aircraft flying over it during take-off or landing operations.
SWEK	Shire of Wyndham East Kimberley
VOR/DME	Combined radio navigation station: VHF omnidirectional radio / Distance-based measuring equipment

VFR/VMC (Visual Flight Rules/  
Visual Meteorological Conditions)

Refers to rules under which flight involving navigation solely by reference to visual cues (rather than requiring reference to radio navigational aids or instruments) is carried out. VFR flight is permissible only when meteorological conditions (cloud base and visibility) are above defined limits. Such conditions are referred to as visual meteorological conditions (VMC). VFR flight does not require pilots to be qualified in the use of instrument navigation, nor does it require expensive radio navigational aids to be provided at airports.

WI

Wind Indicator

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## EXECUTIVE SUMMARY

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### BACKGROUND

REHBEIN Airport Consulting was commissioned by the Shire of Wyndham East Kimberley (SWEK) to prepare an Airport Master Plan for East Kimberley Regional Airport (EKRA). The Master Plan provides a strategic framework for the future development of the airport and sets out the required infrastructure likely to be required to support the forecast demand of passenger and aircraft movements at the airport over the next 10 to 25 years. SWEK identified several specific objectives in commissioning this Master Plan, including the desire to:

- Provide for long-term sustainability;
- Cater for current and future growth;
- Balance between airport functions and surrounding land use;
- Facilitate additional aviation business and commercial development whilst still maintaining long-term control of airport land; and
- Protect land for aviation growth and expansion.

The methodology for completing the Master Plan included stakeholder consultation to solicit the views, issues and concerns of SWEK representatives, airlines, airport users, airport land owners and local businesses and organisations.

### EXISTING FACILITIES

EKRA has a single sealed runway. Runway 12/30 is 1,829 metres long and 30 metres wide and sits within a 150 metre wide graded strip. The runway is therefore suitable for non-precision instrument approach operations by aircraft up to Code 3C. The annual Aerodrome Technical Inspection report undertaken in March 2011 indicates that the runway pavement is reaching the end of its serviceable life. According to EKRA officers, during the wet season serious pavement failures have occurred.

Access between the runway and the RPT apron is provided by Taxiway A which is 15 metres wide and suitable to accommodate Code C jet aircraft. Taxiway B provides access between the Runway 30 threshold and the RPT apron, this taxiway is also suitable to accommodate Code C jet aircraft. Taxiway C provides access across the north side of the RPT and east GA aprons. Although designated as a Taxiway, only Code C taxiway clearances are provided. A part parallel Code B taxiway, designated as Taxiway F, connects the threshold of Runway 12 to Taxiway A. Due to the pavement condition, this taxiway is limited to Code B aircraft up to 5,700kg.

The passenger terminal is served by a sealed RPT apron which accommodates three aircraft parking positions, these can accommodate up to Fokker 100 aircraft. GA aprons are located to the east and west of the RPT apron, these are mainly used for aircraft parking by the based charter operators and the Royal Flying Doctor's Service (RFDS).

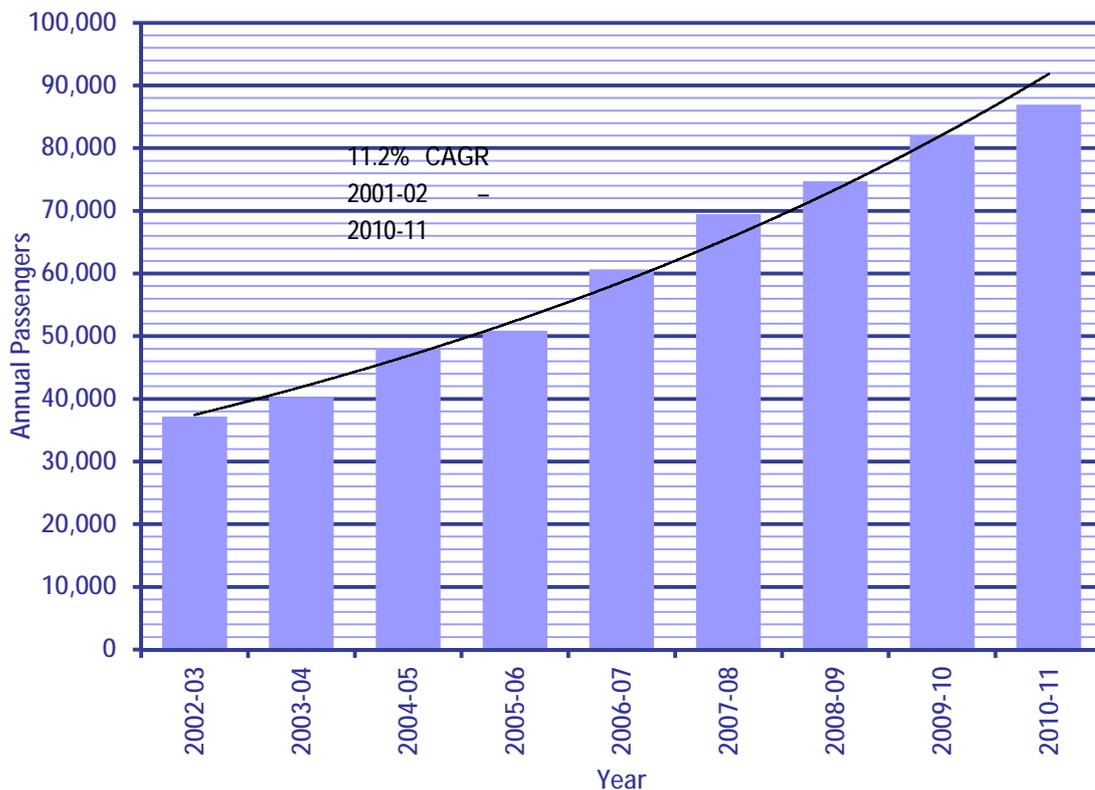
The passenger terminal has recently been redeveloped. The terminal is now a modern, high quality structure with a total footprint of approximately 2,000 m<sup>2</sup> excluding external waiting areas.

EKRA has a Non-Directional Beacon (NDB) and a VOR/DME. Jet A-1 and AVGAS facilities are also accommodated.

## HISTORICAL AVIATION ACTIVITY

Figure I shows RPT passenger numbers during the period 2002-03 to 2010-11. Following the collapse of Ansett Australia in 2002, Airnorth began operations from EKRA. In 2002-03 EKRA saw approximately 37,000 passengers. Since this time, passenger traffic has been growing strongly to almost 87,000 passengers in 2010-11. The compound annual growth rate (CAGR) from 2001-02 to 2010-11 was 11.2%.

Figure I: Historical Passenger Traffic 2002-03 – 2010-11



Source BITRE/SWEK

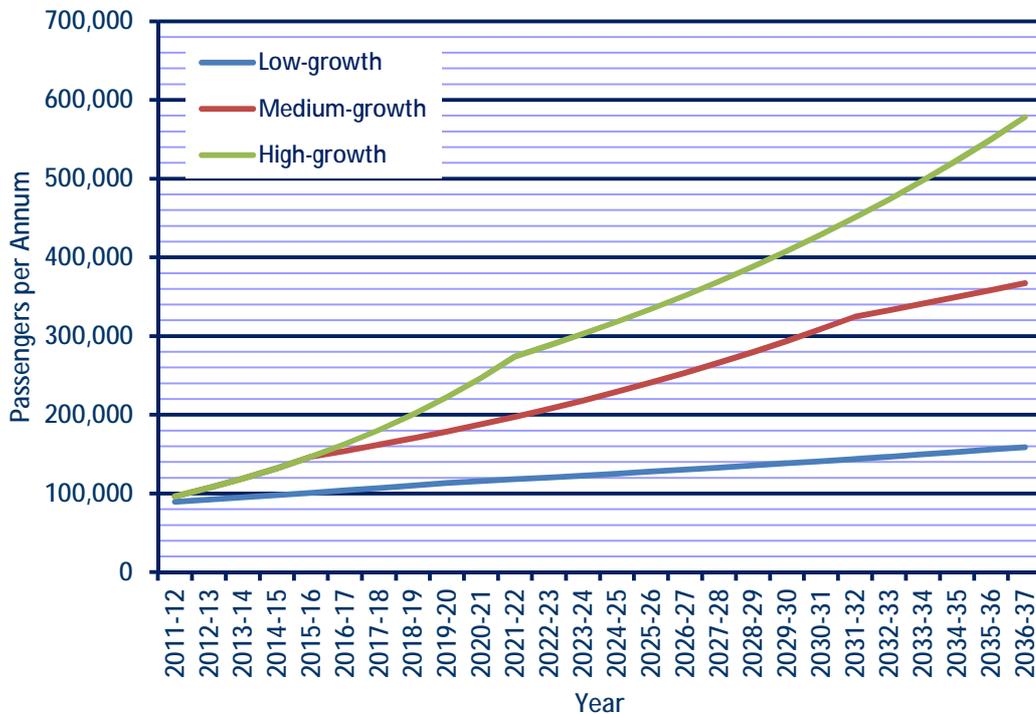
Annual passenger traffic data for the period 1985-86 to 2001-02 is also available. However, due to unknown recording methodologies at the time, this data cannot be directly compared with that for the period 2002-03 to 2010-11. Nevertheless, the CAGR for this period of 5.1% provides a good indication of the general level of growth of RPT passengers at EKRA during this time.

In 2011 there were approximately 26,500 aircraft movements. The compound annual growth in aircraft movement for the period 2002 to 2011 is 1.9%. Approximately 66% of all movements are charter movements, 13% are helicopter movements, 9% are RPT movements and 7% are private movements. The remainder are attributed to medical, training and business-related activities.

## FORECAST AVIATION ACTIVITY

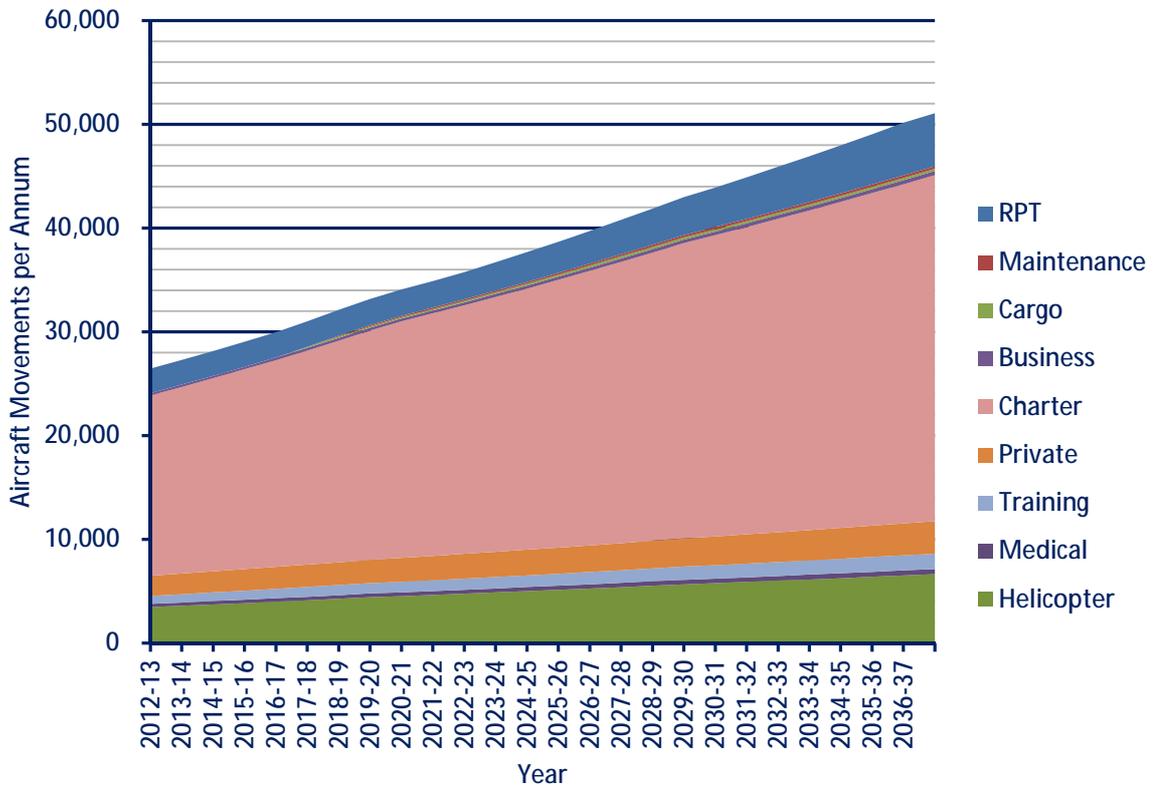
Passenger traffic has been forecast to provide a basis for future infrastructure requirements. High-, medium- and low-growth forecasts were developed based on key regional economic drivers and identified opportunities for additional services and larger aircraft. Figure II summarises the annual passenger forecasts from 2011-12 to 2036-37.

Figure II: Forecast Passenger Traffic 2011-12 to 2036-37



Projections of annual aircraft movements have been developed by segmenting aviation activity into the principal component sectors, each of which having different drivers and prospects for growth at EKRA. Figure III presents the forecast aircraft movements to 2036-37 by segment.

Figure III: Forecast Aircraft Movements 2011-12 to 2036-37



## AERONAUTICAL DEVELOPMENT CONCEPT

### RUNWAY DEVELOPMENT

The development of a longer runway at EKRA, to support future increases in aircraft size and range of destinations, forms a key development item of this Master Plan. The Master Plan safeguards for the development of a runway to support operations by narrow-body jet aircraft such as the A320-200 and Boeing 737-800 to all domestic destinations on RPT services, and possibly to destinations in south-east Asia.

Two runway development options are included within this Master Plan. Both have been included to ensure the required land is safeguarded for these purposes. Either option may be developed depending on the outcome of a number of factors which are as yet unknown.

Runway Option 1 involves the extension, widening and strengthening of the existing runway to 45 metres wide and 2,350 metres long. Depending on the exact condition of the existing pavement and subgrade, it is not clear whether the runway will be required to be closed to undertake the strengthening works. SWEK must keep the runway operational at all times during the day, therefore this option will not be selected if works are required which cannot be performed during overnight closures. Runway Option 1 will require the acquisition of an area of private land to the east of the Runway 30 threshold, as well as an area of Crown land to the north of the runway.

Runway Option 2 involves the development of a new runway, 2,500 metres long and 45 metres wide, located north of, but parallel, to the existing runway at an offset that would allow the runway to be constructed while the existing runway remains operational with only limited restrictions from construction work. Some land acquisition will be required to construct a runway on this alignment east of the existing threshold of Runway 30 and an area of Crown land to the north of the existing runway.

For both runway options a 300 metre wide runway strip is planned for to accommodate future possible Code 4C precision approach operations.

## PASSENGER TERMINAL

The existing passenger terminal building has recently been expanded and upgraded to accommodate the simultaneous operations of up to two Boeing B737-800/Airbus A320-200 aircraft. It is estimated that the current terminal is likely to be able to accommodate potential demand up to around 2021-22 and will require upgrade or expansion when more than two aircraft of this size (or a combination of smaller aircraft), require simultaneous use of the terminal. By 2036/37, between 4,000m<sup>2</sup> and 4,500m<sup>2</sup> of terminal footprint is anticipated to be required.

## AIRCRAFT PARKING

### RPT and Charter

During Stage 1, with the highest traffic envisaged and the most demanding mix of aircraft in terms of apron space, the maximum number of RPT aircraft on the apron simultaneously is likely to be three including 150-160 seat jet aircraft such as the Airbus A320-200/Boeing 737-800. These three parking positions could be accommodated within the existing apron; however, the simultaneous operation of charter aircraft in addition to these RPT operations into EKRA is likely to require the expansion of the existing apron. Therefore an apron expansion of between approximately 2,800m<sup>2</sup> and 5,500m<sup>2</sup> to the west of the existing is anticipated in Stage 1. The existing RPT apron pavement will also require strengthening to accommodate the larger jet aircraft.

By 2036-37, with the highest traffic growth envisaged, the maximum number of aircraft parking positions required by RPT aircraft is four. It is anticipated that RPT apron expansion will continue to the west of the existing apron and will expand onto the eastern edge of the existing west GA apron. Beyond 2036-37, depending on passenger growth and actual aircraft mix, aircraft parking adjacent to the terminal building may not be sufficient and alternative areas for apron expansion will need to be sought. There is potential to develop the area directly to the north of the existing RPT apron to accommodate additional RPT or charter aircraft.

### Private Jet Apron

It is proposed that in Stage 1 a dedicated private jet apron to the west of the RPT apron be developed. One aircraft parking position can be accommodated in this area up to the size of the Bombardier Global Express. Secondary parking positions could also be accommodated on the

apron for up to two smaller jet aircraft. This location would allow the private jet apron to be completely separate from the RPT apron, excluding it from security screening requirements. This location will also allow for convenient landside access. A small area of the west GA apron aircraft parking is likely to be impacted by the development of this apron. When this area is required for RPT apron expansion during Stage 2, private jet parking will need to be relocated to an alternative location, possibly to the north of the existing RPT apron or on the west GA apron.

### East GA Apron

There is little scope for expansion of this apron. To increase the available aircraft parking for the potential future expansion plans of the based charter operators located in this area, parking for visiting GA aircraft will be relocated to the west GA apron when this is expanded. In the event of Runway Option 2 being developed there may be scope to expand the east GA apron on the northern side of Taxiway F.

### West GA Apron

The west GA apron will be expanded to accommodate the relocated visiting aircraft parking as well as any additional GA parking that may be required by new operators that establish themselves within the aviation-related commercial precinct to the south and west of the existing west GA apron. Additional parking will also be included to replace that lost due to the development of a dedicated private jet apron on the eastern edge of the west GA apron. Initially, the existing apron can be expanded to the north towards the weather station. This would approximately double the size of the existing apron to approximately 30,000 m<sup>2</sup>. The apron may also be extended to the northwest of the existing apron along the eastern edge of the proposed aviation-related commercial precinct. This area will be developed in sub-stages according to demand.

## TAXIWAY SYSTEM

The Master Plan proposes the upgrade and extension of the parallel taxiway (Taxiway F) to provide access to a number of airport facilities and precincts. Taxiway F will be extended from Taxiway A to the threshold of Runway 30 to provide Code C taxiway access to the RPT, east GA and west GA aprons. The provision of Taxiway F in this area will provide an alternative to the use of Taxiway C and remove the current issue of GA traffic crossing the RPT apron. Taxiway F, between Taxiway A and Taxiway G, will require significant strengthening and even reconstruction, based on its current poor condition, to accommodate Code C aircraft and provide access to the proposed precincts in this area. Beyond Taxiway G, Taxiway F will ultimately be extended to the threshold of Runway 12 to accommodate Code B aircraft. Clearances for Code C aircraft will however be safeguarded, should this be required in the future.

Taxiway J will be constructed to provide Code B aircraft access to the west GA apron and the aviation-related precinct development in this area.

## OTHER AIRFIELD FACILITIES

### Bureau of Meteorology Weather Station

The Master Plan anticipates that the existing Bureau of Meteorology weather station may be required to be relocated to the north of the runway to make way for an extended west GA apron. However, it is not anticipated that this will be required in the early stages of this Master Plan.

### Visual and Navigational Aids

Runway Option 2 will require the relocation of the Doppler VOR/DME. The Master Plan proposes that this facility be relocated to the north of the runway, within the airport boundary, on land that is currently leased. This location provides sufficient area for all required clearances.

There is no requirement to relocate the Non-Direction Beacon (NDB) from its current location, however the NDB will pose some limitations on the aviation-related commercial development proposed surrounding it. Therefore, there may be some benefit in the future to relocate the NDB to make maximum and most effective use of the land south of the runway available for commercial purposes. The NDB could also be relocated to the north side of the runway.

Both runway options will require the relocation of the wind indicators.

### Fuel Facilities

The existing Shell and Air BP fuel facilities will not be impacted by the developments proposed in this Master Plan. However, an additional fuel facility may be required to be developed on the west GA apron, providing AVGAS and Jet A1 to the GA aircraft located in this area. This will reduce the requirement for aircraft to taxi between the two GA aprons to access fuel.

### Airside Access

The existing airside access gates will remain largely unchanged. With the relocation of the visitor GA parking to the west GA apron and the private jet apron also located in the vicinity, airside access for visiting GA and business jet aircraft pilots and passengers will be via or adjacent to the existing automatic airside access gate to the west of the passenger terminal.

## AIRCRAFT NOISE

The consideration of the impact of aircraft noise is an important factor in the development of Airport Master Plans. An Australian Noise Exposure Forecast (ANEF) has been prepared for the 25 year planning horizon of this Master Plan, 2036/37. An ANEF contour map has been produced, and has been submitted to Airservices Australia for technical endorsement, which should be incorporated into SWEK's local planning scheme to ensure compatible development is proposed in the vicinity of the airport and future airport operations are protected.

The ANEF system is the basis of *Australian Standard AS 2021-2000 Acoustics – Aircraft noise intrusion – Building siting and construction (AS2021-2000)*. AS2021-2000 classifies the construction of new residential developments between 20 and 25 ANEF as conditionally

acceptable, however some people may find that this land is not compatible with residential or educational uses. The 25 ANEF contour for EKRA extends beyond the airport boundary to the southeast over land that is proposed to be acquired for airport use, as well to the north over uninhabited agricultural land. It also extends beyond the airport boundary to the northwest over the Ord River.

To assist the understanding of the community of the noise impact of the airport, 'Number above', or 'N' contours can also be developed. N60 and N70 contours have been produced as part of this Master Plan. The N60 contour map shows that the area immediately surrounding EKRA, is expected to experience 5 events of 60 Db(A) or greater during an average night (19:00 – 07:00). This area is largely unpopulated agricultural land. The N70 contour map shows that areas outside of the airport boundary are expected to experience up to 20 events over 70 Db(A) on an average day (over 24 hours). The majority of this land is undeveloped and uninhabited with the exception of an area of the western side of Kununurra town centre which is largely commercial development.

## NON-AERONAUTICAL DEVELOPMENT CONCEPT

SWEK should consider carefully how the available land will be made available to developers. Council has the option to lease or sell freehold the individual sites. It should be considered that the lease of sites within the airport boundary is generally favourable on the basis that SWEK maintains long-term control of the land. Several sites held on freehold purchase basis may restrict future plans for the adjacent sites and could reduce the flexibility of the surrounding land. This has occurred, to a certain extent, in the east GA area, where there is no longer scope within this Master Plan to utilise this area for key aeronautical facilities such as the expansion of the RPT apron. Through projects at other regional airports, REHBEIN Airport Consulting has also witnessed regional council's difficulties in developing airports based on past decisions to sell land on a freehold basis.

Through stakeholder consultation, a range of aviation-related activities were identified as having potential for development at EKRA. These activities included private hangar storage, light aircraft maintenance, heavy aircraft maintenance, air cargo, support businesses for the mining or oil and gas industries and hotel accommodation, among others. Five potential land-use development types were identified and a number of precincts were located on the airport site for commercial development for these purposes.

The development of these land uses will require the significant upgrade of engineering services. The land use types are outlined below.

### LAND-USE TYPE 1: AVIATION-RELATED COMMERCIAL DEVELOPMENT WITH AIRSIDE ACCESS

Land-use type 1 is proposed to accommodate aviation-related businesses that require airside access and therefore operate their own aircraft or have customers who require access via aircraft, such as light aircraft maintenance.

## LAND-USE TYPE 2: AVIATION-RELATED COMMERCIAL DEVELOPMENT WITHOUT AIRSIDE ACCESS

Land-use type 2 is proposed to accommodate aviation-related businesses that do not require airside access. A variety of potential business types have an aviation connection and will benefit from a location at the airport. These businesses will either support the aeronautical activities at the airport; support the aviation-related businesses that own, operate or deal with aircraft; or gain some benefit from the proximity to the air services offered at the airport to transport people, equipment or component parts being manufactured or maintained.

## LAND-USE TYPE 3: AIRCRAFT MAINTENANCE/ENGINEERING

Stakeholder consultation has indicated that there is potential demand for a heavy aircraft maintenance facility at Kununurra. Land-use type 3 would provide a dedicated area for this purpose with room for future expansion.

## LAND-USE TYPE 4: AIR CARGO/FREIGHT

Land-use Type 4 is proposed to accommodate potential air cargo activities that may develop at the airport in the future.

## LAND-USE TYPE 5: PRIVATE HANGARS

Precinct 5 is proposed to provide private hangar accommodation for private aviators wishing to store their aircraft at the airport.

## DEPOT

The Master Plan proposes that, if required, the town depot can be located in the south east corner of the existing airport site.

## LANDSIDE ACCESS

A detailed review of the existing airport access road markings is required to ensure that landside access is efficient and not confusing to the driver. The Master Plan proposes that both main access points from the Victoria Highway are maintained for access to the passenger terminal building. Alternatively, access points from the highway could be consolidated through the development of a roundabout at the entrance to the airport. This should be subject to a more specific traffic study to be undertaken by SWEK.

To reduce the crossover of traffic at the passenger terminal forecourt, all non-passenger terminal related traffic should use alternative accesses including Dusty Ranking Drive and Cyril Kleinig Drive. A new access from the highway, to the west of the existing access points (the existing speedway access), will be required to be upgraded/constructed to provide vehicular access to the private hangar precinct. Road access will also need to be developed to access the area to the north of the runway which may be used to accommodate the relocated VOR/DME, NDB and weather station as well as aviation-related commercial development.

The Master Plan sets out the anticipated car parking development required to support the forecast growth in traffic. Parking restrictions are proposed for short-stay parking, along with the development of secure and non-secure long-stay parking. It is proposed that a fee charged for the use of the long-stay car parks.

## STAGED DEVELOPMENT PLAN & INDICATIVE DEVELOPMENT COSTS

Development staging of the proposed infrastructure development is subject to a range of external factors as well as demand. The Master Plan, whilst setting out the optimum long-term land-use arrangement for the airport site, incorporates flexibility to adjust the location and timing of particular developments as necessary to suit specific constraints. The Master Plan has set out the proposed airport development over the next 25 years in two stages, Stage 1, the next 10 years (to 2021/22), and Stage 2, to 2036/37. Appropriate trigger points have also been identified for each of the developments.

## IMPLEMENTATION PLAN

In order to commence the implementation of this Master Plan it is important to understand that each infrastructure element needs to go through a development process including stages of planning, design and procurement.

An initial stage of planning is required, part of which is formed by this Master Plan which has highlighted the need for specific infrastructure developments. This should then be followed by a feasibility/planning study for each infrastructure development to undertake more detailed planning required prior to design.

Based on the aeronautical and non-aeronautical infrastructure development proposals set out by this Master Plan for Stage 1 (to 2021-22), Table II highlights the infrastructure developments that are considered to be most pertinent and should immediately continue through the infrastructure development process and more detailed planning/feasibility studies undertaken.

Table II: EKRA 2012/13 Planning Studies

Infrastructure Development	Planning Study Elements
Runway development to Code 4 capability	Existing runway pavements study followed by a detailed planning study for the selected runway option
Taxiway F Extension	Taxiway F extension planning study
West GA Area Development	West GA expansion and reorganisation to accommodate relocated GA visitor parking, private jet apron and to include the initial stages of development of Precinct 1A and 2A
Car Parking and Road Access	Secure and unsecure long-stay parking, implementation of short-stay parking restrictions as well as review of road access arrangements.

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## 1.0 INTRODUCTION

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REHBEIN Airport Consulting was commissioned by the Shire of Wyndham East Kimberley (SWEK) to prepare an Airport Master Plan for East Kimberley Regional Airport (EKRA).

### 1.1 OBJECTIVES

Airport master planning is undertaken to enable sound land use development. An Airport Master Plan is a key strategic tool that sets out a long-term framework for the development of all facilities within the airport and protects future development against the effects of current decisions.

Consistent with these strategic considerations, the *Airports Act 1996* summarises the aims of an Airport Master Plan as follows:

- Establishing strategic direction for the efficient and economic development of the airport over the planning period;
- Providing for the development of additional uses of the airport site;
- Indicating to the public the intended uses of the airport site; and
- Reducing potential conflicts between uses of the airport site, and to ensure that uses of the airport site are compatible with the areas surrounding the airport.

Although the *Airports Act 1996* does not have statutory application to EKRA, this does not reduce the relevance of these four key aims. SWEK has identified several further specific objectives in commissioning this Airport Master Plan, including the desire to:

- Provide for long-term sustainability;
- Cater for current and future growth;
- Balance between airport functions and surrounding land use;
- Facilitate additional aviation business and commercial development whilst still maintaining long-term control of airport land; and
- Protect land for aviation growth and expansion.

The Master Plan provides a focussed framework for the development of the airport over the next ten years. However, to ensure that the development of the airport within this timeframe is sustainable and future development is not limited by development decisions, the Master Plan looks strategically beyond ten years, as far as 25 years, to ensure that anticipated long-term growth at the airport is safeguarded. At this stage SWEK considers the current airport site to be the appropriate long-term location of the airport. However, if in the future more valuable alternative uses are identified for the existing airport land in the long term, future Master Plans should consider the potential relocation of the airport to an alternate site.

The EKRA Master Plan must realistically represent future facilities that will satisfy projected air traffic demand and potential economic growth opportunities and must also ensure compatibility with user needs and regulatory obligations including safety and security.

Whilst this Master Plan sets out strategic infrastructure requirements for EKRA over the next 10 to 25 years, it is important to recognise that the Master Plan makes no assertion as to the commercial viability of any individual component. Suitable trigger points and anticipated timings for each element are identified; however each development should be subject to its own business case prior to any decision to proceed with capital expenditure.

## 1.2 METHODOLOGY

The principal steps in its preparation of this Master Plan were as follows:

- Stakeholder Consultation was undertaken to solicit the views, issues and concerns of key stakeholders including SWEK representatives, airlines, airport tenants and users, local businesses and organisations. Discussion was largely focussed on the future infrastructure requirements and expansion potential of EKRA. The potential for development of all types on airport land was also discussed. Consultation was undertaken in Kununurra as well as by telephone. A visit to Kununurra was made by REHBEIN Airport Consulting personnel in January 2012. **Appendix A** provides a stakeholder consultation schedule;
- Based on consultation with these key stakeholders and consideration of relevant market trends, future aviation activity forecasts were developed. Low, medium and high growth scenarios were developed and various possible aircraft schedules were developed exhibiting a range of aircraft types and frequencies for each scenario. This approach maximises the degree of flexibility offered by the development proposals included within this Master Plan. More detail on the forecasting and facilities analysis is provided in **Sections 4.0 and 5.0**;
- Based on selected future scenarios, aeronautical infrastructure development proposals were set out and a staged development plan formulated to provide guidance on the implementation of the proposals;
- Once the requirements for aeronautical infrastructure and the required supporting services were confirmed, proposals for non-aeronautical development at the airport were developed. These were also based on the results of the stakeholder consultation, desktop research of the local economy and experience at other airports;
- Noise and airspace assessments have also been undertaken to provide direction for land use planning on the land surrounding the airport. ANEF (Australian Noise Exposure Forecast) contours have been developed for EKRA to guide future development surrounding the airport. N60 and N70 contours have also been developed to assist the community to better understand the impacts of aircraft noise. Future Obstacle Limitation

Surfaces (OLS) were also developed which will also help define land use and specific development surrounding the airport; and

- Indicative capital cost estimates have also been developed to aid with the implementation of both the aeronautical and non-aeronautical proposals within this Master Plan.

### 1.3 REPORT STRUCTURE

This Master Plan report is structured as follows:

- Section 2.0 describes the master planning context;
- Section 3.0 describes the existing situation with respect to airport facilities;
- Section 4.0 discusses historical and forecast future aviation activity;
- Section 5.0 outlines the aeronautical development concept;
- Section 6.0 describes the existing and future airspace;
- Section 7.0 considers aircraft noise;
- Section 8.0 outlines the non-aeronautical development concept including commercial development, landside access and car parking;
- Section 9.0 provides indicative capital cost estimates for the proposals set out within this document; and
- Section 10.0 provides an implementation plan for the proposals set out within this Master Plan.

## 2.0 MASTER PLANNING CONTEXT

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### 2.1 LOCATION

East Kimberley Regional Airport (EKRA) is located in the town of Kununurra within the Shire of Wyndham East Kimberley, Western Australia. Kununurra is situated on the Victoria Highway approximately 830 kilometres by road south-west of Darwin and approximately 3,200 kilometres by road north-east of Perth. Broome lies approximately 1,040 kilometres by road to the west of Kununurra.

Drawing B11337A001 at Appendix B shows a location plan of Kununurra and EKRA.

### 2.2 POPULATION

In 2011, SWEK was estimated to have a resident population of approximately 8,100<sup>1</sup>. The region has a relatively young median age of 30 years in comparison to the rest of Western Australia. It is estimated that indigenous people make up approximately half of the overall population.

In the 2006 census the town of Kununurra itself was recorded to have a population of 3,747<sup>2</sup> (place of usual residence), however, 5,618<sup>2</sup> people were recorded to be in Kununurra at the time. This accounts for the large transient population within Kununurra including tourists and short-term contract workers. This indicates that during the dry season (the busiest tourist period), approximately 39% of the Kununurra population are visitors. In 2006, it was also estimated that approximately 25%<sup>2</sup> of the visitor population were in Kununurra for work purposes.

In 2005, the Western Australian Planning Commission forecast a population annual average growth rate for SWEK itself of 2.3% to 2013, 2.1% for the period 2014 to 2018 and 1.9% for the period 2019 to 2028<sup>2</sup>. Overall, it is expected that the Kimberley region as a whole will be one of Australia's fastest growing regions with projected population forecast to grow by an average rate of between 2.4% and 2.9% to 2020<sup>2</sup>.

### 2.3 ECONOMY

The economy of the area is dominated by the agricultural, resource, tourism and human services industries.

#### 2.3.1 AGRICULTURE

The Ord-East Kimberley Expansion Project includes the release of 8,000 hectares of agricultural land, along with improvements to social, community and common-use infrastructure. The Development Package is being funded through a collaborative partnership between Australian and

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<sup>1</sup> Australian Bureau of Statistics

<sup>2</sup> Kununurra Regional HotSpots Land Supply Update, November 2008, Western Australian Planning Commission

Western Australian Governments, involving a total investment of \$415 million between 2009 and 2012 through the construction of the infrastructure<sup>3</sup>.

The project area is located directly north of Kununurra. The land will be released and available for purchase to the national and international market. Current indications are that significant investment is likely to come from China with the most likely use for the land being to grow bulk commodity crops.

This expansion project is building upon the original Ord project which saw the construction of the Ord dam south of Kununurra to provide irrigation to allow land to be cultivated largely for growing commodity crops including sugar cane and intensive horticulture crops such as melons and pumpkins, as well as bananas and mangoes.

### 2.3.2 TOURISM

The East Kimberley region has a strong tourism industry and Kununurra forms a gateway and base for anyone wishing to access the region. The region exhibits a range of tourist attractions and activities which could largely be categorised within the 'soft' adventure tourism market.

The local natural landscape itself attracts many tourists including Lake Argyle, Purnululu National Park and Mitchell River National Park, among several other national parks in the area. Scenic aerial tours around the area, including many over the Bungle Bungles within Purnululu National Park, are operated from EKRA. The region is also a popular location for barramundi fishing and the local diamond industry also provides an attraction for tourists. In addition, the region boasts a number of remote stations that offer accommodation many of which provide luxury accommodation that attract high end tourist trips. The popular El Questro Wilderness Park is located close to Kununurra.

Much of the tourism activity within the area occurs during the dry season, with far fewer tourists during the quieter wet season. There is a desire within SWEK to increase tourism numbers during the wet season in the future. The East Kimberley region also wishes to align itself more with Darwin and tourism activities within the Northern Territory and promote itself as part of the 'Top End' tourism market as well as that in Western Australia.

Total visitor nights in Western Australia overall are forecast to have a compound annual growth rate of approximately 1.8% to 2020<sup>4</sup>. No specific forecast data is available for the East Kimberley itself, however, anecdotal evidence suggests that there is a large untapped tourism market within the region and tourism is therefore expected to grow at a higher rate than that forecast for the rest of the state.

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<sup>3</sup> Department of Regional Development and Lands, Government of Western Australia

<sup>4</sup> Tourism Forecasting Committee, Regional Forecast Tables, Forecast 2011 Issue 2

### 2.3.3 RESOURCES

Mining also provides a significant contribution to the economy of the area. The Argyle Diamond mine operated by Rio Tinto is located 110 kilometres south of Kununurra. The Argyle open pit mine was commissioned in 1985. The Argyle Diamond Mine operates 24 hours per day, 365 days per year. In 2005, the construction of an underground diamond mine at Argyle commenced, however in 2009 its development was slowed as a result of global market conditions. The completion of the underground mine is now expected in 2013, extending the life of the mine to approximately 2019.

Kimberley Metals Group (KMG) operates the Ridges Iron Ore Project which is located approximately 150 kilometres southwest of Kununurra adjacent to the Great Northern Highway. In excess of 67 million tonnes of combination magnetite and hematite has been identified for extraction. The project has a confirmed mine life of four years with potential for increased production through future exploration. KMG commenced shipping iron ore in July 2011 and is currently exporting in excess of 100,000 tonnes of iron ore to China. KMG try to recruit locally but employees are generally sourced from Darwin and Broome with other specialists travelling from the eastern states.

Other mining projects in the area include the Sorby Hills Lead-Silver Project, operated by KBL Mining Limited, which is located approximately 50 kilometres by road from Kununurra. It is currently at the pre-feasibility study stage but commencement of mining is anticipated from 2013. KML also operates, through a joint venture, the Manbarrum zinc-lead-silver project to the east of Sorby Hills over the border in the Northern Territory. Panoramic Resources operates the Savannah Nickel Project located 240 kilometres south of Kununurra which includes underground mining and a process plant.

## 2.4 EAST KIMBERLEY REGIONAL AIRPORT

The EKRA is owned and operated by the Shire of Wyndham East Kimberley. Regular Passenger Transport (RTP) services to and from the airport are operated by Airnorth and Skywest. Airnorth operate direct flights to Darwin and Broome throughout the year as well as direct flights to Perth on a seasonal basis. Airnorth also operate a charter service between Kununurra and Argyle on behalf of Rio Tinto. Skywest operate direct flights to Broome with onward connections to Perth throughout the year. They also offer direct flights to Darwin and Perth on a seasonal basis.

The EKRA is located approximately four kilometres (by road) west of the centre of Kununurra town centre. The airport is located on an area of land of approximately 275 hectares. Of this, SWEK owns 261.5 hectares and the remainder is held on a freehold or lease basis by other parties. The airport land is bordered to the south by the Victoria Highway reserve and a golf course. The remainder of the surrounding land is generally used for agricultural purposes, some of this is privately owned and some is held by the State of WA. The Ord River borders the airport to the south and west.

Access to the airport is via the Victoria Highway from Kununurra town centre to the east and other local centres such as Wyndham to the west.

Further details of the existing airport activities and facilities are described in Section 3.0.

## 2.5 PLANNING INTEGRATION

This Master Plan has been developed with due regard to the existing relevant planning initiatives and strategies for the region and town of Kununurra.

SWEK falls within Planning WA's Kimberley region. The Kimberley Planning Framework sets out the strategic direction for the future development of the region over the next 25 years. This framework is currently under development.

The Kununurra-Wyndham Area Development Strategy developed by Planning WA provides a planning framework to coordinate and promote planning and development. It also incorporates a structure plan for Kununurra.

The Kununurra Strategic Directions: Town Centre Development Concept Plan & Strategic Land Use Plan, July 2010, provides a defined vision for the Kununurra town centre and broader area, and identifies a structure to guide future investment and development in the area. The Strategic Directions plan encompasses the airport site and the land surrounding.

The Local Planning Strategy (LPS) is the principal guiding framework for the direction for the development of SWEK. The LPS sets out the SWEK's strategies and actions for transport, community development, economic development and governance. The LPS identifies the need for the development of a Master Plan for the airport. EKRA falls within the town planning scheme for Kununurra, namely, the Shire of Wyndham-East Kimberley Town Planning Scheme No 7 Kununurra and Environs.

The documents mentioned above not only provide inputs into this study, but equally this Master Plan feeds back into future planning decisions particularly with regard to the outcomes of the noise and airspace assessment of the proposals.

## 2.6 KEY CONSULTATION FEEDBACK

Feedback obtained during the stakeholder consultation included a wide range of issues which provided a valuable background to the EKRA existing situation and future aspirations. The key themes relevant to the development of this Master Plan are summarised as follows:

- Existing runway pavement strength;
- Runway length to accommodate larger jet aircraft;
- Car parking and access issues;
- Future terminal capacity;
- Additional parking for General Aviation (GA) aircraft;

- Private jet parking;
- Development of airport land for aviation-related uses;
- Additional hangar space; and
- Additional lots for expansion of existing businesses at the airport.

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## 3.0 EXISTING FACILITIES AND ACTIVITIES

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The following sections provide a brief description of the main infrastructure components and activities at East Kimberley Regional Airport. Drawing B11337A002 at Appendix B shows the existing airport infrastructure and land uses.

### 3.1 AIRFIELD FACILITIES

#### 3.1.1 RUNWAY 12/30

Runway 12/30 is 1,829 metres long and 30 metres wide and is provided with 3 metre wide shoulders. The runway strip associated with Runway 12/30 has a width of 150 metres and a length of approximately 1,949 metres. The full 150m width is graded. The runway is therefore suitable for non-precision instrument approach operations by aircraft up to Code 3C.

The runway has a sealed surface and a published Pavement Classification Number (PCN) of 40/F/C/1200(174 PSI)/T. The published strength rating is believed to be derived from analysis based on testing that was undertaken during the dry season. Airport operations officers have indicated that during the wet season following periods of heavy rain, the runway pavement has very different characteristics and a lower sub-grade strength.

The annual Aerodrome Technical Inspection report undertaken in March 2011 indicates that the runway pavement is reaching the end of its serviceable life. The runway was originally built more than 40 years ago and shows signs of fatigue. In late 2011, SWEK undertook runway maintenance through the application of stone and an enrichment seal to the central 15 metres of the runway. According to the latest Aerodrome Technical Inspection, undertaken in March 2012, although the new seal is in good condition the pavement remains badly deformed and is below the expected smoothness of a runway surface in accordance with Chapter 10 MOS Part 139.

According to EKRA officers, during the wet season serious pavement failures have occurred.

#### 3.1.2 TAXIWAYS

##### Taxiway A

Taxiway A provides the main access between the apron area and Runway 12/30. It has a sealed surface and is 15 metres wide with shoulders of 3 metres. This taxiway is suitable to accommodate Code C jet aircraft with a wheelbase of less than 18 metres. Taxiway A is lit.

This taxiway was reconstructed in 2008 and the Aerodrome Technical Inspection, March 2012, indicates that the pavement is in good condition with only minor deformation. However, the seal is also in good condition.

## Taxiway B

Taxiway B provides access between the threshold of Runway 30 and the apron area. It has a sealed surface and is 15 metres wide with 3 metre wide shoulders. This taxiway is also suitable to accommodate Code C jet aircraft with a wheelbase of less than 18 metres.

According to the Aerodrome Technical Inspection, March 2012, there is some minor deformation and minor rutting on the wheel tracks. The seal is in good condition and the stone is held in well.

## Taxiway C

Taxiway C provides access across the north side of the RPT and east GA aprons. Although designated as a Taxiway, only Code C taxiway clearances are provided. According to the Aerodrome Technical Inspection, March 2012, the pavement is in good condition.

Taxiway C follows the alignment of the old runway which has been disused for many years. An area of the disused runway to the east of the eastern GA apron and north of the fuel facilities is currently used as an engine run-up bay.

## Taxiways D & E

Taxiways D and E provide Code B and Code A access respectively, from Taxiway C onto the east GA apron. Although designated as taxiways, only taxiway clearances are provided. According to the Aerodrome Technical Inspection, March 2011, both taxiways are in good condition and fit for service.

## Taxiway F

Taxiway F is a parallel taxiway, providing access from the threshold of Runway 12 to Taxiway A. The taxiway is 10.5 metres wide with 3 metre shoulders. The section between Taxiway A and Taxiway G is in reasonable condition with only minor deformation. However, beyond Taxiway G, to the west, the wet ground conditions have caused the taxiway to deform resulting in an undulating surface. As a result of the ground conditions the taxiway is limited to Code B aircraft below 5,700kg.

## Taxiway G

Taxiway G provides access between Taxiway F and the runway. It is 10.5 metres wide with shoulders of 3 metres. This taxiway is also limited for use by Code B aircraft below 5,700kg. Two engine-run-up bays/passing bays, incorporating a compass swing bay, are located adjacent to the intersection of Taxiway F and Taxiway G. According to the Aerodrome Technical Inspection, March 2012, the seal in the vicinity of the same intersection is in poor condition with large areas of deformation causing water to pond.

### 3.1.3 APRONS

#### RPT Apron

The RPT apron is located adjacent to the passenger terminal building. The sealed apron is approximately 75 metres deep by 165 metres wide. It accommodates three aircraft parking bays, two of which can accommodate up to a Fokker 100 aircraft and one that accommodates up to an Embraer E170 aircraft. According to the 2012 Aerodrome Technical Inspection, the apron is generally in a good condition but would benefit from an enrichment seal.

#### East GA Apron

The east GA apron is sealed and is located to the east of the passenger terminal building. The apron was resealed in 2008 and is in good condition. Tie down parking is provided on this apron which is utilised by Shoal Air and Alligator Airways whose hangars front on to the apron. The two parking areas largely utilised by Shoal Air and Alligator Airways are both approximately 110 metres long by 10 metres deep and can accommodate a total of approximately 22 Code A aircraft. There is also a dedicated tie down parking area for visiting aircraft. The visiting aircraft parking area is approximately 100 metres long by 20 metres deep and can accommodate approximately 16 Code A aircraft. Apron taxilanes suitable for Code A aircraft provide access to these parking areas.

There is also an area of leased parking bays with tie down on the western edge of the eastern GA apron which is approximately 65m long by 20 metres deep. The northern most bay provides informal parking for any large visiting aircraft, such as private jets that cannot be accommodated or do not wish to park on the RPT apron. The remainder of this area can accommodate 3 to 4 Code A aircraft.

The eastern end of the east GA apron provides access to the Shell and Air BP fuelling facilities. Air BP has a 24 hour AVGAS dispenser on the eastern edge of the east GA area.

A parking position is also provided at the eastern end of this GA apron for a Pilatus PC-12 aircraft operated by the Royal Flying Doctor Service (RFDS), adjacent to the St John's patient transfer facility.

#### West GA Apron

The GA apron to the west of the passenger terminal building mainly provides aircraft parking for Slingair Heliwork whose hangars front on to the apron. The apron surface was resealed in 2008 and is in good condition.

The apron has two areas for aircraft parking, the largest being approximately 200 metres long by 10 metres deep and the other 150 metres wide by 9 metres deep accommodating a total of 21 Code A aircraft.

Apron taxilanes suitable for Code A aircraft provide access to these parking areas.

### 3.1.4 VISUAL AND NAVIGATIONAL AIDS

#### Markers, markings, signals and signs

The Runway 12/30 graded strip is marked with standard white gable markers in accordance with CASA requirements. Compliant pavement markings are provided and in good condition.

5,700kg aircraft weight limit markings are provided on Taxiway F, Taxiway G and the east GA apron.

Unserviceability markers are provided on the remainder of the disused runway to indicate that this runway is no longer serviceable and the area should not be used by aircraft.

There is one (primary) illuminated wind direction indicator (WDI) with a signal area located between the threshold of Runway 30 and the RPT apron. There is also a secondary illuminated wind direction indicator (WI) near the threshold of Runway 12.

#### Lighting

Runway 12/30 is equipped with Low Intensity Runway Lighting (LIRL) which is pilot activated (PAL). Standby power is available. Both runway ends are equipped with the visual approach slope indicator system T-VASIS.

Taxiway A is also lit with blue taxiway edge lights.

The RPT apron is floodlit. This lighting is considered insufficient for current operations by SWEK personnel.

#### Radio Navigational Aids

The airport has a non-directional beacon (NDB), located within the airport site, south of the runway and west of the main terminal area.

EKRA also has a Doppler VOR/DME radio navigational aid located on the north eastern side of the runway.

Both the NDB and VOR/DME are owned and operated by Airservices Australia.

### 3.1.5 AIR TRAFFIC MANAGEMENT

EKRA is an uncontrolled airport which operates without an air traffic control tower.

The established Common Traffic Area Frequency (CTAF) – Aerodrome Frequency Response Unit (ARFU) for the surrounding airspace requires all aircraft in the vicinity to broadcast their intentions over 127.0 MHz. Vehicles, machinery and taxiing aircraft should also broadcast their intentions whilst operating on movement areas.

### 3.1.6 FUELLING FACILITIES

Both Shell and Air BP have fuel installations at the airport. Both are located to the eastern side of the eastern GA apron. Air BP has recently developed a new fuel facility adjacent to the Shell fuel facility. Air BP provides a 24 hour Avgas dispensing facility as well as Jet A-1 via fuel trucks. Shell also provides AVGAS as well as Jet A-1.

There is a decommissioned Mobil fuel compound located landside, to the south of the Slingair Heliwork hangars. Fuel pipes still connect the decommissioned compound to the airside area, these are proposed to be filled with cement.

### 3.1.7 GSE STORAGE SHED

To the west of the passenger terminal is a shed used to store ground servicing equipment (GSE) as well as airline flight catering supplies.

## 3.2 PASSENGER TERMINAL

The existing passenger terminal building has recently been expanded and upgraded. The new terminal was opened in December 2011 and is designed to accommodate the simultaneous operations of up to two Boeing B737-800/Airbus A320-200 aircraft. It has a total of 1,680m<sup>2</sup> of internal areas and a footprint of 2,480m<sup>2</sup> including external areas. The current facility therefore provides more than adequate capacity for the existing Fokker 100/Embraer E170 operations.

The facilities within the passenger terminal include:

- A check-in hall of approximately 200m<sup>2</sup>, located to the western end of the terminal with 6 check-in desks and one service desk. Each check-in desk has a single-stage injector linked to a baggage conveyor. An area is demarcated for the check-in queue;
- An outbound make-up area, including an enclosed room of approximately 100m<sup>2</sup> accommodating the checked baggage screening (CBS) machine and explosive trace detection (ETD) machine, and a covered baggage make-up area of approximately 270m<sup>2</sup> with an automated conveyance system;
- A covered baggage make-up area of 270m<sup>2</sup> with an automated baggage conveyance system;
- Offices including the ground handling agent (Northern Airport Services), airport manager, meeting room and two further offices totalling approximately 200m<sup>2</sup> ;
- A landside waiting area of approximately 300m<sup>2</sup> including a café and retail area plus an external patio area of approximately 45m<sup>2</sup>;
- Five car hire desks currently occupied by Budget, Hertz, Thrifty and Avis with one currently unoccupied.
- Male and female toilets including shower facilities and a baby change room. Separate toilet facilities are provided for staff;

- A passenger security screening area of approximately 70m<sup>2</sup> including search room;
- A departure lounge of approximately 220m<sup>2</sup> incorporating a private lounge of approximately 30 m<sup>2</sup> plus male and female toilets;
- A baggage reclaim hall of approximately 250m<sup>2</sup> including a baggage reclaim conveyor/carousel of approximately 20 metres in length. A corridor intended for use by international passengers in the future but used as a store room in the interim is also included in this area; and
- An external baggage break-down delivery area of approximately 90m<sup>2</sup>.

### 3.3 GENERAL AVIATION USERS

#### 3.3.1 ALLIGATOR AIRWAYS

Alligator Airways occupies three lots of land within the eastern GA area. Lots 304 and 305 are owned by Rob Kendrick and occupied by hangars and workshops. Lot 307 is owned by Morton and is currently undeveloped and unused.

Alligator Airways offers scenic flights and air tours of the remote attractions in the area including the Bungle Bungles, Ord River, Lake Argyle, Mitchell Falls and King George Falls among others. They also provide passenger charter services, air freight. Alligator Airways have approximately 16 aircraft based at EKRA, with a mix of single engine and twin engine piston aircraft. These aircraft are parked on the eastern GA apron. It was advised that during the busy dry season there is often insufficient aircraft parking particularly when there are several visiting aircraft.

#### 3.3.2 SHOAL AIR

Shoal Air occupies two lots in the eastern GA area. Lot 308 is owned by K. Girardin and S. Irvine and accommodates a hangar, workshop and offices. Lot 306 is owned by S. Irvine and is currently undeveloped and unused.

Shoal Air provides passenger charters largely to remote aboriginal communities, scenic tours to the Bungle Bungles and other remote attractions in the area, air freight and remote air logistics, aerial surveys, aerial photography and search and rescue. Shoal Air have approximately 10 aircraft based at EKRA with a mix of single engine and twin engine piston aircraft. These aircraft are also parked on the eastern GA apron which can at times be congested.

#### 3.3.3 SLINGAIR HELIWORK

Slingair Heliwork WA is a charter aviation company and is part of the Curry Kenny Aviation group. Slingair Heliwork occupies lots 319 and 320 in the western GA area of the airport and is the main user of the western GA apron. Lot 319 is owned by Sligair Pty Ltd and Lot 320 is owned by Curry Kenny Promotions Pty Ltd. It offers charters and scenic flights operated by both fixed-wing and helicopter services. EKRA is its base but it also operates from off-site locations during the peak tourist season.

Slingair Heliwork provides scenic air tours, charters, air safaris, helicopter flights and ground touring to destinations including the Bungle Bungles, Mitchell Falls, Broome, Lake Argyle, the Kimberley Coast and its surrounds. They also provide helicopter charter services that include passenger charter, mining company support, sling work, fire fighting, emergency extraction, search and rescue, aerial geophysical work, low level survey, photography, scenic flights and tailor made itineraries. The company are also contracted to provide air services for mining companies, RFDS, the Australian Maritime Safety Authority, the Western Australia Department of the Attorney General, Department of Environment and Conservation (DEC) among others.

Slingair Heliwork has a fleet of 23 aircraft including a mix of single and twin engine aeroplanes plus 16 helicopters including Bell Longrangers, Bell 206B Jetrangers and Robinson R44s.

### 3.4 LANDSIDE ACCESS

#### Road Access

The main airport access is from the Victoria Highway via Laine Jones Drive. Laine Jones Drive provides a one-way loop via the passenger terminal and car parks and provides access back onto the Victoria Highway. Due to several changes to the access arrangements to cater for the upgrade of the terminal, access markings and signs can cause confusion. Therefore, the access arrangements require some consideration and rearrangement to suit the final layout which has been achieved following the completion of the upgraded passenger terminal.

Laine Jones Drive also extends to the west providing access to the developments in the western GA area and other facilities to the west.

Cyril Kleinig Drive provides direct access from the Victoria Highway to the Western GA area and other facilities to the west of the passenger terminal.

Dusty Rankin Drive provides direct access to the eastern GA area from the Victoria Highway. The road is currently in a relatively poor condition.

#### Car Parking

There are two main car parks adjacent to the passenger terminal. There are a total of 140 parking spaces in this area. Car rental companies have a number of spaces dedicated to them.

Car parking is reportedly at capacity during busy periods particularly in the dry season. Overflow parking takes place on the grass area to the west of the main car park. This is largely used informally by the car rental companies to store their vehicles.

There is also evidence that the car park is being used on a long-term basis with vehicles being left for periods ranging from a few days to a few months.

The existing shed, which is located directly to the west of the passenger terminal building, is currently leased to Northern Airport Services for staff parking.

Each of the general aviation users including Shoal Air, Alligator Airways and Slingair Heliwork has its own parking areas for customers. However, it was reported that during the busy dry season car parking demand is significantly over capacity.

Shoal Air has its own secured parking area for use by their customers. It was reported that there is significant demand for use of this secure parking area by people using the airport to fly on RPT services.

### 3.5 OTHER FACILITIES AND ACTIVITIES

The land within the airport site is a combination of freehold land and leased land. Most of the freehold and leasehold land is currently used for aviation-related activities with the exception an area of land to the north of the site which is currently leased and being used for agricultural activities.

#### 3.5.1 PRIVATE HANGARS AND FACILITIES

There are several other private hangars on the airport site which are owned by other parties. These are summarised below:

- The hangar located on lot 309 is privately owned by Guerinoni Super Pty Ltd. It is leased to Australian Air Express and Budget and is used as a freight and car hire depot;
- Lot 303 is owned by NIOBE Holdings Pty Ltd and accommodates a private hangar/shed which is used for private storage;
- Lot 317 is owned by Marsarmatt Holdings Pty Ltd and has recently been developed as a Thrifty car hire depot;
- Lot 316 is owned by GM & SL Johns Pty Ltd and is currently undeveloped and unused;
- Lot 315 is owned by Lone Eagle WA Pty Ltd and accommodates a hangar. Lone Eagle WA provides agricultural helicopter and aerial services. Helicopters are operated from the site.
- Lot 314 is also owned by Lone Eagle WA Pty Ltd and is used in conjunction with Lot 315. It has been approved for pilot accommodation.

#### 3.5.2 ST. JOHN'S AMBULANCE

A new patient transfer facility has recently been developed in the eastern GA area of the airport to support the operations of the RFDS. The facility is owned by SWEK but maintained and operated by St John's Ambulance. The facility occupies a building that sits within Lot 302.

#### 3.5.3 AIRSERVICES AUSTRALIA

As well as the NDB and VOR/DME, Airservices Australia has a Satellite Ground Station (SGS) located at EKRA to the east of the terminal and car park area. This equipment is still in use and provides air ground communications on 122.4 MHz and site information on 131.55MHz.

#### 3.5.4 DEPARTMENT OF DEFENCE

The Department of Defence has a radar beacon located at the airport to the south of the runway and east of Taxiway A. This beacon is part of the Jindalee Operational Radar Network (JORN). This facility is not necessarily related to airport operations. The current lease to the Department of Defence ends on 30 June 2022, with an option for a five year extension.

#### 3.5.5 BUREAU OF METEOROLOGY

A Bureau of Meteorology (BOM) Automatic Weather Information Service (AWIS) is provided at EKRA. The weather station is located at the airport between Taxiway F and the western GA apron area. The station requires a clearance of at least 100m from all obstacles and also has some limitations on the height of obstacles within 100 to 200 metres.

#### 3.5.6 POWER HOUSE

The power house is located south west of the passenger terminal and accommodates much of the plant for the runway lighting and navigational aids. It also accommodates standby generators and switching gear for the runway lighting, TVASIS and other airfield systems. The power house also accommodates the staff room for airport maintenance personnel as well as the weather reporting activities undertaken by SWEK on behalf of BOM based on the onsite weather station recordings.

This building is old and crowded and new facilities for the non-power-related usages are required.

#### 3.5.7 OLD FLIGHT SERVICE BUILDING

The building located directly southeast of the passenger terminal building was once occupied by Airservices Australia and used as the Flight Service Building. This building is now vacant and plans exist to refurbish and redevelop this building for offices for commercial purposes.

#### 3.5.8 AIRPORT DEPOT AND DOG POUND

To the west of the western GA apron is the airport depot which accommodates a storage area for materials and equipment as well as asphalt mixing area. This is the only place with asphalt mixing capabilities in the area.

The town dog pound is also located to the east of this area adjacent to the NDB hut.

#### 3.5.9 SECURITY CONTROL AND AIRSIDE ACCESS

The airport is surrounded by a fence of approximately 1.2m in height. The fence is not a high security fence.

Airside access is provided through a number of gates on the southern side of the airport site. An automatic boom gate to the west of the passenger terminal building provides the main vehicular access to the airside area of the airport. Additional gates within close proximity to the passenger terminal building also exist. Further airside gates are located to the east, beyond the Runway 30 threshold and to the west, beyond the west GA apron.

### 3.5.10 AIRPORT MANAGER'S HOUSE

A residential property occupied the airport manager is located within the airport, south of the passenger terminal building. SWEK no longer require this facility and propose to demolish the building.

## 3.6 ENGINEERING SERVICES AND INFRASTRUCTURE

### 3.6.1 ELECTRICITY

The passenger terminal and associated facilities as well as the hangars and other associated GA facility buildings are connected to the town electricity supply. Back-up power is also provided via generators located in the power house.

The electricity supply to the airport is reported to be at capacity, particularly since the completion of the new passenger terminal. An upgrade to the supply would need to be upgraded to allow further significant development of facilities to take place. SWEK are currently investigating the installation of a solar electrical system for the passenger terminal building.

### 3.6.2 WATER

The airport is supplied by the town water supply. Two water storage tanks are located to the east of the passenger terminal building. These are for fire fighting purposes.

### 3.6.3 SEWER

The airport has an on-site septic system with the tank located adjacent to the western side of the terminal.

### 3.6.4 COMMUNICATIONS

Telephone and internet connections are provided to the airport from the town network.

## 4.0 HISTORICAL AND FORECAST AVIATION ACTIVITY

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### 4.1 HISTORICAL AVIATION ACTIVITY

#### 4.1.1 PASSENGER TRAFFIC

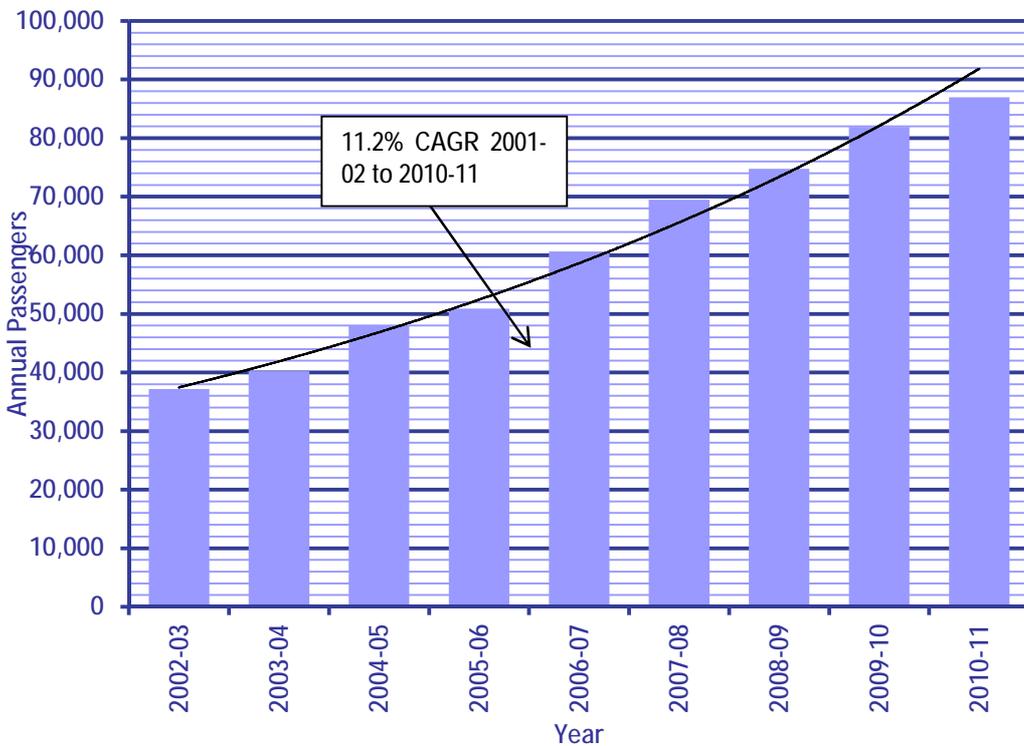
Figure 1 shows RPT passenger numbers during the period 2002-03 to 2010-11. Following the collapse of Ansett Australia in 2001, Airnorth began operations from EKRA. In 2002-03 EKRA saw approximately 37,000 passengers. Since this time, passenger traffic has been growing strongly to almost 87,000 passengers in 2010-11. The CAGR from 2001-02 to 2010-11 was 11.2%.

Figure 2 shows annual passenger traffic at EKRA from 1985-86 to 2001-02. This data has been separated from that for the following years to 2010-11 as it is unclear which passenger types have been included within this data. SWEK have advised that a likely explanation for the level of passengers shown in Figure 2 may be that charter passengers, as well as RPT passengers, have been included. It has been reported that, during 1993 – 1996, the Argyle Diamond mine was at its peak and using the airport daily for charter operations. It is believed that these charter passenger numbers have been included within the passenger data. Therefore data for these years cannot directly be compared with the data collected during the period from 2002-03 to 2010-11 which includes only RPT passengers.

Regardless of the total passenger numbers during this period, the passenger traffic patterns and fluctuations shown in Figure 2 between 1985-86 and 1995-96 provide a good indication of the general level of growth for RPT passengers at EKRA. For example, passenger numbers dropped significantly in 1989-90, coinciding with the pilots' dispute, a similar impact was felt at other Australian airports. The compound annual growth rate (CAGR) between 1985-86 and 1995-96 was 5.1%.

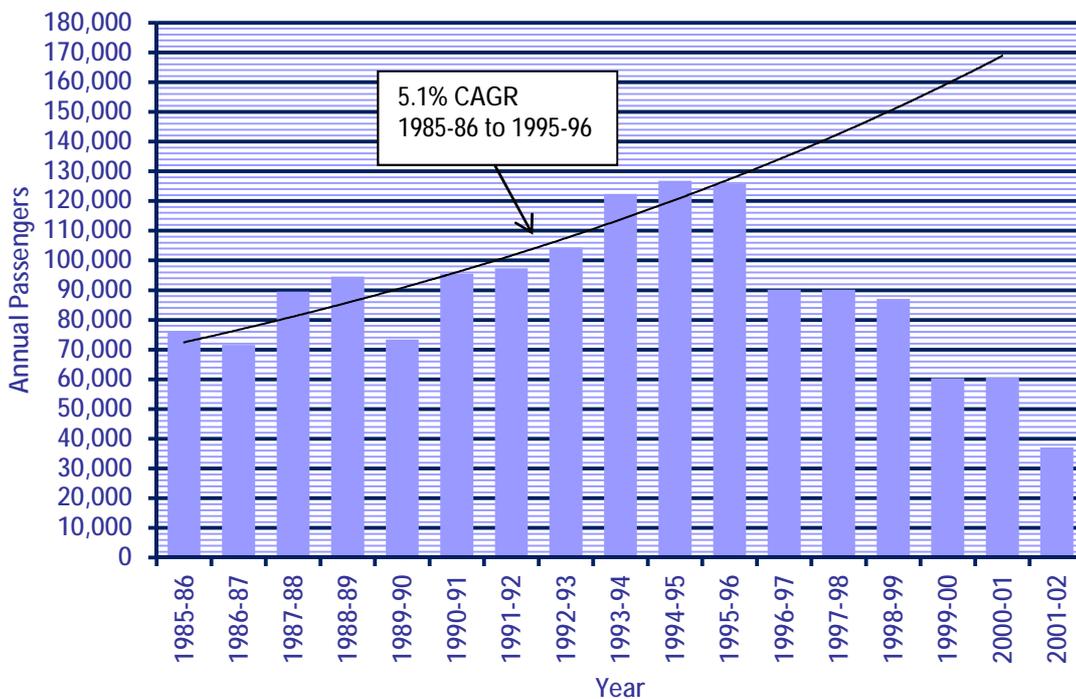
It has been indicated that the significant decline in passenger numbers between 1996-97 and 1999-00 was largely due to a decrease in charter activity by Argyle Diamonds. In 1998, Ansett Australia contracted Airnorth to take on some of the routes between Darwin, Kununurra and Broome with the E120 Brasilia aircraft which proved unpopular with the travelling public. Airnorth continued with the operations once Ansett collapsed in 2001. It should also be noted that changes in methods of data recording as well as key global economic events such as the September 11 attacks are likely to also have had an impact on passenger numbers during 2001-02.

Figure 1: Historical Passenger Traffic 2002-03 to 2010-11



Source: BITRE/SWEK

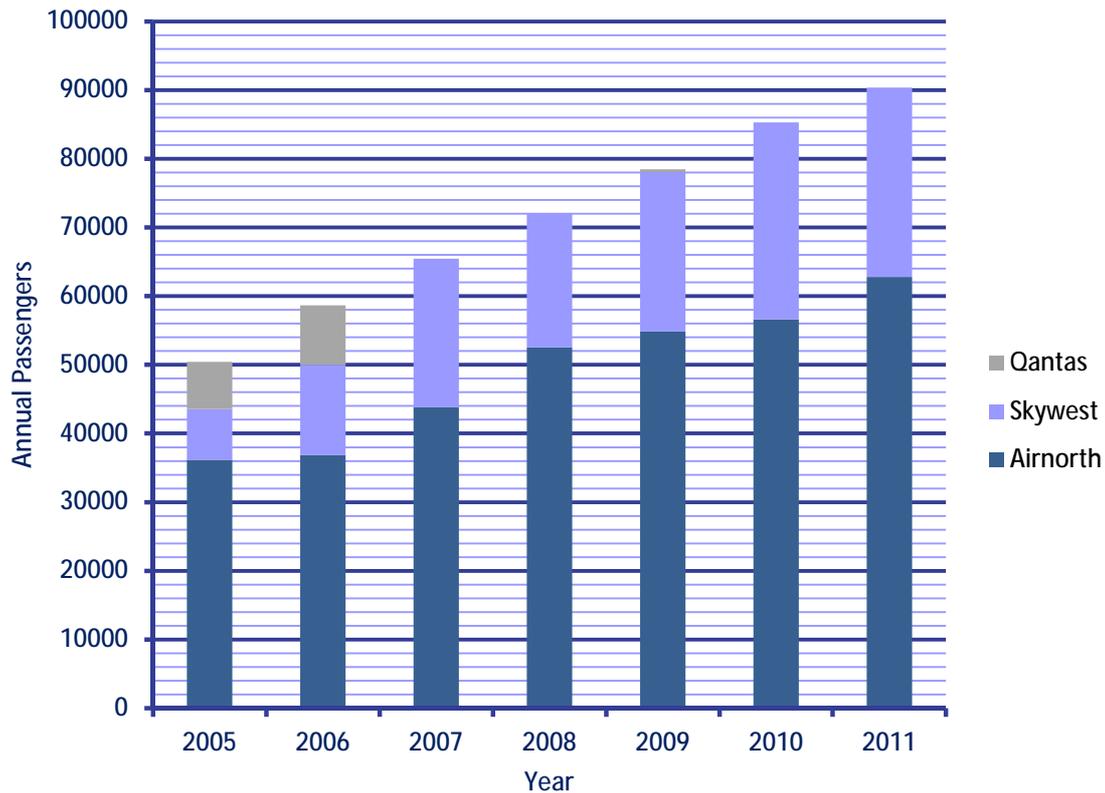
Figure 2: Historical Passenger Traffic 1985-86 to 2000-01



Source: BITRE

Figure 3 shows annual passenger numbers from 2005 to 2011 by airline. Airnorth, Skywest and Qantas have all operated RPT services into EKRA over the last 6 years. Qantas operated a weekly return service to Perth using a BAE 146 aircraft from 2004 to 2006, however Qantas no longer operates into Kununurra. Airnorth carries the majority of passengers with approximately 70% of the passengers over the last 3 years. Skywest carries the remaining 30%.

Figure 3: Historical Passenger Traffic by Airline 2005-2011

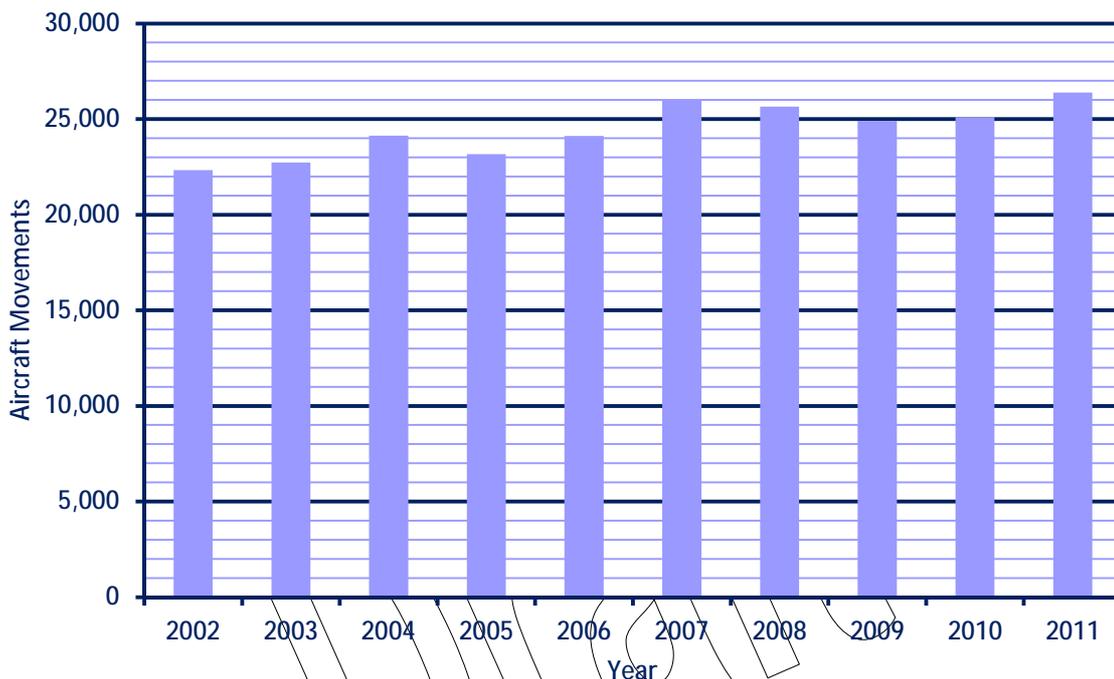


Source: Shire of Wyndham East Kimberley

#### 4.1.2 AIRCRAFT MOVEMENTS

Figure 4 displays total number of aircraft movements at EKRA from 2002 to 2011. The CAGR for the period is 1.9% with almost 26,500 movements in 2011.

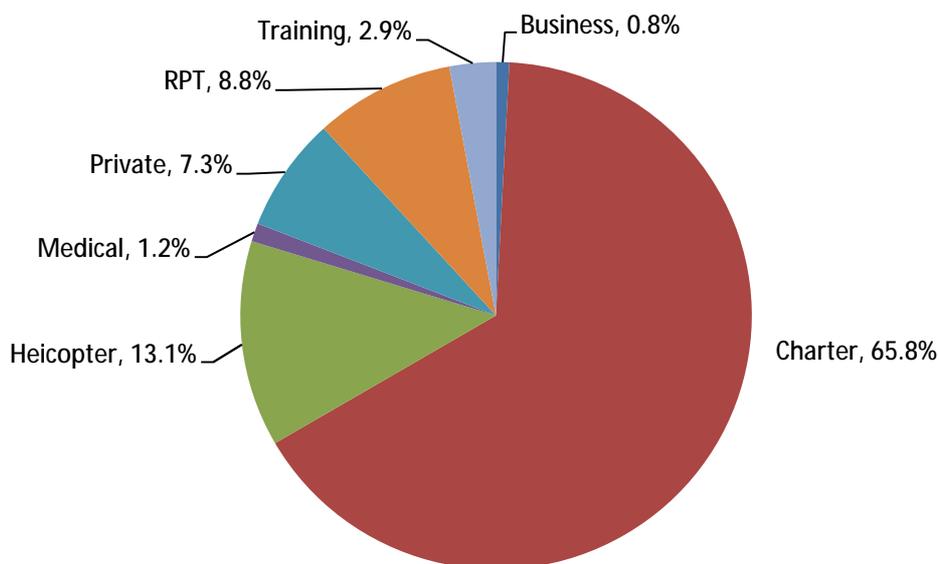
Figure 4: Aircraft Movements 2002 - 2011



Source: Avdata

Figure 5 shows the estimated proportion of movements by the type of activity, based on aircraft movement data for 2011.

Figure 5: 2011 Aircraft Movements by Activity



## Charter

Charter operations are considered to contribute approximately 66% or two-thirds of all movements. Charter operations operated by Alligator Airways, Shoal Air and Slingair Heliwork (excluding helicopter movements) form the vast majority of these movements. These charter services comprise of a range of operations including those operated on behalf of Government agencies to aboriginal communities and a large number which are tourism-related, including scenic flights to local attractions including the Purnululu National Park, Lake Argyle and Mitchell Falls among others. These flights are operated with a range of single and twin piston engine aircraft.

Airnorth operate a charter service to Argyle on behalf of Rio Tinto. This currently operates three return services per weekday to Argyle using the Embraer EMB 120 Brasilia 30-seat, twin-turboprop aircraft.

## Helicopter

Based on the data available, helicopter movements contribute approximately 13% of total aircraft movements at EKRA. The vast majority of these movements are attributed to the two helicopter operators based at the airport, Lone Eagle and Slingair Heliwork. The movements include charter scenic flights to local tourist attractions as well as agricultural-related aerial work, with a large number of operations conducted by Robinson R22 and Robinson R44 helicopters.

## Regular Passenger Transport

Regular Passenger Transport (RPT) movements account for approximately 9% of all aircraft movements at EKRA. Airnorth and Skywest both currently operate RPT services into EKRA.

Airnorth currently operate daily return services direct to Darwin and Broome. Airnorth also operate a weekly return service to Perth on Saturdays. Airnorth operates the Embraer E170 aircraft on the majority of its flights as well as the occasional use of the Embraer EMB 120 Brasilia aircraft.

During the 2011 dry season Skywest operated services direct to Darwin and Broome. It operated two return services per week to both Darwin and Broome plus three one-way services per week to each of these destinations also. However, frequency varies depending on the season. It also provided a one-way service from Perth on a weekly basis. Perth can also be accessed via Broome. Skywest operates exclusively the Fokker 100 aircraft into Kununurra.

## Private

Private operations contribute approximately 7% of all aircraft movements and include movements with smaller piston aircraft for varying private purposes.

## Training

Training movements contribute approximately 3% of all aircraft movements. A large number of these training movements are undertaken by the base charter operators Alligator Airways, Shoal Air and Slingair Heliwork.

## Medical

The Royal Flying Doctor Service (RFDS) uses the airport for medical transfers mainly using the single-engine Pilatus PC-12 aircraft. This currently contributes approximately 1% of the total movements.

## Business

Business aviation movements account for approximately 1% of all aircraft movements at EKRA. This category includes movements by all business jet aircraft.

## 4.2 FORECAST AVIATION ACTIVITY

### 4.2.1 PASSENGER TRAFFIC

Airport infrastructure, particularly the passenger terminal and landside access facilities, needs to be planned with sufficient capacity to accommodate future anticipated passenger levels. Passenger traffic has therefore been forecast to provide a basis for these future facility requirements.

Future passenger numbers and growth rates are related to a variety of factors including travel demand, aircraft types and the resultant seat capacity, aircraft load factors, slot availability at destination airports, airline route economics and traffic growth at existing and potential destinations. Clearly, they are also impacted by a range of external economic system variables that are important to understand in relation to Kununurra.

The planning horizon considered for the EKRA passenger forecasts is 25 years (2036/37). Although, the primary aim of the Master Plan is to provide a strategic planning framework for the next 10 years, it is important to consider the period beyond 10 years to ensure long-term sustainability of the airport and land use planning decisions made at this stage do not prevent the future development of the airport infrastructure.

The key drivers considered to affect passenger numbers and growth rates at EKRA over the 25 year planning horizon are:

- The growth and development in local business activities, particularly in the agricultural land and resource industries;
- Growth and development of the tourism market; and
- Airline activity and business development potential.

The forecasting procedure adopted for this Master Plan included a review of the following:

- Economic conditions affecting SWEK, including the development of agricultural via the Ord-East Kimberley Expansion Project as well as resource activity in the area;
- Other economic development in the region particularly tourism but also government activities;

- Historical and forecast data on passenger movements, aircraft movements, seat capacity and inbound/outbound travel between Kununurra and the existing and future destinations;
- Historical and forecast data on passenger movements, aircraft movements, seat capacity and inbound/outbound travel at other Australian regional airports;
- Airbus and Boeing industry global market forecasts for Australia/New Zealand and the Oceania region as a whole; and
- Other information provided by SWEK.

Based on the drivers highlighted and the information reviewed, three overall growth scenarios have been developed to help determine the infrastructure requirements in the future. The passenger numbers have been forecast separately for each scenario based on assumed annual growth rates.

### Low-Growth Scenario

The low-growth forecast assumes a scenario where economic development within the East Kimberley region experiences limited, if any, growth beyond that which is already in progress today. The scenario is based on an assumed compound annual growth rate (CAGR) of 3% to 2018-19. This is based on the population growth forecasts for the region during this period, which stand at 2.3% - 2.1%<sup>5</sup>, plus some additional growth to account for the current on-going business and tourism activities in the area including existing agriculture activities and mining activities. This includes the Argyle Diamond mine which has an estimated mine life to 2019. This scenario assumes that all other mines in the region have a mine life no longer than 2019. The scenario also assumes that growth in tourism will remain low. It is reasonable to assume that passenger traffic will grow in line with population growth at a minimum with demand for travel from the resident population. Post 2018-19, the growth rate reduces to 2% in line with population growth only.

This scenario is intended to present the lowest level of growth realistically likely to occur.

### Medium-Growth Scenario

The medium-growth forecast assumes that the growth rate experienced over the last nine years is as a result of the market recovering from the shocks that occurred prior to this period including the withdrawal of Argyle Diamond mine personnel from Kununurra which impacted RPT flights as well as charter flights, the September 11 terrorist attacks in 2001 and most significantly, the collapse of Ansett Australia in 2002. Based on this assumption, the medium growth scenario continues the growth trend of the last nine years (11% CAGR) to the point where passenger numbers catch up with those that would have been achieved through the continuation of the long-term growth trend experienced prior to these events. The year where this occurs is 2015-16. As discussed in Section 4.1.1, during the period 1985-86 to 1995-96 the CAGR was approximately 5.1%, if the market shocks had not occurred following this period it could be expected that this growth rate approximately formed the long-term growth trend for the airport. Therefore, this growth rate has

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<sup>5</sup> Western Australia Tomorrow, Western Australia Planning Commission, 2005

been applied for the period 2016-17 to 2031-32. Following this period, to 2036-37, it is assumed that the passenger growth begins to tail off and further decreases to a rate of 2.5%.

This growth scenario takes into account passenger growth that may result from the potential increase in RPT services and an increase in aircraft size.

### High-Growth Scenario

The high-growth scenario assumes the local economy continues to boom as it has been over the last nine years with significant passenger growth at a rate of 11% for the next ten years to 2021-22. This assumes that the activities mentioned in relation to the medium-growth scenario occur but the effects on passenger traffic have increased longevity. This has been based on the expectation that by 2021 there will be increased frequency to existing destinations, as well as new destinations including direct flights to some destinations in the eastern states. The scenario also includes the consideration that international services of some kind may be operated from Kununurra to a destination in South-East Asia.

From 2022-23 to 2036-37, it is assumed that passenger traffic grows at a rate of 5.1% which is based on the continuation of the long-term growth trend from 1985-86 to 1994-95.

It should be noted that there is no guarantee that such large passenger growth will be achieved in reality. However, this scenario is included to provide a worst-case scenario in terms of long-term land and infrastructure requirements and by planning for such a high level ensures that all potential outcomes can be accommodated. Figure 6 and Table 1 present projected annual passenger numbers for the High, Medium and Low passenger growth scenarios for the forecast period, 2011-12 to 2036-37.

Figure 6: Forecast Passenger Traffic 2011-12 to 2036-37

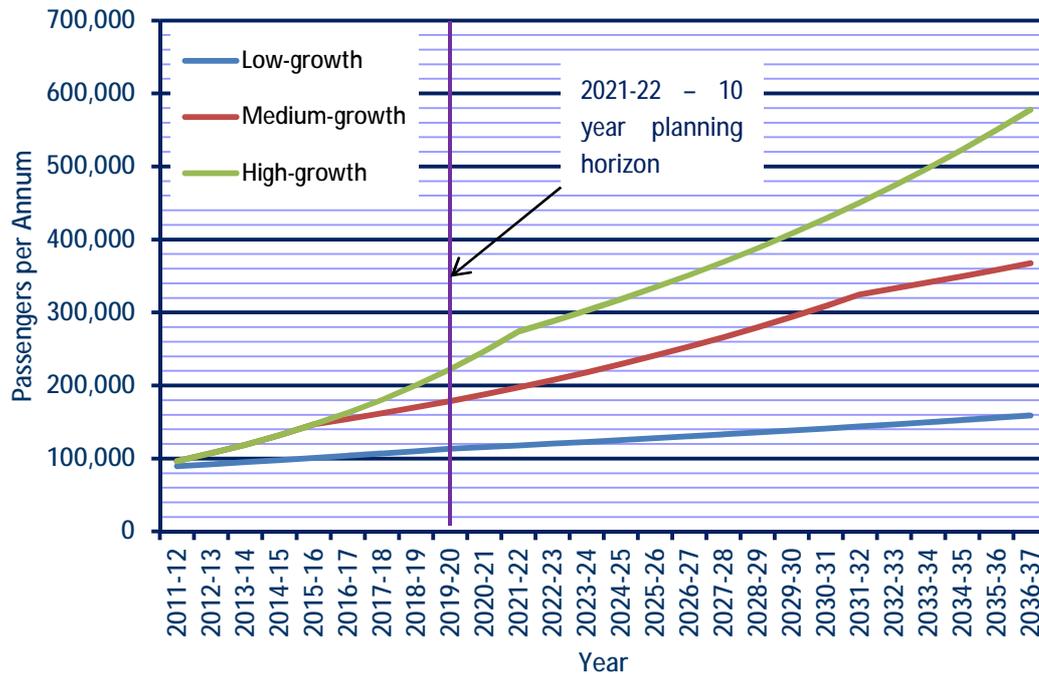


Table 1: Forecast Passenger Traffic 2011-12 to 2036-37 by Passenger Growth Scenario

Year	Low Growth	Medium Growth	High Growth
2009-10 <sup>(A)</sup>	81,933	81,933	81,933
2010-11 <sup>(A)</sup>	86,955	86,955	86,955
2011-12	89,564	96,520	96,520
2012-13	92,251	107,137	107,137
2013-14	95,018	118,922	118,922
2014-15	97,869	132,004	132,004
2015-16	100,805	146,524	146,524
2016-17	103,829	153,997	162,642
2017-18	106,944	161,851	180,533
2018-19	110,152	170,105	200,391
2019-20	113,457	178,781	222,434
2020-21	115,726	187,898	246,902
2021-22	118,040	197,481	274,061
2022-23	120,401	207,553	288,038

Year	Low Growth	Medium Growth	High Growth
2023-24	122,809	218,138	302,728
2024-25	125,265	229,263	318,167
2025-26	127,771	240,955	334,394
2026-27	130,326	253,244	351,448
2027-28	132,932	266,160	369,372
2028-29	135,591	279,734	388,210
2029-30	138,303	294,000	408,008
2030-31	141,069	308,994	428,817
2031-32	143,890	324,753	450,686
2032-33	146,768	332,872	473,671
2033-34	149,704	341,193	497,829
2034-35	152,698	349,728	523,218
2035-36	155,752	358,466	549,902
2036-37	158,867	367,428	577,947

<sup>(A)</sup> Actual passenger numbers provided by SWEK

#### 4.2.2 AIRCRAFT MOVEMENTS

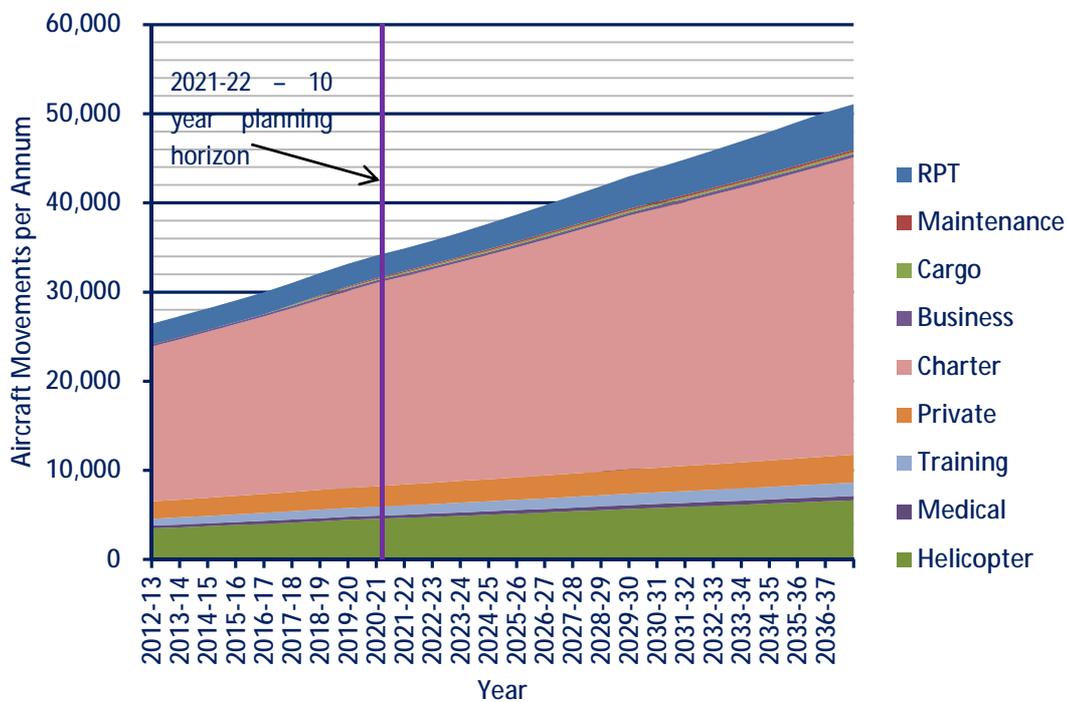
Projections of annual aircraft movement numbers have been developed by segmenting aviation activity into the principal component sectors, each of which has differing drivers and prospects for growth at SWEK. These sectors are:

- Regular Public Transport (RPT);
- Charter
- Business-related (corporate, agricultural, aerial work, etc.);
- Helicopters;
- Freight/Cargo;
- Aircraft maintenance;
- Private (including all recreational and other private operations);
- Training; and
- Medical.

The forecast aircraft movement growth in each segment is shown in Figure 7 and discussed overleaf. The forecast movements represent a high-growth scenario and totals approximately

50,000 movements per year by 2036-37. Figure 7 highlights the 10 year intermediary planning horizon.

Figure 7: Forecast Aircraft Movements 2012-13 to 2036-37



### Regular Public Transport

Regular Public Transport (RPT) movements were estimated with reference to the forecast passenger numbers for the high-growth scenario, for which an assumed flight schedule offering a moderate level of frequency appropriate to that of the overall level of traffic, and with consideration of the assumed increase in aircraft size, was developed.

By 2021-22, passenger numbers are estimated to be 247,000 in the high-growth scenario. These passengers are estimated to be served by approximately 2,600 RPT aircraft movements undertaken primarily by 76-seat Embraer E170 and 100 seat Fokker 100 aircraft with a few services operated by larger 150 to 160 seat jet aircraft, such as the Airbus A320 or Boeing 737-800. This is considered to represent a reasonable estimate of likely RPT operations in 2020-21 if traffic reaches the anticipated levels. However, if the passenger numbers were to be carried on a mix of aircraft offering a greater frequency of services movements would be higher, and could be as high as 4,500 per annum.

By 2036-37, for the projected 577,000 passengers in the high-growth scenario, approximately 5,100 annual RPT movements by 2036-37 is envisaged. It is expected that these would be operated by predominantly 150-160 seat jet aircraft with some operations serviced by 70-100 seat jet aircraft. This is considered to represent a reasonable estimate of likely RPT operations in 2036-

37 if traffic reaches the anticipated levels. However, if the passenger numbers were to be carried on a mix of aircraft offering a greater frequency of services, movements would be higher and could be as high as 6,900 per annum.

### Charter

Based on current aircraft movement information, the vast majority of charter operations at EKRA are undertaken by the based charter operators Alligator Airways, Shoal Air and Slingair Heliwork. A significant proportion of these operations are scenic flights, many of which are operated during the dry season when tourism numbers peak. On this basis, the projected growth in charter aircraft movements has been forecast based on total visitor night tourism forecasts for Western Australia<sup>6</sup>. The growth rate stated for the period to 2018-19 (1.8%) was almost doubled to 3.5% to account for the East Kimberley's tourism potential that is currently largely untapped. This figure also includes anticipated growth in charter movements related to resource FIFO operations and Government-related charters. Beyond this period, the growth rate reduces to 2.5% for the period 2019-20 to 2028-29, and to 2% for the period 2028-29 to 2036-37, as tourism and resource industry growth steadies.

### Business

The growth rates used to forecast business movements at EKRA to 2036-37 have been based on a number of drivers. This category includes movements by business jets. Therefore the growth of this category has been linked to the anticipated growth in business and particularly mining in the area. Population growth forecasts take into consideration the overall growth of the area and are linked to the anticipated growth in the economy. Therefore, business related movements have been forecast with a consideration of the local population forecast plus additional growth to take account of the anticipated growth in the areas of mining and other business activities. Therefore, the forecast population growth rates were increased by approximately 30% to account for this resulting in a growth rate of 3% for the period to 2018-19, 2.3% for the period 2019-20 to 2028-29 and 1.7% for the period 2029-30 to 2036-37.

### Helicopters

The majority of the helicopter operations at EKRA are charter operations and a significant proportion of those are estimated to be tourism-related, therefore, the growth rates assumed for helicopter movement forecasts to 2036-37 are the same as those used to forecast the charter movements, as described above. The growth rates used are 3.5% to 2018 and 2% - 2.5% to 2036-37.

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<sup>6</sup> Tourism Forecasting Committee, Western Australia Regional forecasts tables compound annual growth rates, 2011 Issue 2

## Air Cargo

Based on the stakeholder consultation, the potential for the operation of air cargo services has been considered. The Ord-East Kimberley Expansion Project may result in the operation of cargo services from EKRA to carry fresh produce to Asia. The support for this suggestion has been mixed and the feasibility of this opportunity is dependent on a number of uncertain factors. However, this Master Plan aims to safeguard land for the development of infrastructure in the future and on the basis of considering a worst-case scenario, a small number of air cargo movements have been included within the forecasts. In the absence of any information on the level of operations, for the purposes of forecasting, it has been assumed that there will be approximately four cargo movements per week (assuming the use of Boeing 737-400/700) by 2036-37.

## Maintenance

Stakeholder consultation has indicated that there is a potential opportunity for the development of a maintenance facility for one of the airlines currently operating into Kununurra. It is difficult to accurately predict at this stage the nature of maintenance activity that may be undertaken here, however, it has been assumed that the maintenance facility would accommodate jet aircraft up to 160-190 seats. It has been assumed, for the purposes of the forecasts, that the maintenance facility may accommodate third party heavy maintenance and will contribute up to approximately five movements per week by 2036-37.

## Private

It is assumed that the large majority of the current private operations at EKRA are operated by local people with aircraft or the ability to hire an aircraft. Therefore, private aircraft movements have been forecast to 2036-37 based on the population forecast growth rates for the local area, 2.1% - 2.3% to 2018 and 1.7% - 1.9% to 2036-37<sup>5</sup>.

## Training

In 2011 it is estimated that the majority of training movements at EKRA were undertaken by the based charter operators Alligator Airways, Shoal Air and Slingair Heliwork for internal training purposes. Although training movements are not directly related to tourism trends, the majority of the general movements undertaken by these operators are tourism-related. On the basis that the based charter operators will need to acquire more aircraft and more flight crew to service the increased tourism demand, it has been assumed that training movements will grow at the same rate as charter movements. Specifically, 3.5% to 2018 and 2% - 2.5% to 2036-37.

## Medical

As with private aviation, aeromedical movements, operated by the RFDS, are generally driven by population growth for the local area. Therefore, growth rates used to forecast medical-related operations to 2036-37 are also 2.1% - 2.3% to 2018 and 1.7% - 1.9% to 2036-37<sup>7</sup>.

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<sup>7</sup> Western Australia Tomorrow, Western Australia Planning Commission, 2005.

## 5.0 AERONAUTICAL DEVELOPMENT CONCEPT

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The proposed aeronautical development concept, covering airfield and terminal infrastructure requirements and development staging, has been prepared on the basis of satisfying a set of critical planning parameters. Section 5.1 sets out the critical planning parameters upon which the aeronautical development proposals are based. This is followed by presentation of the proposals and development concepts for the runways, taxiways and passenger terminal in Sections 5.2 through 5.7. Drawings B11337A003A to B11337A005 set out the concepts described in this section and can be found in Appendix B. With the exception of the runway and associated taxiway development, all other development shown in Drawings B11337A003A and B11337A003B is the same.

### 5.1 CRITICAL PLANNING PARAMETERS

Whilst the forecasts of overall passenger traffic and aircraft movements described in Section 4.0 are useful for gaining an understanding of likely future activity levels, they are of only limited value as inputs to the planning of individual aeronautical facilities. Therefore, more specific key planning parameters have been developed consistent with these overall forecasts.

To determine the key planning parameters, which include terminal sizing requirements and aircraft parking capacity, a scenario-based approach was adopted which considered the potential impacts of varying combinations of passenger traffic and operating aircraft size/frequency in order to plan for the worst case scenario in terms of infrastructure development.

#### 5.1.1 PLANNING SCENARIO ANALYSIS

Likely aircraft types, operating frequencies and schedules were determined through discussion with SWEK and the incumbent RPT airlines, Airnorth and Skywest, together with reference to other industry knowledge and the application of a general understanding of airline operations to determine nominal future flight schedules for a range of different scenarios.

The key airline operational scenarios encapsulated in the base schedules are:

- **High-Frequency operations** utilising generally the smallest aircraft types appropriate to the level of traffic, to the greatest number of destinations considered viable and offering the highest frequency of service considered viable to each destination;
- **Low-Frequency operations** utilising generally the largest aircraft types appropriate to the level of traffic, serving only the existing destinations and offering the lowest frequency of service to those destinations; and
- **Medium-Frequency operations** utilising aircraft types of intermediate size to serve traffic to existing destinations, and in some cases the most likely additional destinations, on a moderate frequency to each destination.

Each operational scenario can be applied to either of the three passenger growth scenarios developed in Section 4.2. This combination gives a possible nine planning scenarios at each future year (providing a possible total 225 scenarios over the 25 year period). Clearly, many of these scenarios overlap, in terms of critical facility requirements. A total of eight key planning scenarios were selected with the objective of covering the worst-case situation in terms of facility requirements at 2021-22 (10 years) and 2036-37 (25 years), together with analysis of intermediate years to assist in the determination of likely trigger points for the implementation of new or expanded infrastructure. Considered as a whole, these traffic scenarios represent the widest practical range of potential traffic levels and related services frequencies.

For each scenario a base schedule detailing the typical weekly airline operations including airline, origin/destination, scheduled arrival/departure time and operating aircraft type was developed. These nominal schedules with peak aircraft load factors were used to determine the maximum number of passengers in the terminal at one time and maximum concurrent aircraft parking requirements.

### 5.1.2 TERMINAL AND AIRCRAFT PARKING REQUIREMENTS

The planning scenario analysis was used to determine the maximum number of passengers in each key element of the passenger terminal. The maximum number and mix of RPT aircraft on the apron at any one time was also identified at key years. This provided the critical planning parameters for the passenger terminal, RPT apron and the components of the runway and taxiway systems used by RPT aircraft. It is also anticipated that there will be a continuation of charter passenger operations with increasing FIFO operations into the airport which will also require the use of the passenger terminal.

The RPT and large charter/FIFO requirements are summarised in Table 2.

Table 2: Key RPT Planning Parameters

Scenario	Key Terminal Design Parameters (No. of Passengers*)	Max. Apron Parking Requirements (No. of aircraft)				
		RPT			Charter	
		F100	E190	A320-200/ B737-800	F100	Total
Stage 1 (2021-22)	750	1	1	1	1	4
Stage 2 (2036-37)	1000	1	1	2	1	5

\*Includes both arriving and departing passengers

Functional space requirements for the terminal were then developed by reference to the International Air Transport Association (IATA) *Airport Development Reference Manual* (9<sup>th</sup> Edition) for a Level of Service C. Level of Service C represents a good balance between passenger comfort and space efficiency and is generally adopted as the appropriate level for planning purposes. It

should be noted that design parameters for passenger and checked baggage security screening, where considered to be required, have been based on current Commonwealth requirements and experience at other regional airports in Australia.

### Retail Space

An allowance for retail space has been included appropriate to an airport terminal of the current and anticipated size at EKRA. This allowance was based on benchmarking of retail areas at some typical Australian and overseas regional airports and based on industry best practice.

## 5.1.3 DESIGN AIRCRAFT CHARACTERISTICS

### ICAO Reference Code

The dimensions, shape and layout of basic aerodrome facilities such as runways, taxiways and aprons are essentially determined by the performance capability and size of the aircraft that are intended to use them. The planning and design of these facilities therefore begins by identifying the most demanding or critical aircraft that will use them.

In Australia, like most countries, this is achieved by using an ICAO reference code system. The reference code has two elements, a number and a letter, which are derived by grouping aircraft with similar performance capability and key physical dimensions. Thirteen aircraft groupings, each with a unique code number and letter combination such as 1A, 2B, 3C and 4D have been identified.

The objective is to plan individual facilities for the critical aircraft likely to use them. Different facilities at the airport, such as those intended for RPT services and those intended solely for GA aircraft, are normally planned for their specific critical aircraft. On the other hand, common use facilities such as the primary runway and taxiway system will be planned for the most demanding aircraft envisaged to use the airport.

### Pavement Strength

The strength of airfield pavements is classified using the ICAO Aircraft Classification Number/Pavement Classification Number (ACN/PCN) system. The ACN is calculated by the aircraft manufacturer for each aircraft, based on the damaging effect of the aircraft on different types of pavement. The ACN is dependent on both the maximum weight of the aircraft and the number, type and configuration of the landing gear. The ACN also includes a component related to the tyre pressure of the main gear, which can often become the critical parameter in relation to pavement strength.

### Principal Aircraft Parameters

Table 3 summarises the principal relevant planning parameters that relate to aeronautical facilities for each of the key aircraft types that might conceivably use EKRA in the future.

**Table 3: Principal Design Aircraft Key Parameters**

Aircraft Type	Wingspan (m)	Tail Height (m)	MTOW (kg)	ICAO Aerodrome Reference Code	ACN <sup>(1)</sup>	Typical Passenger Capacity (Pax)
Cessna 172	10.9	2.7	1,160	1A	< 5,700 kg	N/A
Cessna 310	11.3	3.3	2,495	1A	< 5,700 kg	N/A
Beech Super King Air 200	16.6	4.5	5,670	1B	< 5,700 kg	N/A
Pilatus PC-12	16.2	4.3	4,740	2B	< 5,700 kg	N/A
Bombardier Dash 8-100	25.9	7.5	15,650	2C	8	37
Embraer EMB-120 Brasilia	19.8	6.4	11,500	3C	6	30
Bombardier Global Express	28.7	7.6	44,500	3C	28	19
ATR 72	27.0	7.7	22,000	3C	12	66
Embraer E-170	26.0	9.7	37,200	3C	19	78
Fokker 100	28.0	8.5	45,810	3C	24	107
Embraer E-190	28.7	10.5	46,990	4C	28	106
Airbus A320-200	33.9	11.8	73,500	4C	37	150
Boeing B737-800	35.8	12.6	70,535	4C	40	175

(1) For flexible pavement on a medium (category B) sub-grade

## 5.2 RUNWAY CAPACITY

The capacity of the proposed Runway 12/30 configuration has been considered. The number of annual movements forecast for 2036/37 is 51,050, based on experience from other airports the existing runway configuration will provide more than sufficient capacity for the next 25 years and beyond.

## 5.3 RUNWAY LENGTH, WIDTH AND STRENGTH

The development of a longer runway at EKRA to support future increases in aircraft size and range of destinations, forms a key development item of this Master Plan. Based on the information gathered during stakeholder consultation, the Master Plan should safeguard for the development of a runway to support operations by narrow-body jet aircraft such as the A320-200 and Boeing 737-800 to all domestic destinations on RPT services and possibly to destinations in south-east Asia.

The Master Plan process has assessed several potential runway development options to identify the concept that allows for the maximum runway length anticipated to be required whilst at the same time avoiding disruption to existing facilities. It is also paramount that the construction of

runway facilities at EKRA does not impact upon operations and the existing runway can remain operational throughout.

Two runway development options are included within this Master Plan. Both have certain characteristics that may make them more viable to develop depending on the potential funding that may become available. Both have been included to ensure the required land is safeguarded for these purposes and either option can be developed depending on the outcome of a number of factors which are as yet unknown.

### 5.3.1 RUNWAY OPTION 1 (EXISTING RUNWAY)

To accommodate the anticipated operations, the existing runway will require strengthening, widening and extending as described below and shown on Drawing B11337A003A at Appendix B.

#### Length

Available land exists between the threshold of Runway 12 and the Ord River to the northwest to accommodate a runway extension. Due to the characteristics of the terrain in this area, only a limited section of the total land area is likely to be suitable to accommodate the extension of the runway due to the considerable quantities of fill that would be required. Therefore the maximum possible runway extension that can be achieved is approximately 520 metres. This results in a total runway length of approximately 2,350 metres. This runway length would support operations by Code 4C, 150 to 160 seat jet aircraft such as the Boeing 737-800 and A320-200 to destinations on the east coast of Australia. Some limited destinations in South-east Asia, such as Denpasar or Singapore may also be possible, however some limitations on payload may be required.

A runway turning node should be constructed at the Runway 12 threshold, prior to Taxiway F being fully upgraded to accommodate Code C aircraft, to allow Code C aircraft to turn.

#### Width

The runway extension would be required to be constructed with a width of 45 metres together with the widening of the existing pavement to accommodate Code 4C aircraft.

#### Pavement Strength

As described in Section 3.1.1, the condition and strength of the existing runway is poor, particularly during the wet season. To accommodate larger aircraft strengthening will be required. Depending on the exact condition of the existing pavement and subgrade, it is not clear whether the runway will be required to be closed to undertake the strengthening works. If it proves possible to undertake the strengthening through an asphalt overlay, then this can easily be undertaken in overnight shifts, allowing the runway to remain operational during the day. However, if more substantial reconstruction work is required during the day, this will require the closure of the runway. SWEK must keep the runway operational at all times, therefore this option will not be selected if works are required which cannot be performed during overnight closures. A more

detailed engineering assessment of the existing runway is required to ascertain the nature of strengthening works that will be required, and therefore whether the future use of the existing runway to accommodate larger aircraft is feasible.

Depending on the time-frame for the development of this runway option and the full strengthening works, interim pavement maintenance will be required to ensure the pavement continues to be useable for operations in the short-term.

The runway extension pavement should be constructed with a pavement classification number (PCN) of 40 (assuming a flexible pavement and Category B subgrade) that is suitable to accommodate Code 4C aircraft including the Boeing 737-800 and Airbus A320-200.

### Runway Strip and Runway End Safety Areas

A runway strip of 300 metres can be accommodated which will allow for the operation of precision approach operations by Code 4C aircraft. No land acquisition is required to accommodate any part of the runway, runway strip or runway end safety areas (RESAs). The northern edge of a 300 metre wide runway strip coincides with the existing airport boundary at the north-western end of the runway. The transitional surface in this area will also require any objects to sit beneath the 1:7 slope that will extend upwards from the edge of the runway strip. To ensure the 300 metre wide runway strip is not compromised and the perimeter fence does not penetrate the transitional surface it would be prudent for SWEK to acquire the section of land that currently sits on the north-western side of the runway from the State of WA.

### Runway Lighting

New runway lighting should be implemented with the extension of the runway. It is anticipated that the existing runway lighting on the existing runway will also be upgraded at this time.

The existing T-VASIS approach path indicator system is located as such that it will require relocation when the runway is extended and widened. Due to the age of this technology a new Precision Approach Path Indicator (PAPI) system should be installed at each runway end.

### 5.3.2 RUNWAY OPTION 2 (NEW RUNWAY)

Due to the likely limitations on the development of the existing runway in terms of both maximum length and the potential impact on existing operations, the option of developing a new runway located north of, but parallel, to the existing runway has been included within the Master Plan. The proposed runway location has been selected through the assessment of a range of other possible runway alignments. Due to the characteristics of the surrounding terrain, the existing 12/30 alignment is the only one that has the realistic potential to accommodate operations by larger aircraft to destinations further afield than are currently served. The proposed runway centreline can be accommodated at a minimum distance of approximately 105 metres from the existing runway centreline. This offset would allow for the runway to be constructed while the existing runway remains operational with only limited restrictions from construction work, which could largely be

completed during the day. Some land acquisition will be required to construct a runway on this alignment, however the majority of the works remain within the existing airport boundary as shown in Drawing B11337A003B at Appendix B.

### Length

With consideration of the terrain to the northwest and the Victoria Highway to the southeast, the maximum runway length that can be accommodated is approximately 2,500 metres. This length will accommodate unrestricted operations by 150-160 jet aircraft such as the Boeing 737-800 and Airbus A320-200 to all domestic destinations as well as to destinations within south-east Asia including Singapore, Kuala Lumpur and Denpasar. Some destinations in southern China may also be within reach however, some limitations on payload may need to be applied.

The runway will be required to be constructed with a width of 45 metres to accommodate Code 4C aircraft.

### Pavement Strength

The runway pavement should be constructed with a pavement classification number (PCN) of 40 (assuming a flexible pavement and Category B subgrade) that is suitable to accommodate Code 4C aircraft including the Boeing 737-800 and Airbus A320-200.

### Runway Strip and Runway End Safety Areas

A 300 metre wide runway strip should also be provided to allow for the operation of Code 4 instrument approach operations. Some land acquisition is required to accommodate the runway strip on the north side of the runway, towards the Runway 12 threshold. The land that would be required is currently Crown land and negotiations with the State would be required to ensure that this land can be acquired. Negotiations with the State are currently ongoing regarding the acquisition of an area of land on the south side of the runway. Based on the outcomes of these to date, the acquisition of the piece of land to the north is anticipated to be possible.

The edge of the runway strip lies directly adjacent to the area of freehold land to the north of the Crown land, towards the Runway 12 threshold. The existing trees directly adjacent to the runway strip will be required to be limited to a height of approximately 1.5 metres at the location closest to the runway strip to accommodate the transitional surface which slopes upwards from the edge runway strip at a grade of 1:7. SWEK can either negotiate this requirement with the land owner or alternatively acquire the area of freehold land that will be affected to ensure the land use in this area is maintained under their control. On the basis that this land is currently planted with sandalwood trees, which can grow to a height of up to 5 metres, a portion of land 1,150 metres long by 50 metres wide would need to be acquired.

To accommodate the maximum possible runway length at the Runway 30 end, some land acquisition will be required to the south east of the runway to accommodate the runway strip and RESA. This area is private land and also directly adjacent to an existing structure. This structure

will be an obstacle within the approach and take-off climb surfaces of Runway 12/30 and if not removed will limit the available runway length to approximately 2,400m (assuming an approximate height of 6 metres of the structure). It is therefore recommended that SWEK acquire this area of land and arrange for removal of the structure in due course to ensure the maximum runway length is available. Ownership by SWEK will ensure that control is maintained of the development on this land that could become obstacles.

### Runway Lighting

All new runway lighting will be installed on the new runway. A PAPI system should also be provided at each runway end.

## 5.4 PASSENGER TERMINAL FACILITIES

### 5.4.1 CAPACITY OF EXISTING FACILITIES

On the basis of the scenario planning and the design criteria for the new terminal, in general the terminal is likely to be adequate for approximately the next 10 years, until 2021/22 in a high growth scenario with both low (the design criteria) and high frequency RPT operations. The low frequency scenario is expected to include the simultaneous accommodation of two aircraft up to Boeing 737-800/Airbus A320-200 (as used for the design criteria for the existing terminal building). The high frequency scenario anticipates simultaneous operations of three aircraft including an Embraer E-170, a Fokker 100 (or other aircraft of similar size) plus an A320-200/Boeing 737-800.

However, in addition to the RPT services outlined above, there is also potential for FIFO or other charter services to be operated out of EKRA. These operations could feasibly be conducted with progressively larger aircraft, up to 100 seats. If FIFO operations were to coincide with RPT operations, the existing passenger terminal will reach capacity sooner. To accommodate this additional capacity, there is potential for some reconfiguration works within the terminal, such as increasing the size of the departure lounge by expanding it into the existing landside waiting area, including the area currently occupied by the café and the covered outdoor area.

The best estimate therefore is that in general the current terminal is likely to be able to accommodate potential demand up to around 2021-22 and will require upgrade or expansion when more than two aircraft of A320-200 or a Boeing 737-800 size (or a combination of smaller aircraft), require simultaneous use of the terminal. The frequent operation of an international service may also require significant expansion to the terminal with the development of a dedicated international departure lounge with areas to accommodate the requirements for customs, immigration and quarantine.

However, to ensure that the more immediate operational requirements of the passenger terminal continue to operate effectively, SWEK should liaise with terminal stakeholders including the ground handling agent, Northern Airport Services, as well as car hire companies. Minor reconfiguration of the terminal may be required in the near future to accommodate the needs of these stakeholders. .

## 5.4.2 FUTURE TERMINAL REQUIREMENTS

The future terminal space requirements were estimated by application of the design parameters established from the planning scenario analysis and summarised in Table 2 in Section 5.1.2. The overall space requirements in Stages 1 and 2 are summarised in Table 4.

Table 4: Estimated Future Terminal Spatial Requirements

Element	Functional Space (m <sup>2</sup> )	Retail Space (m <sup>2</sup> )	Total (m <sup>2</sup> )
Stage 1 (2021-22)	2,300	100	2,400
Stage 2 (2036-37)	4,000	200	4,200

The methodology adopted to determine the space requirements in Table 4 leads to a conservative estimate of overall footprint, which is considered reasonable to plan and safeguard for, but which may not all be required. Ultimately between 4,000m<sup>2</sup> and 4,500m<sup>2</sup> of terminal footprint is anticipated in 2036-37. This would be sufficient to handle the highest traffic growth forecast with the most demanding combination of aircraft sizes and frequencies envisaged.

When required this Master Plan proposes that terminal expansion should occur to the west of the existing building as indicated on Drawing B11337A005 at Appendix B. This will require the relocation of the existing shed that is currently leased for parking to Northern Airport Services staff as well as the existing tug shed.

It is also anticipated that a separate cargo area may be required within or directly adjacent to the passenger terminal to handle cargo carried in the holds of passenger aircraft. In the short term this could be accommodated within the shed to the west of the terminal currently used for car parking by Northern Airport Services. As the terminal expands into this area, a new facility should be constructed, either as part of the new terminal facility or as a separate building to the west of the terminal building.

A significant increase in the area required to store GSE will also be required, this should also be located to the west of the existing baggage make-up area with the redevelopment of the existing tug shed of a sufficient size to accommodate any GSE. Any airline support services such as the storage of catering supplies can also be continued to be accommodated within the same facility.

## 5.5 AIRCRAFT PARKING

### 5.5.1 EXISTING RPT APRON

The existing RPT apron has sufficient capacity to accommodate RPT operations until approximately 2021-22 depending on passenger growth and the actual mix of aircraft. However, considering the likelihood of an increase in charter operations with increasingly larger aircraft, it is anticipated that an apron extension may be required prior to 2021-22, again depending on the

growth and respective operating schedules of both RPT and charter operations as well as the actual aircraft mix.

## 5.5.2 FUTURE RPT APRON REQUIREMENTS

### Stage 1

The planning scenario analysis indicates that by 2021/20, with the highest traffic envisaged and the most demanding mix of aircraft in terms of apron space, the maximum number of RPT aircraft on the apron simultaneously is likely to be three in the high frequency scenario and two aircraft in the low frequency scenario. The high frequency scenario will prove most demanding in terms of apron space, requiring simultaneous parking for two Embraer E-190/Fokker 100 (or other aircraft of similar capacity), and one Airbus A320/Boeing 737-800 (or other aircraft of similar capacity). These three parking positions could be accommodated within the existing apron; however, the operation of charter aircraft in addition to these RPT operations into EKRA is likely to require the expansion of the existing apron. It is estimated that an additional parking position for a Fokker 100, or other aircraft of similar size, will need to be accommodated. Therefore an apron expansion of approximately 2,800m<sup>2</sup> to the west of the existing is proposed in Stage 1. However, to provide additional flexibility by enabling the accommodation of a larger aircraft at this position, such as a B737-800, this expansion could be up to 5,500 m<sup>2</sup>.

The existing RPT apron pavement will need to be strengthened to accommodate the larger aircraft mentioned above including the Airbus A320 and Boeing 737-800 prior to the accommodation of these aircraft on the apron. Flood lighting should be installed with the development of the extended apron. The existing RPT apron flood lighting should be upgraded or replaced at this time.

### Stage 2

By 2036-37, the planning scenario analysis indicates that, with the highest traffic growth envisaged, the maximum number of aircraft parking positions required by RPT aircraft is four in the high frequency scenario and three in low frequency scenario. The high frequency scenario will prove most demanding in terms of apron space. Aircraft that are likely to be operated will continue to be similar to those anticipated to be used in Stage 1.

It is anticipated that RPT apron expansion will continue to the west and will incorporate the proposed Stage 1 private jet apron, which is discussed further in Section 5.5.3 and shown on Drawing B11337A005 at Appendix B.

Beyond 2036-37, depending on passenger growth and actual aircraft mix, aircraft parking adjacent to the terminal building may not be sufficient and alternative areas for apron expansion will need to be sought. There is potential to develop the area directly to the north of the existing RPT apron to accommodate additional RPT or charter aircraft as shown on Drawing B11337A004 at Appendix B, these parking positions would be remote and appropriate arrangements would need to be made to provide safe pedestrian/vehicle access between the aircraft and passenger terminal building.

### 5.5.3 PRIVATE JET APRON

There is currently a relatively frequent use of EKRA by private business jets, particularly in the dry season. There is however no dedicated parking for these types of aircraft and the operators generally like to avoid the use of the RPT apron so that the requirement for security screening is avoided. It is proposed that in Stage 1 a dedicated private jet apron to the west of the RPT apron be developed with a single parking position for an aircraft up to the size of the Bombardier Global Express. Secondary parking positions for smaller, Code B, private jet aircraft will also be accommodated on this apron. It is proposed that this apron is located as far west as possible, adjacent to the Slingair Heliwork hangars, to allow space for the expansion of the RPT apron. Part of the private apron will extend over the south east corner of the existing west GA apron area. It is also anticipated that the private jet apron will be developed over the area of land which is currently identified as Lot 318. Although this is currently still under the ownership of SWEK, plans exist to sell this lot for private development. It is recommended that any such plans should not proceed to ensure that the development of the private jet apron is not impeded.

This location allows the private jet apron to be completely separate to the RPT apron for as long as possible to ensure segregation of operations for security screening purposes. Flood lighting should be installed on the new private jet apron to allow night operations. Landside access can be provided to the apron via the existing automatic gate.

When this area is required for RPT apron expansion during Stage 2, private jet parking will need to be relocated to another location on the airport, potentially the additional apron area to the north of the existing RPT apron as shown in Drawing B11337A005 at Appendix B. Due to the extensive nature of the aircraft movement area and the limited amount of available land, in Stage 2 it is likely that the private jet apron will need to be subject to security screening requirements.

### 5.5.4 EAST GA APRON

Evidence suggests that during busy periods the east GA apron is at capacity with the area designated for visiting aircraft also being used by the based charter operators. Due to the location of the apron between the Air BP and Shell fuel facilities to the east and the RPT apron to the west there is little scope for expansion of this apron.

To increase the available aircraft parking for the potential future expansion plans of the based charter operators located in this area, parking for visiting GA aircraft should be relocated to the west GA apron when this is expanded. Any new GA development or operations should also be located on the west GA apron where additional capacity will be available following its expansion.

The RFDS aircraft parking position located to the east of the apron should remain at this location, adjacent to the St. John's Ambulance patient transfer facility.

The construction of parallel taxiway (Taxiway F) between the Runway 30 threshold and Taxiway A to prevent a requirement for aircraft to use Taxiway C will reduce the current issue of GA traffic crossing the RPT apron. All aircraft on the east GA apron will be required to use Taxiway B or

Taxiway F to access the runway and any facilities to the west of the RPT apron. Taxiway C will become redundant and the area currently occupied by this taxiway can be integrated into the east GA apron and the space utilised as a taxilane and additional parking. Some reconfiguration of the existing taxilanes and parking positions may be required to maximise the area as a whole. Access prevention on to the RPT apron from the east GA apron should be managed through the development of an appropriate operational procedure.

In the event of Runway Option 2 being developed there may be scope to expand the east GA apron on the northern side of Taxiway F.

#### 5.5.5 WEST GA APRON

Indicative evidence suggests that the west GA apron is at capacity during busy periods. Due to the limited potential for expansion of the east GA apron, the west GA apron should be expanded to accommodate the relocated visiting aircraft parking as well as any additional GA parking that may be required by new operators that establish themselves within the aviation-related commercial precinct to the south and west of the existing west GA apron.

The apron should be expanded to accommodate both Code A and Code B aircraft to accommodate the future expansion of Slingair Heliwork and other possible new operations. In Stage 1, the Master Plan proposes that a dedicated private jet apron be developed on the eastern edge of the west GA apron. Some GA aircraft parking will be lost due to this development. Therefore, sufficient additional capacity should be constructed as part of the west GA apron expansion in Stage 1 to make up for this.

Initially, the existing apron can be expanded to the north towards the weather station, however remaining at a distance that still provides 100 metres clearance. This would approximately double the size of the existing apron to approximately 30,000 m<sup>2</sup> including aircraft parking and taxilanes. Some reconfiguration of the existing taxilanes and parking positions will be required to ensure the area created by the expansion is maximised. It is proposed that landside access for visiting GA aircraft pilots and passengers will be via the existing automatic Gate 10.

To accommodate demand for GA parking resulting from the development of the aviation-related commercial precinct and the potential development of a fuel facility in this area, the apron can also be extended to the northwest of the existing apron along the eastern edge of the proposed aviation-related commercial precinct. The apron can extend as far as the proposed aircraft maintenance precinct and Taxiway F to the northwest. This area can be developed in sub-stages according to demand.

As additional GA aircraft parking is required in Stage 2, the BoM weather station could be relocated and the west GA apron can expand further to the north, up to Taxiway F.

It is anticipated that once Taxiway F is extended between the Runway 30 threshold and Taxiway A, all aircraft utilising the west GA apron should have no requirement to utilise Taxiway C to access the east GA apron including the fuel facilities.

## 5.6 TAXIWAY SYSTEM

The taxiway system is required to link the runways with the aircraft parking areas and support facilities on the aerodrome. An effective taxiway network is critical in maximising the operational capacity of the airfield. However, taxiway capacity is difficult to define precisely, or in the same way as it is for runways. Instead, it is necessary to rely largely on experience, drawn from the operation of other airports, as to what constitutes an effective taxiway network.

Development of the taxiway network is discussed in the following sections. Where taxiway development will differ depending on the runway option selected this is highlighted.

### 5.6.1 TAXIWAY A

The existing main taxiway (Taxiway A) links Runway 12/30 with the RPT apron and the west GA apron and currently accommodates Code C aircraft. It is not anticipated that this taxiway will require future widening, however the pavement strength should be upgraded when the RPT apron pavement is upgraded to accommodate Airbus A320-200/Boeing 737-800 aircraft.

If Runway Option 2 is selected, Taxiway A will require an extension of approximately 100 metres from the existing runway to the new runway.

### 5.6.2 TAXIWAY B

Taxiway B currently connects the Runway 30 threshold to Taxiway C and can accommodate Code C aircraft. The Master Plan proposes that the taxiway pavement is strengthened to accommodate Airbus A320-200/Boeing 737-800 aircraft to provide access between Runway 30 threshold and Taxiway F.

If Runway Option 2 is selected, the existing Taxiway B will be redundant. It is anticipated that a new taxiway connection to the Runway 30 threshold will be required from the new Taxiway F, which has been designated as Taxiway B for the purposes of this Master Plan.

### 5.6.3 TAXIWAY C

Taxiway C currently connects Taxiway B to the west GA apron and the RPT apron. When Taxiway F is extended southeast from Taxiway A, Taxiway C should then be integrated into the east GA apron and utilised as a taxiway and parking area as required.

### 5.6.4 TAXIWAY F

The requirement for a parallel taxiway is based on a number of operational considerations including aircraft mix, traffic peaking, traffic volume and the number of taxiway exits, among other things. A full detailed assessment has not been undertaken within this Master Plan however, a full parallel taxiway is typically needed when the normal peak demand is expected to exceed around 20 movements per hour. Based on a 16-hour operating day and assumed average hourly demand of 0.5 times the peak hourly demand, this equates to an approximate annual total of around 60,000 movements. As forecast annual aircraft movements for 2036-37 is considerably less than this, a full

parallel taxiway is not considered necessary within the period of this Master Plan. However, an extended Taxiway F provides Code B and Code C aircraft access to the commercial development precincts located adjacent to it, therefore, parallel taxiway development has been included within this Master Plan. In light of there currently being sufficient space to accommodate a full length Code C parallel taxiway, the area and clearances required for this should be safeguarded for the future development of the airport.

### Runway Option 1

Existing Taxiway F forms a partial parallel taxiway between the Runway 12 threshold and Taxiway A. Separation between Taxiway F and the centreline of existing Runway 12/30 is approximately 157 metres, approximately 11 metres less than the minimum separation required for Code 4C precision approach operations. Use of Taxiway F is currently limited to Code B aircraft below 5,700kg due to the poor condition of the pavement in this area. Based on this it is anticipated that Taxiway F will require significant strengthening and even reconstruction to ensure this taxiway is fully operational. At this time the taxiway centreline should be realigned to the south to allow Code 4C precision approach operations to be undertaken on Runway 12/30, should they be required in the future.

This Master Plan proposes that Taxiway F will provide access to the proposed Aircraft Maintenance Precinct, Air Cargo Precinct, Aviation-related Commercial Precinct and the Private Hangar Precinct. Therefore, Taxiway F should be upgraded to accommodate Code C aircraft between Taxiway A and the end of the north western end of the Air Cargo Precinct. Beyond this to the Runway 12 threshold land should be safeguarded to accommodate a full length Code C parallel taxiway. However, in the first instance this section of Taxiway F may only need to be constructed to accommodate Code B aircraft.

The existing connector between Taxiway F and the Runway 12 threshold should be maintained and strengthened to accommodate Code B aircraft. This has been redesignated as Taxiway H for the purpose of this Master Plan.

The Master Plan also proposes that Taxiway F be extended from Taxiway A to Taxiway B. This will provide taxiway access between Runway 30, the east GA area and the west GA area without the requirement for aircraft to use Taxiway C. This will aid the current issues experienced from the cross-over of aircraft on the RPT apron. This section of Taxiway F should be constructed to accommodate Code C aircraft as this will provide the main access for RPT aircraft to the Runway 30 threshold. Some rearrangement of the east GA apron will be required as this section of Taxiway F and the associated strip will encroach on to the apron slightly, however, no existing marked parking areas will be affected.

### Runway Option 2

The Master Plan proposes that the parallel taxiway for Runway Option 2 be retained along the same alignment as that proposed for the Runway Option 1 parallel taxiway. Although this results in

a greater than required clearance from the runway centreline for precision approach operations, it ensures that development along the parallel taxiway can commence prior to a runway option being selected.

The development for Taxiway F will be as described for the development of Runway Option 1 with the exception of further extensions at both ends to provide full parallel taxiway access. At the Runway 30 threshold, beyond existing Taxiway B, Taxiway F will follow the alignment of the disused runway before turning to connect with the runway threshold. This section should also be constructed to accommodate Code C aircraft to avoid the requirement for a runway turning node.

#### 5.6.5 TAXIWAY G

Taxiway G currently connects Runway 12/30 to Taxiway F and can accommodate Code B aircraft, however it is currently limited to aircraft of a maximum of 5,700kg. For both runway options, with the upgrade of the existing section of Taxiway F, Taxiway G should also be upgraded and strengthened to accommodate Code C aircraft and provide access to the Aircraft Maintenance Precinct and Air Cargo Precinct. For Runway Option 1, Taxiway G will also require extension of approximately 80 metres from the existing runway to the new Runway 12/30.

#### 5.6.6 TAXIWAY H

The existing Taxiway F connection to the Runway 12 threshold should be strengthened to accommodate Code B aircraft and in Runway Option 2, will be required to be extended by approximately 70 metres from the edge of the existing runway to the new runway. For the purposes of this Master Plan this has been designated as Taxiway H. This taxiway will provide Code B aircraft access between the runway and Precinct 1B (aviation-related commercial development) and Precinct 5 (private hangars).

#### 5.6.7 TAXIWAY I

A new taxiway connection between the extended Taxiway F and the Runway 12 threshold will be required to be constructed. For the purposes of this Master Plan this has been designated as Taxiway I.

#### 5.6.8 TAXIWAY J

Taxiway J is proposed to connect Taxiway F to the new Aviation-related Commercial Precinct to the west and the west GA apron. The taxiway should be constructed to accommodate Code B aircraft and will be required when significant development within the Aviation-related Commercial Precinct occurs

### 5.7 OTHER AIRFIELD FACILITIES

#### 5.7.1 AERODROME RESCUE AND FIRE FIGHTING SERVICES

It is anticipated that EKRA will accommodate a sufficient number of aircraft movements during Stage 2 to require aerodrome rescue and fire fighting services (ARFFS). An ARFFS facility with a

minimum of two vehicles will be required. Drawing B11337A004 at Appendix B indicates the proposed location which should provide response times in line with CASA requirements.

### 5.7.2 BUREAU OF METEOROLOGY WEATHER STATION

For both runway options, it is anticipated that the existing weather station could remain in its current location for some time. The weather station will continue to require at least 100 metre clearance from all obstacles and it is anticipated that this can be achieved for up to the next 10 years. The weather station also has some limitations on obstacle heights within 100 to 200 metres radius. The existing Slingair Heliwork hangars fall within this radius currently (within approximately 200m) therefore it is not anticipated that development within the Aviation-related Commercial Precinct to the west will need to be limited as this is located more than 200 metres from the weather station (within approximately 220 metres). However, the exact requirements for this area should be confirmed with BoM when further planning for this area commences. When the west GA apron requires expansion north, the weather station will likely require relocation to accommodate this as development will infringe on the required 100 metre clearance. The Master Plan proposes that the weather station be relocated to the north of the runway as indicated on Drawing B11337A004 at Appendix B.

### 5.7.3 VISUAL AND NAVIGATIONAL AIDS

#### Runway Option 1

It is not anticipated that the existing Doppler VOR/DME will be required to be relocated even with the accommodation of a 300 metre wide runway strip to accommodate Code 4 precision approach operations. Objects with a height greater than 4 metres should not be located within 150 metre clearance of the facility. There will however be some limitations on any future commercial development that may happen in this area beyond 150 metres radius.

The Master Plan does not anticipate a requirement to relocate the NDB from its current location on the south of the runway. There will be some limit to the aviation-related commercial development proposed surrounding the NDB in this area, however this is not anticipated to have a major impact on development. However, there may be some benefit in the future to relocate the NDB to make maximum and most effective use of the land south of the runway available for commercial purposes, the NDB could be relocated to the north side of the runway along with the weather station. It should be noted however that it is not likely that the co-location of the NDB and the VOR/DME facilities would be possible and the required obstacle clearance of each will still need to be maintained. However, it will be possible to achieve some overlap of the object clearance areas for each facility, there will therefore be some benefit in locating these facilities near each other.

#### Runway Option 2

The development of Runway Option 2 will require the relocation of the VOR/DME from its current location. The Master Plan proposes that it is relocated to an alternative location on the north of the runway as indicated on Drawing B11337A003B at Appendix B. This location will accommodate

the object clearance requirements for this facility, which includes a 150 metres radius clearance of obstacles of more than 4 metres in height. The Doppler VOR also has some limitations on obstacle heights beyond 150 metres to 300 metres which will also be required to be adhered to in relation to any future commercial development of this land.

As with Runway Option 1, there is no requirement to relocate the NDB from its current location on the south of the runway. However, there may be some benefit in the future to relocate the NDB to make maximum and most effective use of the land south of the runway available for commercial purposes, the NDB could be relocated to the north side of the runway along with the VOR/DME and weather station. Although, again, the obstacle clearance requirements will need to be maintained for each of the facilities (weather station, NDB and VOR/DME), it will be possible to overlap the areas that will be required to be kept clear of obstacles to a certain extent, therefore there will be some benefit in locating these facilities near each other. As shown on Drawing B11337A004 at Appendix B, the NDB, weather station and VOR/DME are proposed to be located to an area as far north as possible to ensure the maximum amount of land is left available adjacent to the runway allowing airside access for the proposed future aviation-related commercial development.

Both runway options will require the relocation of the existing wind indicators due to the extension of Taxiway F and the runway and runway strip proposals. For Runway Option 2, the wind indicator required to be located in the vicinity of the Runway 30 threshold may need to be located on the right-hand side (as pilots approach the runway) as insufficient land exists on the left-hand side.

#### 5.7.4 FUEL FACILITIES

It is not anticipated that expansion of the existing lease/freehold areas, currently occupied by Air BP and Shell, located adjacent to Runway end 30, will be required within the next 25 years. Discussion with these fuel providers indicated that sufficient space currently exists within these areas for any expansion of their facilities that may be required.

However, with regard to the future development and expansion of the western GA area, described in Section 3.1.3 and the development of the aviation-related commercial precincts with airside access also to the west, an additional fuel facility (both AVGAS and Jet A1) may be required to the west of the RPT apron to serve aircraft in this area. This will reduce the requirement for aircraft to taxi between the two areas to access fuel. A location on the western edge of the proposed Aviation-related Commercial Precinct has been identified within this Master Plan for this fuel facility and is shown in Drawings B11337A003A and B11337A003B at Appendix B. An appropriate area of aircraft apron will also be required to be developed adjacent to this if the west GA apron has not been extended to this point when the fuel facility is required.

#### 5.7.5 AIRSIDE ACCESS

The existing airside access gates can remain largely unchanged. Depending on the runway option selected and the construction of new airside security fence, airside access and the inclusion of new

gates should be considered carefully at the time depending on the location of the development and the easiest access points.

The Master Plan proposes that the existing automatic Gate 10 should be used to access the private jet apron and visiting GA aircraft on the west GA apron. Pedestrian access should be provided between the visiting GA aircraft parking and Gate 10, along the western edge of the proposed private jet apron. The existing airside gate, providing access to the existing visiting GA aircraft parking on the east GA apron, will no longer be required for this purpose and will be dedicated to providing airside access for depot activities such as mowing.

#### 5.7.6 SECURITY CONTROL

##### Fencing and Gates

Security fencing and the existing airside gates should be maintained to ensure functionality. Security control at the airport should be maintained and developed in accordance with the approved Transport Security Program as required by the Aviation Transport Security Act 2004 and Aviation Transport Security Regulations 2005.

Areas of the existing airside fence have been identified as inadequate by incumbent airlines. These airlines have implemented their own measures to mitigate against the risks which they consider are presented by the current fence particularly when RPT aircraft remain at the airport overnight. However, to ensure that EKRA complies with the security requirements and is as attractive to new airlines and the incumbent airlines as possible, the security fencing and gates should be upgraded where required.

For Runway Option 1, although no land acquisition is required to accommodate any part of the runway, runway strip or runway end safety areas (RESAs), the northern edge of the 300 metre wide runway strip lies adjacent to the existing airport boundary at the north-western end of the runway. A perimeter fence will also need to be accommodated and should be located at sufficient distance from the edge of the runway strip to remain beneath the transitional surface. To achieve this, the fence will be required to be located within land currently owned by the State. This area of land should be acquired to ensure this can be accommodated and the land is protected from any incompatible future development.

##### Passenger and Checked Baggage Screening

Passenger and checked baggage screening is currently carried out at EKRA as required by the existing regulations. The current process also meets the requirements for aircraft greater than 20,000kg Maximum Take-Off Weight (MTOW) operating RPT air services which will come into force on 1 July 2012, as set out in the ATSRs. The recently upgraded passenger terminal includes the necessary process and equipment to undertake this screening. It is anticipated that the security requirements will continue into the future and are likely to become more stringent with smaller aircraft requiring screening. The overall terminal footprint estimated to be required in the future includes the requirement to screen all aircraft using the RPT apron regardless of their size.

## 5.8 STAGED DEVELOPMENT PLAN

The anticipated staging of the proposed aeronautical development concept described in the preceding sections is summarised in the following sub-sections. Development staging is subject to a range of external factors as well as demand. The timing and location of developments as set out below will need to be subject to periodic review and adjustment as a result of these factors. The Master Plan, whilst setting out the optimum long-term land-use arrangement for the airport site, incorporates flexibility to adjust the location and timing of particular developments as necessary to suit specific constraints.

### 5.8.1 STAGE 1 – 10 YEAR PLAN (2021-22)

The key components of the aeronautical concept proposed in Stage 1 are summarised in Table 5. Expected trigger points for implementation of each component are also indicated. On the basis of the anticipated growth and development in aeronautical activities current at the time of the preparation of this Master Plan, Stage 1 development is expected to occur sometime time between 2012 and 2022. Actual development timeframes will depend on a number of factors including the preparation of detailed business cases for each element. Section 10.0 of this report provides details surrounding the recommended implementation plan for the proposed Stage 1 developments.

Table 5: Proposed Stage 1 Development

Proposed Development	Anticipated Trigger
Runway development to Code 4 capability (Runway Option 1 to the maximum length of 2,350 metres) (Runway Option 2 development to at least 2,500 metres)	Operation of A320 / B737 aircraft
Taxiway F extension between Taxiways A and B (Code C) including either: -Runway Option 1: Strengthening of Taxiway B; or -Runway Option 2: Development of a new Taxiway B together with extensions from other existing taxiways.	Immediate
West GA apron expansion including development of Taxiway J	Relocation of visitor GA aircraft parking and development of Precinct 1A (aviation-related development)
Private Jet Apron Development (approx. 2,500 m <sup>2</sup> )	Immediate
RPT Apron Expansion (approx. 2,500 m <sup>2</sup> )	Operation of more than two simultaneous aircraft of A320 or B737 size plus a simultaneous large charter operation
Upgrade existing Taxiway F and G between Taxiway A and the runway (Code C)	Precinct 3 and 4 development (Aircraft maintenance and air freight development)
Pavement strengthening for existing RPT apron and Taxiway A to B737 / A320 capability	Operation of A320 or B737 aircraft
Passenger terminal expansion/reconfiguration (Total Area: 2,000 m <sup>2</sup> to 2500 m <sup>2</sup> )	Operation of more than two simultaneous aircraft of A320 or B737 size

Proposed Development	Anticipated Trigger
Upgrade Taxiway F between Taxiway G and Private Hangar Precinct (Code C)	Development of Precincts 1B and 5 (Aviation-related and private hangar development)

### 5.8.2 STAGE 2 – 25 YEAR PLAN (2036-37)

The key components of the aeronautical development concept proposed in Stage 2 are summarised in Table 6. Expected trigger points for implementation of each component are also indicated. On the basis of the anticipated growth and development in aeronautical activities current at the time of preparation of this Master Plan, Stage 2 development is expected to occur at some point between 2022 and 2037. Actual development timeframes will depend on a number of factors including the preparation of business cases for each element.

**Table 6: Proposed Stage 2 Development**

Proposed Development	Anticipated Trigger
Full Taxiway F development	Further development of Precinct 5 (Private hangars)
RPT Apron Expansion (approx.. 5,000 to 7,000 m <sup>2</sup> )	Simultaneous operation of additional B737-800/A320-200 aircraft
Passenger terminal expansion (Total Area: 4,000 m <sup>2</sup> to 4500 m <sup>2</sup> )	Operation of additional B737-800/A320-200 aircraft
West GA apron expansion (approx. 150,000 m <sup>2</sup> )	Future development of Precinct 1A/Increased GA Activity
Private Jet Apron Expansion /Relocation (5,000 m <sup>2</sup> )	As required/Expansion of RPT apron on to private jet apron

## 6.0 AIRSPACE

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### 6.1 SURROUNDING TERRAIN

The airport elevation is approximately 44 metres (145 feet) AHD. The airport site is relatively flat. The elevation at the threshold of Runway 12 is 43 metres and at Runway 30 threshold it is 44 metres.

The terrain surrounding the airport site penetrates the existing Obstacle Limitation Surfaces (OLS) at several locations. To the northeast, approximately 3.5 kilometres from the airport, the inner horizontal and conical obstacle limitation surfaces (See Section 6.2) are penetrated by a ridge. Kelly's Knob is a significant feature of this section of the terrain at approximately 148 metres AHD, located within the inner horizontal. A hazard beacon is located on top of Kelly's Knob.

To the south east, approximately 6 kilometres from the airport, another ridge running approximately north south along the eastern edge of the Ord River penetrates the conical surface. Approximately, 3.4 kilometres to the west of the airport, terrain also penetrates both the inner horizontal and conical OLS surfaces. A lit mast is located within this area at a height of 112 metres.

As well as the two lit masts mentioned above there is also one located approximately 3.2 kilometres from the aerodrome to the north east, at a height of 132 metres.

### 6.2 OBSTACLE LIMITATION SURFACES

Obstacles on or in the vicinity of an airport, whether natural features or man-made structures, may prevent its optimal utilisation by aircraft through:

- Reducing the runway distances available for take-off or landing;
- Reducing the authorised take-off and landing weights for some aircraft;
- Restricting certain types of aircraft; and/or
- Limiting the range of weather conditions in which aircraft can operate.

The shape and dimensions of the OLS for an airport are determined on a case by case basis and needs to be assessed by CASA to determine its operational impact. No structure located on airport should be allowed to exceed the vertical limits of the OLS unless required to do so to serve its operational purpose.

#### 6.2.1 EXISTING OLS

Runway 12/30 is a Code 3 non-precision instrument runway with a width of 30 metres within a 150 metre wide runway strip. Obstacle limitation surfaces protect take-off and landing and visual circling for this runway. The existing OLS applicable to current operations at EKRA is shown in Drawing B11337A006 in Appendix B.

As mentioned in Section 6.1 the inner horizontal and conical surfaces are penetrated by terrain in a number of locations as well as the lit masts located at the following locations and with the following heights, as included within the AIP ERSA:

- 195 metres (639 feet), 65 degrees and 4000 metres from the ARP;
- 132 metres (433 feet), 57 degrees and 3200 metres from the ARP; and
- 112 metres (368 feet), 265 degrees and 3400 metres from the ARP.

## 6.2.2 FUTURE OLS

With respect to the future OLS, both runway options, including the extension of the existing and the construction of a new runway, will be very similar in terms of the impact of the OLS as they are both on the same alignment and the new runway centreline is located just 105 metres from the existing runway. However, provision is made for the development of Runway Option 1 with respect to the OLS. Therefore provision has been made for an extension to Runway 12/30, with a total length of 2,350 metres capable of accommodating Code 4 precision approach operations. The future OLS is shown in B11337A007 in Appendix B. Once a runway option has been selected the future OLS may need to be revisited.

The existing structure currently located approximately 315m southeast of the existing Runway 30 threshold will penetrate the approach and take-off climb surfaces of both runway options. This structure will require removal to allow the development of either of the runway options. The areas beneath the approach and take-off climb surfaces, as indicated on Drawing B11337A007, are proposed remain development free to protect the OLS surfaces within this area.

The terrain that currently penetrates the inner horizontal and conical surface to the north east, south east and west of the airport will continue to do so, however, these penetrations are not likely to unduly effect operations by the anticipated larger aircraft including the B737-800 or A320-200.

## 6.3 INSTRUMENT PROCEDURES

### 6.3.1 EXISTING PROCEDURES

Current published instrument approach procedures for Kununurra include Distance Measuring Equipment (DME) or Global Positioning System (GPS) Arrivals; a VHF Omni-directional Range (VOR) approach to runway 30; a VOR or Non-Directional Beacon (NDB) approach to circling minima; and Area Navigation (RNAV) non-precision runway approaches based on the Global Navigation Satellite System (GNSS). Published approach minimum descent altitudes (MDA) may be reduced by 100 feet if an accurate altimeter setting is available. The published approach procedures are authorised for use by category A, B C and D aircraft except for the NDB-A or VOR-A procedure and the DME or GPS Arrival procedures which are not available to category D aircraft.

DME or GPS Arrivals are published for inbound tracks of 209 degrees magnetic from waypoint JULUP and Darwin, 070 degrees magnetic from Gibb River, and 113 degrees magnetic from Wyndham. These procedures provide the approach minima set out in Table 7.

Table 7: Existing Procedure Approach Minima

Arrival Track	Minima Cat A & B		Minima Cat C	
	MDA (ft)	Visibility (km)	MDA (ft)	Visibility (km)
209 °M JULUP – Kununurra	1260	2.4	1290	4.0
070 °M Gibb River – Kununurra	1720	2.4	1720	4.0
113 °M Wyndham – Kununurra	1570	2.4	1570	4.0

Two sectors are also published for DME or GPS arrival procedures. The northern sector includes inbound tracks from the north of Kununurra between 110 degrees magnetic clockwise to 290 degrees magnetic, while the southern sector covers inbound tracks south of Kununurra from 290 degrees magnetic clockwise to 110 degrees magnetic. Circling MDA for both sectors is 1,850 feet and minimum visibility 2.4 kilometres for category A and B aircraft, and 1,850 feet and 4 kilometres for category C aircraft.

The VOR RWY 30 procedure provides a straight-in approach to runway 30 with MDA 750 feet and 3.4 kilometres visibility for category A, B, C and D aircraft.

The Non-Directional Beacon (NDB) – A or VOR-A approach procedure provides for descent to circling minima of 1190 feet and 2.4 km visibility for category A and B aircraft; and 1290 feet and 4.0 km visibility for category C aircraft.

The RNAV (GNSS) RWY 12 and 30 procedures provide a straight-in approach MDA of 750 feet with minimum visibility 3.4 km for category A and B aircraft, 4.0 km for category C aircraft and 5.0 km for category D aircraft.

The published approach procedures are adequate for transient weather conditions such as the passage of frontal weather where cloud ceiling and visibility are reduced for relatively short periods. However, the operational benefit of these procedures is limited where reduced ceiling and visibility conditions persist for longer periods.

### 6.3.2 FUTURE PROCEDURES

No specific allowance has been made within this Master Plan for the development of new instrument procedures. However, extension of the existing runway or the construction of a new parallel runway will require review or redesign of all published instrument approach procedures. In order to ensure that future development in the vicinity of the airport does not introduce unacceptable constraints on future instrument procedures that may include precision approaches, it is recommended that suitable future PANS-OPS (Procedures for Air Navigation Services - Aircraft Operations protection surfaces) be developed. Proposed developments can then be reviewed against these surfaces as well as the OLS to ensure future airport operations remain protected.

## 7.0 AIRCRAFT NOISE

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The consideration of the impact of aircraft noise is an important factor in the development of individual Airport Master Plans. An understanding of the noise impact on land adjoining the airport provides valuable information to local government authorities for planning of adjacent land uses. A thorough understanding of both existing and future noise impacts from airport operations is essential to the development of compatible land use zoning in planning schemes around airports. It is also important for the general public to be able to understand possible future noise impacts in a wider sense, to assist individuals in making their own assessment for their acceptability.

The provision, in this section of the Master Plan, of information on projected noise impacts for EKRA, is intended to enable Council to make informed decisions for the development and implementation of future Airport Master Plans and Local Environmental Plans to ensure that:

- Sensitive receptors are located in areas of acceptable aircraft noise;
- The amenity of other surrounding developments is not adversely affected by aircraft noise; and
- Airport operations are protected, in the long-term, from stakeholder conflicts due to the encroachment of inappropriate development into noise affected zones.

### 7.1 THE ANEF SYSTEM

The principal means of assessment of potential aircraft noise exposure at a given site in Australia is based on the Australian Noise Exposure Forecast (ANEF) system. The ANEF system was developed in the early 1980s based on a social survey of the reaction of people around several Australian airports to noise from aircraft. The ANEF combines the effects of the intensity, duration and number of noise events as well as incorporating a penalty for events at night which is illustrated by contours.

The ANEF is intended to be used to guide the long-term decisions of land-use planners about types of compatible development in areas that may be subject to significant levels of aircraft noise in the future. Additionally, the ANEF system is the basis of *Australian Standard AS 2021-2000 Acoustics – Aircraft noise intrusion – Building siting and construction* (AS2021-2000) which provides guidance on the protection of new buildings against aircraft noise intrusion and on the acoustical adequacy of existing buildings in areas near aerodromes.

Although the ANEF system is considered suitable for land-use planning purposes it is not without limitations. The ANEF system is a 'one size fits all' approach to land use planning. The ANEF criteria for acceptable land use are the same whether the land is in the vicinity of a major international airport or a small regional aerodrome without jet aircraft. The system does not take into consideration local conditions, for example an airport on a Greenfield site is treated as one which has already been developed.

The ANEF is a complex metric which combines the effects of loudness, duration and frequency of noise events to develop a measure of the cumulative noise dose. Although a technically complete measure of noise impacts, it does not illustrate noise in a way to which the non-expert can easily relate. Nevertheless, the ANEF remains the only aircraft noise metric for which land use planning guidelines and requirements (as set out in AS2021:2000) have been developed .

## 7.2 NOISE MODELLING SOFTWARE

### 7.2.1 INTEGRATED NOISE MODEL

The ANEF noise contours for SWEK were prepared using the Integrated Noise Model (INM) version 7.0(b). The INM software has been developed and progressively refined by the United States Federal Aviation Administration to enable the estimation of noise impacts around airports resulting from aircraft operations.

The INM calculates noise impacts by applying standard or user defined aircraft flight profiles, performance data and noise curves to the specific runway configuration and flight tracks. Under the ANEF system, the time of day at which operations take place is also factored into the noise computation. This allows for varying sensitivity in people's reaction to noise.

In interpreting the output of the model it should be noted that:

- Aircraft movements are allocated as a day or night operation, defined as being the hours between 7.00 am to 7.00 pm and 7.00 pm to 7.00 am respectively;
- The number of approach and departure operations modelled relate directly to the actual number of approach and departure movements; and
- The INM requires touch and go (TGO) training to be modelled as a circuit, the initial take-off coupled with the final landing, in conjunction with a number of TGO operations (i.e. each INM circuit or TGO corresponds to two aircraft movements).

The model has been constructed to produce the Australian Noise Exposure Forecast (ANEF) metric defined in AS2021:2000.

INM only considers noise from aircraft taking off, landing and in-flight. Ground-based noise, such as that from taxiing aircraft or engine run-ups or that from ground vehicles or equipment is not included in the model, and therefore cannot be represented in the ANEF. Individual developments which have the potential to generate significant ground-based noise, such as engine run-up facilities or the development of a new RPT terminal and apron, should incorporate further, more detailed, studies to provide an assessment of the noise impacts of these proposals. Airport operational matters influencing noise from ground-based sources should be managed in consultation with local residents through a community consultation strategy.

### 7.2.2 TNIP

The Transparent Noise Information Package (TNIP) has been produced by the Department of Infrastructure, Transport, Regional Development and Local Government (DITRD LG) to enable

aircraft noise disclosure information to be rapidly produced for individual airports. The software takes data outputs from INM (discussed in Section 7.2.1) to produce a range of flight path and aircraft movement based noise descriptors or to produce and manipulate conventional noise contours. The 'Number Above' noise contours are produced using TNIP.

### 7.3 AUSTRALIAN NOISE EXPOSURE FORECAST

The ANEF is a contour map based on forecast aircraft movements and is the only contour map under the ANEF system which is intended to have status in land-use planning decisions.

The ANEF has been prepared based on the forecast number of movements to 2036-37 as presented in Section 4.2.2. Overall, the 2036-37 forecast has been estimated to reach approximately 50,000 movements per annum.

Runway Option 2 has been modelled only as this represents the worst case scenario due to its longer length. The runway alignment in both options are the same and Runway Option 2 is located approximately 100 metres parallel and to the north of the existing runway.

The EKRA ANEF contours developed as part of this Master Plan, and included in Appendix B, were submitted for endorsement by Airservices Australia in the manner of endorsement approved by the Minister for Infrastructure, Transport, Regional Development and Local Government on 17 April 2012. The ANEF contours have not yet been reviewed by Airservices Australia, and have therefore not yet been endorsed.

The ANEF shows the significant contours including the 20, 25 and 30 ANEF. In terms of the ANEF contours that are significant under AS2021-2000:

- The 30 ANEF contour extends beyond the airport boundary in the following places:
  - To the south east, largely over land that is proposed to be acquired by this Master Plan for airport use;
  - To the north over agricultural land which is currently uninhabited; and
  - To the northeast, over a corner of a section of land identified for rural residential development in the Kununurra Future Directions (KFD) plan.
- The 25 ANEF contour extends beyond the airport boundary in the following places:
  - To the southeast, largely over land that is proposed to be acquired by this Master Plan for airport use and an area of privately held agricultural land;
  - The south easterly tip of the contour extends beyond the main irrigation channel and slightly onto land that is currently occupied by the existing waste water treatment plant and identified for mixed business and future urban development in the KFD plan;
  - The contour also extends beyond the airport boundary to the north of the runway over currently uninhabited land, part of which is proposed as rural residential in the KFD plan; and

- The contour also extends beyond the airport boundary to the northwest over foreshore reserve land and the Ord River itself.
- The 20 ANEF contour extends beyond the airport boundary in the following places:
  - To the southeast of the airport over an area of land, on the south side of the Victoria Highway, which accommodates an existing borefield, and is identified for potential tourism or residential purposes in the KFD plan;
  - Beyond the main irrigation channel and onto land that is currently occupied by the existing waste water treatment plant and identified for mixed business and future urban development in the KFD plan;
  - To the north over currently uninhabited land. To the northwest of this the contour extends over land that is proposed as rural residential development in the KFD plan; and
  - To the northwest of the airport over land identified as foreshore reserve in the Kununurra Future Directions plan, the Ord River and currently uninhabited land on the opposite side of the river which is also proposed as rural residential development in the Kununurra Future Directions plan.

AS2021-2000 classifies the construction of residential development between 25 and 30 ANEF to be unacceptable, however, the development of hotels, motels and other short-term residential facilities are classified as conditionally acceptable. For construction of new residential developments between 20 and 25 ANEF, AS2021-2000 classifies it as conditionally acceptable, however some people may find that this land is not compatible with residential or educational uses. 'Conditionally Acceptable' means that the relevant aircraft and the required noise reduction should be determined and the aircraft noise attenuation to be expected from the proposed construction should be in accordance with the construction guidelines set out in the document.

Council should incorporate the ANEF contours into their local planning scheme to ensure future development is aligned with airport's forecast development.

#### 7.4 N60 AND N70 CONTOURS

The ANEF system is generally recognised as being the most technically complete description of aircraft noise in use in the Australian context and the ANEF is the only metric recognised under AS2021:2000. However, it is also widely recognised that the ANEF system is not easily translated into the important factors which affect how individuals react to aircraft noise: the number of overflights and the loudness of individual events. This is due to the way the ANEF combines the effects of loudness, duration and frequency of noise events to develop a measure of the cumulative noise dose.

'Number above', or 'N', contours illustrate the average number of events per day louder than a certain sound level. In the case of the N60, this level is 60 Db(A). The single event level of 60 Db(A) is specified in Australian Standard AS2021:2000 as the indoor design sound level for normal

domestic areas in dwellings and 70 Db(A) is the noise level at which conversation is disturbed within a house with the windows open.

Contours such as the N60s and N70s assist the community to better understand the impacts of aircraft noise by giving individuals the ability to interpret aircraft noise based on actual counts of aircraft with a noise profile greater than a certain level over a range of flight paths. The provision of 'Number Above' contours has been recently recommended by Department of Infrastructure, Transport, Regional Development and Local Government (previously the Department of Transport and Regional Services) in a discussion paper entitled *Guidance Material for Selecting and Providing Aircraft Noise Information*.

The Western Australia Environmental Protection Agency recognises this and 'number above' noise contours (N60 and N70 contours) are generally requested by them in relation to any potential rezoning of surrounding land. They have also proven to be a good way to produce a 'whole of airport' picture of single event aircraft noise patterns which is easy for the general public to understand.

N70 and N60 maps for EKRA have therefore been produced and based on 2036/37 forecast traffic, as set out in Section 4.2.2. These are shown in Drawing B11337A008 and B11337A009 respectively at Appendix B.

Drawing B11337A008 shows that areas outside of the airport boundary are expected to experience up to 20 events over 70 Db(A) on an average day (over 24 hours). The majority of this land is currently undeveloped and uninhabited with the exception of an area of the western side of Kununurra town centre which is largely occupied by commercial development. The KFD plan however, proposes a range of land uses for this area including potential residential, medium-density residential, tourism and mixed business. Some consideration should be given to this development in relation to this contour plan.

The single contour on Drawing B11337A009 shows that the area within the contour, immediately surrounding EKRA, is expected to experience 5 events of 60 Db(A) or greater during an average night (19:00 – 07:00). This area is currently largely undeveloped and unpopulated land, however, the KFD plan proposes a range of land uses including, rural residential and tourism-related which should be given some consideration in relation to this contour.

## 8.0 NON-AERONAUTICAL DEVELOPMENT CONCEPT

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EKRA is a key gateway to the East Kimberley, in terms of business, transportation and tourism. The airport is an essential asset providing key links to Perth, Broome and Darwin and provides aeronautical infrastructure for emergency operators, private aviators and based charter operators that serve a large number of tourists as well as Government departments. The airport has significant areas of land available and has the potential to be an even more important contributor to the economic development, community growth and vitality of the area.

Airports with available land that is not required for future aeronautical infrastructure have the potential to generate diverse revenue streams and produce economic generators. Revenue raised through the use of this land can be used to pay for major investments and expenditure growth. The airport also has a wider economic benefit to the area. The airport and the businesses located there employ local people. Furthermore, airports also invest relatively large amounts to meet new requirements, maintain their infrastructure and expand capacity. These investments often comprise both local construction and equipment.

SWEK would like to continue to take advantage of the available land at the airport to develop aviation-related activities and businesses whilst not infringing on the aeronautical requirements of the airport.

### 8.1 KEY BUSINESS ACTIVITIES

During consultation with key stakeholders the following business activities were identified as those that may have the potential to develop at EKRA:

- Aircraft-related businesses including fixed-wing and rotary aircraft maintenance;
- Other businesses with aviation connections including the use of the airport to transport key, high-value components and equipment such as oil and gas equipment servicing;
- Heavy aircraft maintenance/engineering base;
- Private hangar space;
- Air cargo/freight of fresh produce and other commodities;
- Hotel/accommodation on airport for visiting workers such as contractors and consultants;

As businesses begin to locate at the airport, opportunities exist to exploit potential synergies between businesses. For example, the establishment of a light aircraft maintenance business at the airport may attract other charter operators and private aviators to locate to the airport.

### 8.2 PROPOSED LAND-USE AND PRECINCTS

Based on the key business activities, a number of development land-use types have been identified. Five development land-use types have been identified, these are:

- Land-use Type 1: Aviation-related commercial development with airside access;

- Land-use Type 2: Aviation-related commercial development without airside access;
- Land-use Type 3: Aircraft Maintenance/Engineering;
- Land-use Type 4: Air Cargo/Freight; and
- Land-use Type 5: Private hangars.

To accommodate the proposed development types, a number of precincts have been identified based on their specific requirements, likelihood, timing, synergies with other activities at the airport and the available land. A total of eleven potential development precincts have been identified within the existing airport boundary. These are:

- Precincts 1A, 1B, 1C and 1D: Aviation-related Commercial Precincts (with airside access);
- Precincts 2A, 2B; 2C and 2D: Aviation-related Commercial Precincts (without airside access);
- Precinct 3: Aircraft Maintenance/Engineering Precinct;
- Precinct 4: Air Cargo/Freight Precinct; and
- Precinct 5: Private Hangar Precinct.

The precincts and proposed land-uses within each are indicated in Drawings B11337A003A /B11337A003B and B11337A004 at Appendix B. With the exception of the runway development all other proposed development is the same in Drawings B11337A003A and B11337A003B

The proposed precincts and land-uses are those that have been identified during the preparation of this Master Plan, through the stakeholder consultation process, considering both the major demand sectors. Demand for particular land uses is the key factor which will determine the extent to which the proposed development may occur.

SWEK should consider carefully how the available land will be made available to developers. Council has the option to lease or sell freehold the individual sites. It should be considered that the lease of sites within the airport boundary is generally favourable on the basis that SWEK maintains long-term control of the land. Several sites held on freehold purchase basis may restrict future plans for the adjacent sites and could reduce the flexibility of the surrounding land. This has occurred, to a certain extent, in the east GA area, where there is no longer scope within this Master Plan to utilise this area for key aeronautical facilities such as the expansion of the RPT apron. Through projects at other regional airports, REHBEIN Airport Consulting has also witnessed regional council's difficulties in developing airports based on past decisions to sell land on a freehold basis.

Whilst the likely desirability to users of a freehold tenure is acknowledged, many of the same benefits in terms of security of tenure and ability to obtain finance can be achieved through a long-leasehold arrangement. SWEK should give careful consideration to the future implications of providing freehold tenure and especially the effective sterilisation of land that may result from a haphazard allocation of freehold sites.

The development of the precincts in an orderly and attractive manner is important in establishing the potential synergies between businesses that locate at the airport and which may in turn attract other businesses to locate there. The building types within each of the precincts should be suitable for the activities that are intended to be undertaken there. For example, the aviation-related commercial precinct with airside access should see the development of suitable aircraft hangars with attached workshops or offices. These structures should be of a suitable quality and standard. SWEK should therefore consider developing a set of guidelines for building and construction on airport land to ensure development undertaken by third parties is of an appropriate standard and quality required by Council.

#### 8.2.1 PRECINCTS 1A TO 1D: AVIATION-RELATED COMMERCIAL PRECINCTS WITH AIRSIDE ACCESS

Precincts 1A to 1D have been identified for the development of aviation-related commercial development and due to their proximity to the airside facilities have been identified as being able to accommodate airside taxiway access to each site within each of the precincts. This key characteristic will drive the type of business or activity which should be developed within these precincts. The development on each of the sites should be specifically aircraft-related and the taxiway access used for airside access by aircraft either accommodated on that site, such as a charter operator or a business with its own aircraft, or by aircraft that require the services provided by the business located on that site, such as aircraft maintenance.

Evidence for the stakeholder consultation suggests that there is potential demand for the expansion of existing charter operations and the attraction of new businesses, such as aircraft maintenance that would be attracted to develop within these precincts. The development of such businesses within these sites will contribute to the attraction of the airport for other similar and supporting businesses.

##### Precinct 1A

Precinct 1A includes the existing west GA area currently occupied by Sling Heliwork's facilities. The Master Plan proposes that Precinct 1 is developed before any of the other precincts with airside access are developed. Airside access should be provided by taxiways that accommodate Code B aircraft which will provide access to the taxiway network and Runway 12/30.

The size of the subdivided lots should be based on demand and a range of lot sizes should be considered to increase the attractiveness of the location to a range of business types. The lots should however be of sufficient size to accommodate a hangar/workshop and apron suitable for Code A and some Code B aircraft.

It is anticipated that the subdivision of lots extends west from the existing development as indicated on **Drawing B11337A003 at Appendix B**. The existing Department of Defence (DoD) lease areas lie within the area that this precinct is proposed to expand into. The DoD lease is due to expire in 2022. It is recommended that the DoD lease is not extended beyond this and the DoD facility

removed to allow expansion of Precinct 1A, subject to actual demand for commercial lots at the time. It is not anticipated that this area will be required until towards the end of Stage 1, which is approximately 2021-22.

Road access to this precinct would be via the existing Cyril Kleinig Drive.

### Precinct 1B

Precinct 1B consists of the existing east GA area which currently accommodates Alligator Airways, Shoal Air and the St. John's Ambulance Patient Transfer Facility, among other activities. The majority of the existing lots within this precinct are held on a freehold basis and SWEK does not have any control over the activities that are undertaken here. All available lots have been sold but two remain undeveloped. Stakeholder consultation indicated that the current owners wish to develop the lots for aviation purposes and one of these may be leased to another charter helicopter operator. There is no land available for any additional lots.

Stakeholder consultation indicated that there is insufficient car parking for the customers of the business in this area during the busiest periods. The area between the southern-most edge of the lots and the highway reserve could be formalised into a car parking area to provide additional capacity. SWEK will need to determine its stance on the provision of parking infrastructure for freehold businesses. Access to this precinct should be via the most easterly connector road between the highway and Dusty Rankin Drive to separate GA-related traffic from passenger terminal traffic.

### Precinct 1C

Precinct 1C is located adjacent to existing Taxiway F. It provides a prime location for any aviation-related business requiring airside access. Airside access to this precinct is proposed to be provided by an upgraded Taxiway F which should accommodate Code B aircraft. The lots should be of sufficient size to accommodate a hangar/workshop and apron suitable for Code A and some Code B aircraft.

Road access to this precinct could initially be via Cyril Kleinig Drive and the internal access roads developed as part of Precinct 1A. At a later date, when demand exists, the existing unsealed road that provides access to the speedway and western areas of the airfield, could be upgraded to provide an additional access point to this area.

### Precinct 1D

Precinct 1D is located on the north side of the runway. This area should be developed once capacity within Precincts 1A, 1B and 1C has been exhausted due to the significant works required to provide access to this area. This area may not be required within the next 25 years, however the land should be safeguarded for the future. The development of Runway Option 2 will have a slightly different impact on the extents of this precinct due to the accommodation of the relocated VOR/DME.

## 8.2.2 PRECINCTS 2A TO 2D: AVIATION-RELATED COMMERCIAL PRECINCTS WITHOUT AIRSIDE ACCESS

Precincts 2A to 2D have been identified for the development of aviation-related commercial development without airside access. Due to the location and nature of the land in this area, it is not possible to provide airside access. However, a variety of business types exist that have an aviation connection and will benefit from a location at the airport. These businesses will either support the aeronautical activities at the airport, support the aviation-related businesses that own, operate or deal with aircraft or gain some benefit from the proximity to the air services from the airport that can be used to transport people, equipment or component parts being manufactured or maintained.

Evidence for the stakeholder consultation suggests that there is potential demand for the development of businesses that support the mining industry in the local area and the off-shore oil and gas activities through the maintenance and provision of component parts that are highly time-sensitive and valuable. There is also potential for businesses that supply or support the activities in the aircraft maintenance and air cargo/freight precincts which are discussed later in this section.

### Precinct 2A

Precinct 2A includes the existing subdivision to the west of the passenger terminal area. This currently accommodates a range of activities and it is anticipated that the subdivision will continue eastwards encompassing the existing site of the decommissioned Mobil facility once the remaining equipment has been removed. Lot sizes should be developed as demand requires but sufficient area should be provided to accommodate a workshop and associated facilities. It is anticipated that parking will also need to be accommodated within each of the subdivision lots. However, if this is not the case SWEK should consider the provision of a dedicated car park or designated on-road parking within the precincts to ensure access roads are not blocked by parked cars. One of the subdivided lots could be identified for parking. This precinct will be accessed via Cyril Kleinig Drive

### Precinct 2B

Precinct 2B is located to the west of Precinct 1A and south of Runway 12/30. Its location south of Precinct 1C and the Air Cargo/Freight and Aircraft Maintenance Precincts means that airside access is not possible. It is anticipated that this precinct would be developed once capacity has been exhausted in Precinct 2A. It is anticipated that the DoD facility currently located within this area will be removed prior to demand for development in this area.

Road access to this precinct could initially be via Cyril Kleinig Drive and the internal access roads developed as part of Precinct 1A. At a later date, when demand exists, the existing unsealed road that provides access to the speedway and western areas of the airfield, could be upgraded to provide an additional access point to this area.

## Precinct 2C

Negotiations have already begun to acquire the area of land identified as Precinct 2C from the State of WA. This land has both the advantage of its location at the airport as well as the commercial benefits of the passing traffic. Due to its easy access to the town centre and the airport services, this precinct may provide a suitable location for the development of a hotel or other type of accommodation. Evidence suggests that there is a shortage of accommodation for visiting workers to Kununurra including contractors and consultants. Although this is not specifically aviation-related there are synergies in locating this type of development at the airport, particularly in that a large proportion of those people staying within this accommodation will arrive and depart Kununurra via the airport.

This location would also lend itself to the development of a fuel station due to its proximity to the highway. The number of rental cars picked up and dropped off at the airport is expected to increase with passenger traffic growth resulting in increased demand for such a facility.

Access to this precinct will also be via Cyril Kleinig Drive or directly from the highway depending on the type of development and its access requirements.

## Precinct 2D

Precinct 2D is currently outside of the airport boundary within an area of Crown land. Although there is no current indication for the demand for this land it should be safeguarded now for potential aviation or non-aviation related development that could be sited at this location.

### 8.2.3 PRECINCT 3: AIRCRAFT MAINTENANCE PRECINCT

Precinct 3 has been identified in the location as indicated in **Drawing B11337A003A** and **Drawing B11337A003B** at **Appendix B** to provide efficient and easy access to the runway and other airport facilities for heavy aircraft maintenance activities that may develop at the airport. Stakeholder consultation has indicated that there is potential demand for a heavy aircraft maintenance facility from Skywest. Precinct 3 would provide a dedicated area for this purpose with room for expansion parallel to the runway. Sufficient area exists within the area identified for Precinct 3 in Stage 1 for at least two maintenance hangars (approximately 68 metres by 60 metres each), east of the Department of Defence lease areas, capable of accommodating a A320-200 or Boeing 737-800 aircraft. In Stage 2, if demand existed for additional hangar space, the Department of Defence lease areas would need to be removed or alternatively development can occur on the north western side of the lease areas, identified as Precinct 4: Air Cargo/Freight Precinct. The use of Precinct 3 could be interchangeable with Precinct 4 depending on demand. It is anticipated that the DoD facility currently located within this area will be removed prior to demand for development within this area.

Road access to this precinct would be via Cyril Kleinig Road and the internal access roads developed to access Precinct 1A. If development occurs within Precinct 3 prior to the full

development of Precinct 1A, the appropriate road access will need to be developed through the area identified for Precinct 1A.

#### 8.2.4 PRECINCT 4: AIR FREIGHT PRECINCT

Precinct 4 has also been located to provide efficient and easy access to the runway and other airport facilities for aircraft operating dedicated air freight operations.

The far north west of this precinct can be developed prior to the removal of the Department of Defence lease areas. Depending on demand for each of the activities, the use of Precinct 3 could be interchangeable with Precinct 4 depending on demand.

Road access will also be provided via Cyril Kleinig Road and later by the existing unsealed speedway access road.

#### 8.2.5 PRECINCT 5: PRIVATE HANGAR PRECINCT

Precinct 5 is proposed to provide private hangar accommodation for private aviators wishing to store their aircraft at the airport. This specific location has been identified for this development as it does not have some of the more attractive features that will stimulate demand from commercial activities such as proximity to other airport facilities and highway access. Private aviators do not generally possess these specific demands and Precinct 5 provides efficient airside access to Runway 12/30.

Road access to Precinct 5 will need to be via the existing unsealed road that provides access to the speedway and the western airfield areas. This road would not necessarily need to be sealed in the first instance, just upgraded to ensure access can be maintained during the wet season. Once suitable demand exists the road could be sealed.

#### 8.2.6 DEPARTMENT OF DEFENCE LEASE AREAS

Although unlikely to be required within the next 5 to 10 years, the Department of Defence radar beacon facility will require relocation in the future to accommodate expansion of the aviation-related commercial precinct and aircraft maintenance precinct. The existing DoD lease expires in 2022. It is recommended that the actual demand for commercial development in the area at the time is reviewed and if required the lease should not be extended. There is no known specific requirement for the radar beacon to be located at the airport. As negotiations with the Department of Defence regarding the potential relocation of this equipment in the future may take some time, these discussion should commence on the adoption of this Master Plan,

#### 8.2.7 DEPOT

##### Airport Depot

It is anticipated that the existing depot facility currently located within the powerhouse should be relocated to its own facility. The old Flight Service Building located to the southeast of the passenger terminal building is proposed to be redeveloped for other purposes including offices.

This building would provide sufficient accommodation for an airport maintenance personnel staff room and Bureau of Meteorology weather recording facilities. A storage area for frequently used depot equipment, such as mowers, could also be located adjacent to the building. This location is located in close proximity to airside and provides convenient airside access. More heavy duty machinery that is less frequently used should be located at an alternative location, possibly co-located with the town depot at another site on the airport.

### **Town Maintenance Depot**

The potential for the relocation of the town depot to the airport site is being considered by SWEK. It is considered that some benefits could be gained through the co-location of the town and airport depots. The nature and size of the land required for the town depot does not lend itself for location on airport land, which could otherwise be developed for aviation-specific purposes. An area of available land does however exist to the far west of the airport site and could accommodate the depot if required, this is indicated on Drawing B11337A003A and Drawing B11337A003B at Appendix B.

SWEK need to investigate and give further consideration to this proposal, to fully understand the potential of this proposal in terms of integrating equipment and personnel.

### **8.2.8 AIRSERVICES AUSTRALIA SATELLITE GROUND STATION**

It is anticipated that the existing Airservices Australia Satellite Ground Station (SGS) equipment will continue to be required and will remain in its current location. Opportunity may exist to condense the lease area to the maximum that is required by Airservices Australia for this equipment and the surrounding area could more usefully be used for car parking.

### **8.2.9 POWER HOUSE**

The relocation of the depot facilities currently located at the Power House to the old Flight Services Building will allow for the existing powerhouse equipment to remain in its current location but for the building to be consolidated and the surrounding structure reduced to only that necessarily required. This will allow for this area to be used for car parking as required in the future.

### **8.2.10 OLD FLIGHT SERVICES BUILDING**

Plans to refurbish and develop the old Flight Services Building for commercial purposes should go ahead. Some of the building should be retained for a staff room for airport maintenance employees and the BoM measurement activities that are currently undertaken in the power house. The offices should be leased as they may be required at a later date to accommodate other airport-related activities administration activities if capacity does not exist within the passenger terminal building.

An area outside the building should also be retained for the construction of a small shed to accommodate the airport depot equipment including that used for mowing. This location provides ease of access to airside via an airside gate in this location.

## 8.2.11 ENGINEERING SERVICES

### Electricity

The electricity supply to the airport will need to have sufficient capacity to support the airfield systems, passenger terminal and non-aeronautical development proposed. Indications show that the existing electricity supply is not sufficient for current activities and therefore will certainly require upgrade within Stage 1. The exact timing of this upgrade will largely be driven by the rate of development within the non-aeronautical precincts. It is suggested that all airport facilities and development located on the airport site is connected to the town electricity supply.

The existing switching gear and generators can be retained within the existing powerhouse following consolidation of this building.

### Sewer

The development of the passenger terminal in Stage 2 will require the relocation of the septic system to an alternative location which is suggested to be to the west of the passenger terminal building. The septic system will also require upgrade to accommodate the increased capacity of the airport terminal. The potential for the terminal to be connected to the sewer system should also be investigated.

A toilet dump should also be developed for the use of overnight RPT and charter aircraft using the airport. No facilities are currently available for these aircraft during the night.

Suitable septic systems should also be developed for the non-aeronautical development precincts.

## 8.3 LANDSIDE ACCESS AND CAR PARKING

### 8.3.1 ACCESS ROADS

The recent redevelopment of the passenger terminal and the previous construction of a new car park has resulted in a confusing access road layout. A detailed review of the existing airport access and car park road markings is required to ensure that landside access is efficient and not confusing to the driver. As the car parking is developed at the airport, the access roads should be upgraded and rearranged accordingly.

The Master Plan proposes that both arms of Laine Jones Drive, which provide the main access points from the Victoria Highway, are maintained to provide dedicated access to the passenger terminal building and the car parks. A one-way system should be maintained with the western most access point used as entrance only and the eastern most access road used as exit only. Access to and from Laine Jones Drive could be consolidated and minimised through the development of a roundabout at the entrance to the airport from the highway. This should be subject to a more specific traffic study to be undertaken by SWEK.

The existing road that connects both arms of Laine Jones Drive, that lies between the northernmost car parking area and the southernmost car parking area, should be used to provide access to

either side of Laine Jones Drive, within the one-way system, to allow access to the secure long-stay car park and an exit route for vehicles parked to the south west of the passenger terminal. This limits the need for vehicles to use the passenger terminal forecourt area.

To further reduce crossover of traffic at the passenger terminal forecourt, all non-passenger terminal traffic should use alternative accesses. All traffic accessing the east GA area should do so via Dusty Rankin Drive and access this using the most easterly access point from the highway, near the fuel facilities. All traffic accessing the west GA area should do so via Cyril Kleinig Drive to the west of main access roads. Each of the access points should be effectively signposted from the highway and the appropriate line markings implemented.

It is proposed that Cyril Kleinig Drive will provide the main access to the proposed aviation-related precinct to the west of the existing west GA apron. As development expands within this western area and the development of the precincts expand west, including the development of the private hangar precinct, it is proposed that the existing unsealed road that provides access to the speedway as well as the west of the airfield should be upgraded to provide access to these areas.

Access to the relocated VOR/DME (if Runway Option 2 is selected), NDB, weather station and aviation-related development on the north side of the runway is proposed to be via an upgrade to the existing unsealed road that runs along the alignment of the drainage channel to the northeast of the airport site. This is located on private property and negotiations will need to be started to understand the possibility for the development of this access road prior to beginning the process of relocating any of the facilities. The road will initially only be of sufficient quality to provide airport personnel access to the VOR/DME, NDB and weather station. When development commences within Precinct 1D, the road will need to be upgraded to provide suitable access to the commercial activities located in this area.

### 8.3.2 PASSENGER TERMINAL FORECOURT

The Master Plan proposes that a dedicated area for passenger pick-up and set-down is developed on the passenger terminal building forecourt. The existing one-way road that runs parallel to the passenger terminal building currently serves this purpose and can continue to do so however the arrangement should be formalised with dedicated areas marked for buses, taxis and general vehicles. This pick-up/set-down lane should be further expanded as the terminal building expands to the west.

### 8.3.3 CAR PARKING

Car park development is proposed to be accommodated in the area to the south of the passenger terminal building with expansion to the east and west.

#### Rental cars

Demand for rental cars is expected to increase as passenger numbers increase. It is proposed that during Stage 1 the car parking area closest to the passenger terminal building will continue to

accommodate some rental car parking for pick-up and drop-off. It is recommended that approximately 30 spaces be dedicated to rental vehicle parking, approximately half of the spaces currently available within this car parking area. If additional parking is required for rental vehicles, it is recommended that these vehicles be parked within the non-secure, long-stay car park which is described later in this section, and be subject to the parking fees charged within this car park.

In Stage 2, additional rental parking is likely to be required, additional spaces can be dedicated to rental cars within the car parking area closest to the terminal. However, if SWEK wishes to maintain some of this area as short-stay parking then additional rental vehicle parking can be provided in the unsecure long-term car park (explained in more detail later in this section). There is more than adequate space within this area to accommodate both passenger and rental car parking. It is estimated that in addition to the 30 spaces adjacent to the passenger terminal, another 50-100 rental car spaces may be required in this area. SWEK should clearly define their stance on the long-term accommodation of rental vehicles at the airport. However, long-term storage of rental vehicles at the airport should not be encouraged and rental vehicles companies should lease alternative areas within the commercial precincts or outside of the airport boundary for this purpose.

### Short and Long-Stay Parking

As well as the development of the physical infrastructure for parking SWEK need to develop a parking strategy for the airport. The strategy should include the parking products that they wish to offer at the airport along with an effective pricing strategy to ensure that the parking is used as it is intended. The Master Plan proposes that restricted short-stay, secure long-stay, non-secure long-stay parking, rental car and staff parking all be accommodated within defined areas at the airport. The following paragraphs discuss the physical requirements of this parking, however the operational strategy for these developments should be considered carefully prior to implementation.

Based on the high-growth scenario forecast passenger numbers it is anticipated that up to a total of 250 short and long-stay car parking spaces (excluding rental and staff) may be required by 2021-22 and up to approximately 450 spaces (excluding rental and staff) by 2036-37. The development of dedicated long-stay parking will release some capacity in the existing car park for short-stay parking. Until SWEK develops its parking strategy and the parking developments are in place it is difficult to provide the exact timing for the development of additional parking capacity. This should be reviewed once the parking strategy has been set.

### Short-Stay Parking

To accommodate parking over the next 10 years, the area between the passenger terminal and the highway should be allocated as short-stay parking, with the exception of the dedicated rental vehicle spaces. Additional, short-stay parking can later be developed within the existing highway reserve, subject to the development of the town bypass and the degrading of the highway. If additional short-stay parking is required prior to the development of the bypass, short-stay car

parking can be accommodated to the southwest of the passenger terminal building on the area adjacent to the existing power house (following consolidation of this facility). In total, this will provide approximately 150 short-stay car parking spaces. Additional short-stay car parking demand beyond 2021-22 to 2036-37 can also be provided in the area surrounding the power house and the Airservices Australia SGS (following consolidation of the current lease area). These areas together with the area developed in Stage 1 can provide more than sufficient parking spaces to meet short-stay parking demand over the next 25 years. It is proposed that all short-stay parking is to have a time-limit of approximately 2 hours, and this should be enforced to ensure the effectiveness of this parking measure.

### Long-Stay Parking

Based on current usage of the existing car parks, there appears to be an existing demand for long-term car parking also. Activities at other airports and stakeholder consultation also indicate that there is a demand for a secure long-stay car park. It is proposed that parking within this area should be offered at a fee. The Master Plan proposes that the secure car park be located to the south east of the passenger terminal building on the existing grassed area and the existing aerodrome manager's house, which is proposed to be removed. This car park will be fenced, with an automatic boom gate, CCTV and lighting. The fee will be charged through a ticketing system. For regular users an annual parking pass system could be established. It is proposed that approximately 25 to 50 secure, long-stay parking spaces be developed initially with potential to expand the car park at this location to accommodate up to approximately 90 spaces. This is likely to provide adequate capacity during Stage 1 and possibly into Stage 2. If additional capacity is required, an additional secure, long-stay car park could be developed to the southwest of the passenger terminal, south of the existing power house.

An unsecure, long-stay parking facility should also be provided for those passengers that do not desire a secure parking arrangement. A fee could also be charged for this area of parking if this fits with SWEK's parking strategy. The fee charged should be relative to that charged for the secure, long-stay car park to ensure the two parking products are differentiated. It is proposed that this fee be charged through the use of a ticket machine and checks undertaken to enforce this. For regular users an annual parking pass system could be established. The Master Plan proposes that this non-secure long-stay car park is located to the south west of the passenger terminal building between Laine Jones Drive and the power house. It is anticipated that 25 - 50 spaces could be developed immediately with expansion up to 100 spaces in the next 10 years. During Stage 2 this facility can be expanded to the west as required. If additional short-stay parking is required prior to the development of the bypass and the ability to construct additional short-stay parking in the highway reserve, some of this unsecure, long-stay parking could be relocated further west to accommodate short-stay parking at this location due to its close proximity to the passenger terminal.

## Staff Car Parking

The area located between the old Flight Services Building and the airport manager's house would provide a convenient airport staff car park and parking for the new activities to be undertaken within the old Flight Services Building. As the airport grows and staff numbers increase, a new staff car park will need to be developed southwest of the passenger terminal.

## 8.4 STAGED DEVELOPMENT PLAN

The optimum staging of the non-aeronautical development is in part linked to the proposed aeronautical development concept staging plan described in Section 5.8.

### 8.4.1 STAGE 1 (2021-22)

Stage 1 should include the development of infrastructure to accommodate opportunities which are considered likely and which also represent low to moderate development costs.

Stage 1 non-aeronautical development should therefore be concentrated to the east of the passenger terminal building continuing east from the existing development (east of Lots 321, 315, 314 and 313) within Precincts 1A and 2A. Anecdotal evidence suggests that there is demand for expansion of existing facilities of some of the base charter operators and other businesses as well as other commercial demand which could locate in this area.

Stage 1 may also include the development of the Precinct 3, the Aircraft Maintenance Precinct, depending on demand. Anecdotal evidence also suggests that there may also be some demand for development within Precinct 5, the Private Hangar Precinct, within Stage 1.

Stage 1 should also include a car parking review with the development of an airport parking strategy. Based on this the relevant car parks should be developed and access roads upgraded and rearranged accordingly.

Section 10.0 of this report provides details surrounding the recommended implementation plan and next steps for some of the proposed Stage 1 developments.

### 8.4.2 STAGE 2 (2036-37)

Stage 2 should see the development of infrastructure to accommodate opportunities that are considered less certain than those included in Stage 1 and which have a moderate to high cost.

Stage 2 of the non-aeronautical development will include expansion of aviation-related commercial development with airside access within Precinct 1A, if capacity remains, or into Precinct 1C. Due to the likely considerable infrastructure developmental costs required to provide access to the north of the runway, Precinct 1D should only be developed when capacity on the south of the runway has been exhausted and demand exists, this may be beyond 2036/37.

Stage 2 will also include expansion of aviation-related commercial development without airside access within Precinct 2A, if capacity remains, or into Precinct 2B and 2C. Precinct 2D should only

be developed when capacity on the south of the runway has been exhausted and demand exists, this may be beyond 2036/37.

Precinct 3, the Aircraft Maintenance Precinct and Precinct 5, the Private Hangar Precinct should be expanded as required. Precinct 4, the Air Cargo/Freight Precinct, should also be developed as and when demand exists.

Table 8 provides a summary of the non-aeronautical development proposals within each stage together with anticipated trigger points.

**Table 8: Non-Aeronautical Development Staging**

Proposed Development	Anticipated Trigger
<b>STAGE 1 (2021-22)</b>	
Precincts 1A– Aviation-related Commercial with airside access	Immediate
Precinct 2A – Aviation-related Commercial without airside access	Immediate
Precinct 3 – Aircraft Maintenance	Demonstrated demand from interested parties
Long-stay parking (secure and non-secure)	Immediate*
Short-stay Parking Expansion	Parking strategy development and capacity issues within existing short-stay parking*
Existing Access Roads Review/Upgrade	Immediate
Precinct 5 - Private Hangar Precinct	On demand
<b>STAGE 2 (2036-37)</b>	
Precincts 1C– Aviation-related Commercial with airside access	Maximum capacity within Precinct 1A
Precinct 2B – Aviation-related Commercial without airside access	Maximum capacity within Precinct 2A
Precinct 3 expansion – Aircraft Maintenance	Demonstrated demand from interested parties
Precinct 4 – Air Freight & High Service Business	Demonstrated demand from interested parties
Precinct 5 expansion – Private Hangars	Demonstrated demand from interested parties
Car Park Development	On demand

\*Car parking demand is a product of a number variable including the parking products offered, pricing strategies and the unique context of the airport and its passengers. Therefore, it is difficult to highlight precise trigger points for development. SWEK should develop an airport parking strategy which includes parking products and pricing. This will help to define trigger points for future development of parking at the airport.

## 9.0 FINANCIAL PLAN

Indicative order of magnitude costs have been developed for all capital works proposed by this Master Plan in Stage 1 and Stage 2.

### 9.1 INDICATIVE DEVELOPMENT COSTS

The costs have been developed based on REHBEIN Airport Consulting's experience from similar projects at other regional airports as well as unit rates provided in Rawlinson's Australian Construction Handbook 2011. All costs have been adjusted for inflation as appropriate, as well as the impact of the remote regional location with a 1.55 multiplier on Perth rates.

#### 9.1.1 COST ESTIMATES

The indicative costs for each stage of development are presented in Table 9. Costs associated with either Runway Option 1 or 2 have been indicated as such, all other costs apply to both runway options.

Table 9: Indicative Development Cost Estimates

Proposed Development	Indicative Cost (\$ million)
<b>STAGE 1</b>	
Runway Option 1: Extend (to 2,350m), widen (to 45m) and strengthen existing runway (including PAPI and lighting)	17.0
Runway Option 2: New runway (2,500m long by 45m wide) (including PAPI and lighting)	26.3
Pavement strengthening for RPT apron and Taxiway A to A320-2/B737-8 capability	2.9
RPT apron expansion of 2,800m <sup>2</sup>	0.6
West GA apron expansion of 23,500m <sup>2</sup>	3.6
Private jet apron (2,650m <sup>2</sup> )	0.4
Upgrade of existing Taxiways F & G to code C and Taxiway F extension between Taxiway A and B	5.2
Taxiway J (Code B)	0.5
Runway Option 1: Taxiway B strengthening	0.5
Runway Option 2: Taxiway extensions & strengthening	1.3
Passenger terminal upgrade/reconfiguration	1.5
Short-stay parking expansion (approx. 80 spaces)	0.4
Secure long-stay parking (approx. 100 spaces)	0.7
West access road upgrade	0.4
Precinct 1A - Aviation-related commercial with airside access	6.2

Proposed Development	
Precinct 2A - Aviation-related commercial without airside access including access roads and services	1.4
Precinct 3 - Aircraft maintenance including access roads and services	0.4
Precinct 5 - Private hangars including access road and services	2.0
STAGE 2	Indicative Cost (\$ million)
RPT apron expansion of 7,000m <sup>2</sup>	1.4
West GA apron expansion of 57,500m <sup>2</sup>	8.9
Private jet apron expansion of 5,500m <sup>2</sup>	1.1
Runway Option 1: Taxiway F extension to Runway 12 end	1.8
Runway Option 2: Taxiway F extension to Runway 12 end	2.1
Terminal expansion (2,150m <sup>2</sup> )	6.5
Short-stay parking expansion (100 spaces)	0.5
Secure long-stay parking expansion (100 spaces)	0.6
Precinct 1B - Aviation-related commercial with airside access	0.7
Precinct 2B - Aviation-related commercial without airside access including access roads and services	0.4
Precinct 3 expansion- Aircraft maintenance including access roads and services	0.4
Precinct 4 - Cargo	0.4
Precinct 5 expansion - Private hangars	0.6

### 9.1.2 ASSUMPTIONS AND EXCLUSIONS

A range of assumptions and exclusions were made in order to produce the indicative development costs, there are as follows:

- No detailed survey information is available;
- Land acquisition costs have not been included within these cost estimations;
- Development costs for the site identified for the town depot have not been included;
- Costs included for the development of the commercial precincts do not include ground improvements, it is anticipated that this will be carried out by the lessee/owners. Costs for services and access to the subdivided sites have been considered only. However, SWEK may want to consider undertaking the ground work depending on the commercial model being followed;

- It has been assumed that the lessees/owners of subdivided sites within the precincts will provide their own septic tank;
- Services for the new subdivided sites will be connected to the existing services near to the passenger terminal area;
- Relocation costs of the Department of Defence radar beacon has not been included;
- GST has not been included; and
- No allowance for design and management costs or construction contingency has been made.

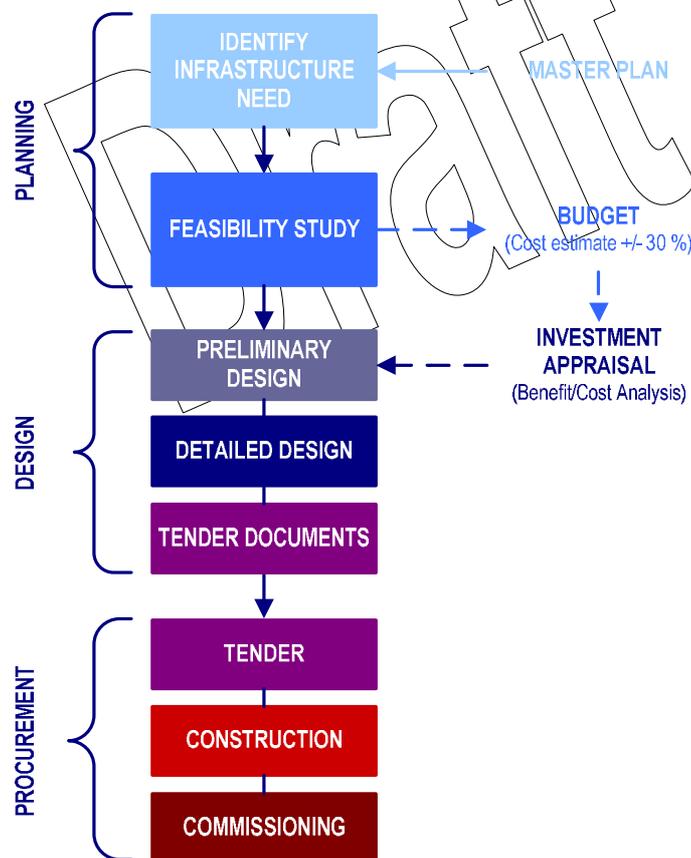
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## 10.0 IMPLEMENTATION PLAN

In order to commence the implementation of this Master Plan it is important to understand the process which will need to be completed. Figure 8 sets out conceptually the infrastructure development process that each development proposed in Section 5.0 and Section 8.0 will need to go through to support the potential future growth of EKRA.

This section discusses the process in more detail in relation to Stage 1 and particularly the developments that are considered to be most pertinent to the development of EKRA. These developments should be first to continue through the infrastructure development process.

Figure 8: Infrastructure Development Process



### 10.1 FEASIBILITY

Based on the aeronautical and non-aeronautical infrastructure development proposals set out by this Master Plan for Stage 1 (to 2021-22), Table 10 highlights the infrastructure developments that are considered to be most pertinent and should immediately continue through the infrastructure development process and more detailed planning/feasibility studies undertaken.

Table 10: EKRA 2012/13 Feasibility Studies

Infrastructure Development	Feasibility Study Elements
Runway development to Code 4 capability	Existing runway pavements study followed by a detailed planning study for the selected runway option
Taxiway F Extension	Taxiway F extension planning study
West GA Area Development	West GA expansion and reorganisation to accommodate relocated GA visitor parking, private jet apron and to include the initial stages of development of Precinct 1A and 2A
Car Parking and Access Roads Upgrade	Airport car parking strategy followed by more detailed estimations of car park space requirements. Access roads should be upgraded in line with the car parking developments with investigation into the requirement for a roundabout at the airport entrance.

An initial stage of planning is required, part of which is formed by this Master Plan which has highlighted the need for these specific infrastructure developments. This now needs to be followed by a feasibility study for each infrastructure development, to include more detailed planning required prior to design and achieve more accurate cost estimates for investment appraisal purposes. The following sections and Table 10 outline the feasibility studies identified to be undertaken following endorsement of the Master Plan. It is anticipated that this 'Feasibility' stage will take approximately one year to complete based on the number of studies, their inter-relation and complexity.

### 10.1.1 RUNWAY DEVELOPMENT

Two runway development options have been included within this Master Plan to ensure that operations at EKRA are not significantly impacted by runway development works in the future. The next stage of planning should include further detailed engineering assessment of the existing runway pavement to understand whether the development of Runway Option 1 is feasible without significantly impacting operations. The additional benefits of Runway Option 2, the development of a new runway, in terms of the additional runway length that can be achieved also needs to be considered carefully against the estimated additional cost.

It should be noted that for either runway options, the full runway length recommended to accommodate 150-160 seat jet aircraft does not necessarily need to be constructed immediately. For example, if the development of the existing runway is selected, the poor condition of the existing runway pavement may necessitate the upgrade of the existing pavement immediately. This can be done without the extension without the extension which can be developed at a later date when demand requires it or funding allows it. Alternatively, for the development of a new runway may only be constructed to replace the existing runway, at a similar length, due to the poor pavement condition. Extensions can be undertaken at a later date when demand requires or when funding allows.

### 10.1.2 TAXIWAY F EXTENSION AND UPGRADE

Once a runway option has been selected for future feasibility/planning studies, planning can commence of the extension and upgrade of Taxiway F.

### 10.1.3 WEST GA AREA DEVELOPMENT

To accommodate the relocation of the visitor GA parking the west GA apron will require extension and reorganisation of the existing parking and taxiways to ensure best use is being made of the available space. At the same time the west GA area should be reviewed as a whole and planning undertaken for the private jet apron and the initial stages of development of Precincts 1A and 2A to ensure that any developments, such as taxiway location, are in line with future proposals for this entire area.

### 10.1.4 CAR PARKING AND ACCESS ROADS

An airport parking strategy should be developed and followed by a planning study for the airport car parking and access road arrangements. This project is relatively unrelated to any of the other planning studies proposed. This study should explore and plan in detail the requirements for long-term, secure and unsecure parking as well as short-term parking arrangements, including those for hire vehicles, and access roads.

### 10.1.5 OTHERS

The remaining infrastructure elements identified for development within Stage 1, highlighted in Sections 5.0 and 8.0, can continue with the planning phase in later years, as funding allows or as operations require. The trigger points can be used as guidance for development of these elements.

## 10.2 DESIGN

Once the planning phase has been completed, the most critical of the infrastructure elements can be identified to move into the next stage of the infrastructure development process. Depending on funding available, all or some of the infrastructure developments included in Table 10 can commence the design process. This will begin with a preliminary design, followed by a detailed design and finalised through the development of the tender documentation to allow the projects to be put out to tender. It is anticipated that this stage will take one to two years to complete and is likely to commence in 2013-14 at the earliest for the most pertinent infrastructure development projects.

## 10.3 PROCUREMENT

Procurement includes tendering the project and awarding the contract for construction to the winning bidder. The infrastructure is then constructed and commissioned. It is likely that the earliest this stage could commence for any of those infrastructure developments included in Table 10 is 2014-15. The length of this stage will depend on the size and complexity of the project.

## 10.4 MASTER PLAN REVIEW

It should be noted that this Master Plan sets out the anticipated infrastructure required over the next 10 to 25 years based on current activities and knowledge. As the commercial and capacity context of the airport is constantly changing, this Master Plan should be reviewed at regular intervals. Master Plans are generally reviewed every 5 years or following any major change in key drivers for airport growth.

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## APPENDIX A

### STAKEHOLDER CONSULTATION SCHEDULE

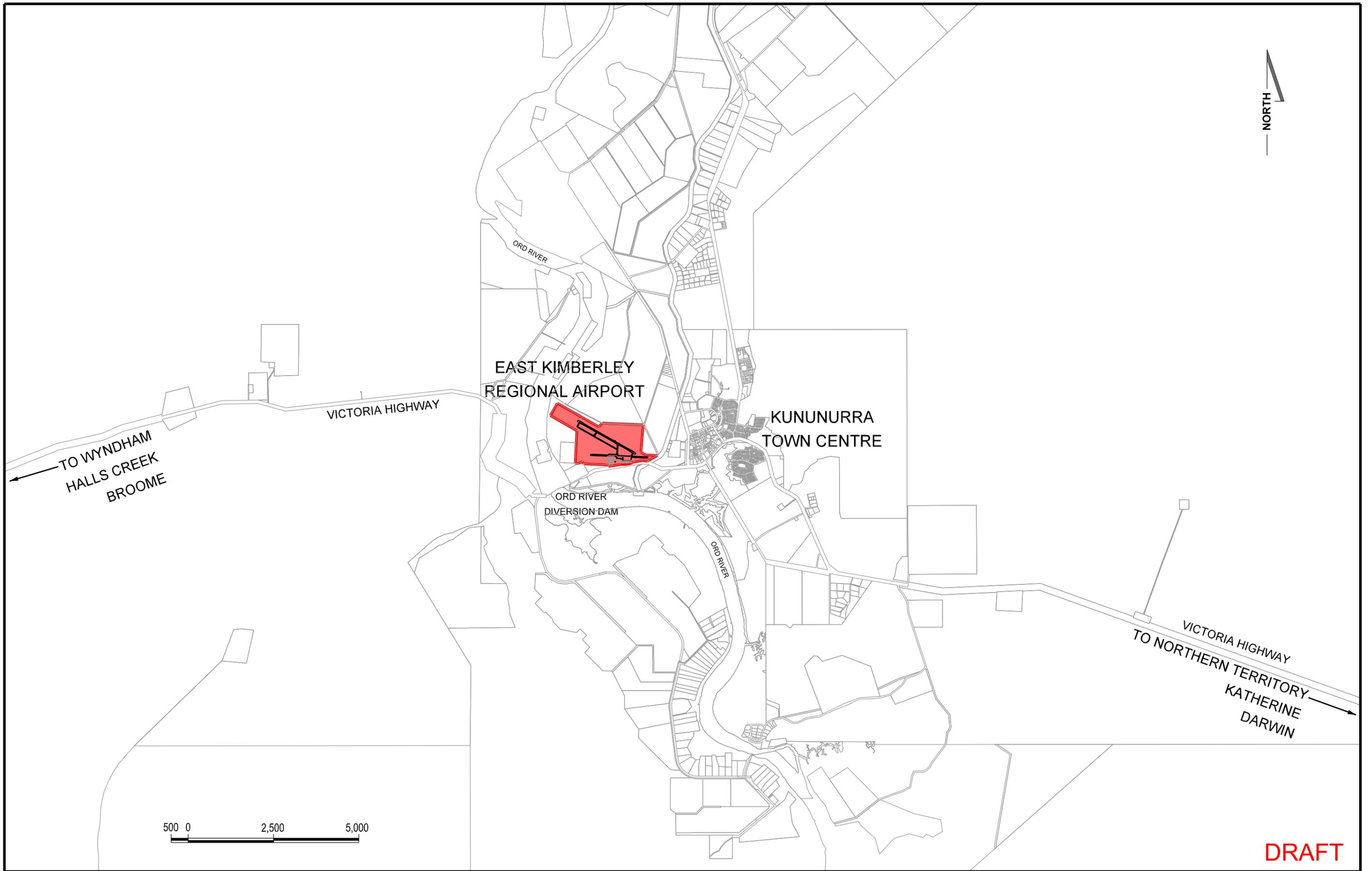
Organisation	Name	Title	Date	Location
Shire of Wyndham East Kimberley	Gary Gaffney	Chief Executive Officer	19 January 2012/21 February 2012	Kununurra
	Cr John Moulden	Shire President	19 January 2012/21 February 2012	Kununurra
	Kevin Hanmagan	Director Infrastructure	19 January 2012	Kununurra
	Nick Kearns	Director Development Services	19 January 2012/21 February 2012	Kununurra
	Jo-Anne Ellis	Director Corporate Services	21 February 2012	Kununurra
	Katya Tripp	Strategic Planner	19 January 2012/21 February 2012	Kununurra
	Jennifer Ninyyette	Senior Planning Officer	19 January 2012	Kununurra
	Rick Spry	Airport Manager	21 February 2012	Kununurra
	Brian Sargeant	Airport Operations Officer	19 January 2012/21 February 2012	Kununurra
	Janet Takarangi	Economic Development Officer	19 January 2012/22 February 2012	Kununurra
	Peter Kerp	Manager Engineering Services	19 January 2012	Kununurra
Kununurra Chamber of Commerce	Brad Williams	President/ Kimberley First National Real Estate Owner	19 January 2012	Remote
Kununurra Visitors Centre	Nadia Donnelly	General Manager	31 January 2012	Remote
Department of Regional Development and Lands	Peter Stubbs	Director, Ord East Kimberley Expansion, Dept. of Regional Development and Lands	12 January 2012	Remote
Airnorth	David Ranger	Commercial Manager	6 February 2012	Remote
Skywest Airlines	Alan Stuart	Executive General Manager, Commercial	27 January 2012	Remote
Shoal Air	Steve Irvine	Managing Director	19 January 2012	Kununurra
Alligator Airways	Rob Kendrick	Managing Director	19 January 2012	Kununurra
Slingair Heliwork WA	Paul Cripps	Operations Coordinator	20 January 2012	Kununurra
	Brett Hill	Chief Pilot	20 January 2012	Kununurra
	Sonja Mitchell	Acting General Manager Slingair	14 February 2012	Remote
Avis	Leigh-Anne Adams	Branch Manager	20 January 2012	Kununurra

Organisation	Name	Title	Date	Location
Budget	Sue Stubbs	Branch Manager	20 January 2012	Kununurra
Thrifty	Nina Mehler & Brett Fargher	Branch Managers	20 January 2012	Kununurra
Kimberley Metals Limited, Sorby Hills Project	Ed Newman	Project Manager	31 January 2012	Remote
Kimberley Metals Group, Ridges Iron Ore Project	David Gallagher	Chief Financial Officer	21 February 2012	Remote
Northern Airport Services	Frank Rodriguez	Managing Director	30 January 2012	Remote
Shell	Peter Devenish	Site Manager	08 February 2012	Remote
Air BP	Gordon Tomkin	Site Manager	08 February 2012	Remote
Lone Eagle Aviation Services	Lance Conley	Owner/Manager	08 February 2012	Remote
Lockheed Martin (Department of Defence)	Paul Semple	Site Management Contractor	01 February 2012	Remote
State of WA	Cody Rampant	Acting Project Officer	31 January 2012	Remote
Airservices Australia	Richard Holding	Site Manager	01 February 2012	Remote
Melon Farm NT Pty Ltd (Thrifty Car Hire)	Rick Paul	Manager	08 February 2012	Remote

Draft

## APPENDIX B

### DRAWINGS



**DRAFT**

Ordinary Council Meeting Public Minutes

No.	Date	By	Amendment	Checked
1	31/05/12	JSS	DRAFT	BJH
0	08/05/12	JSS	DRAFT	BJH

No.	Date	By	Amendment	Checked

**REHBEIN** Airport Consulting

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The Association of Consulting Engineers Australia  
21 August 2012

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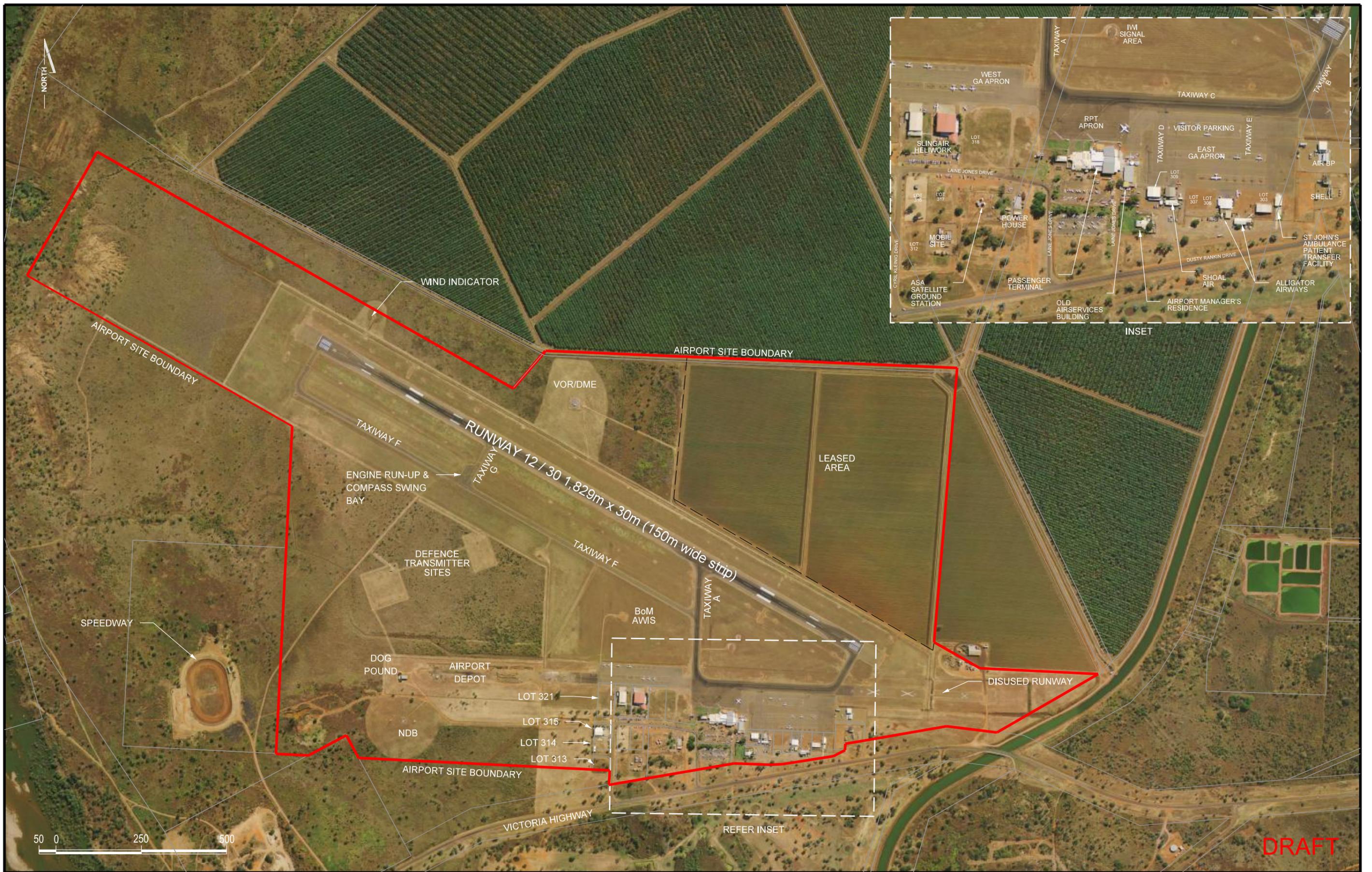
Project: EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN

Title: LOCALITY PLAN

Client: SHIRE OF WYNDHAM EAST KIMBERLEY

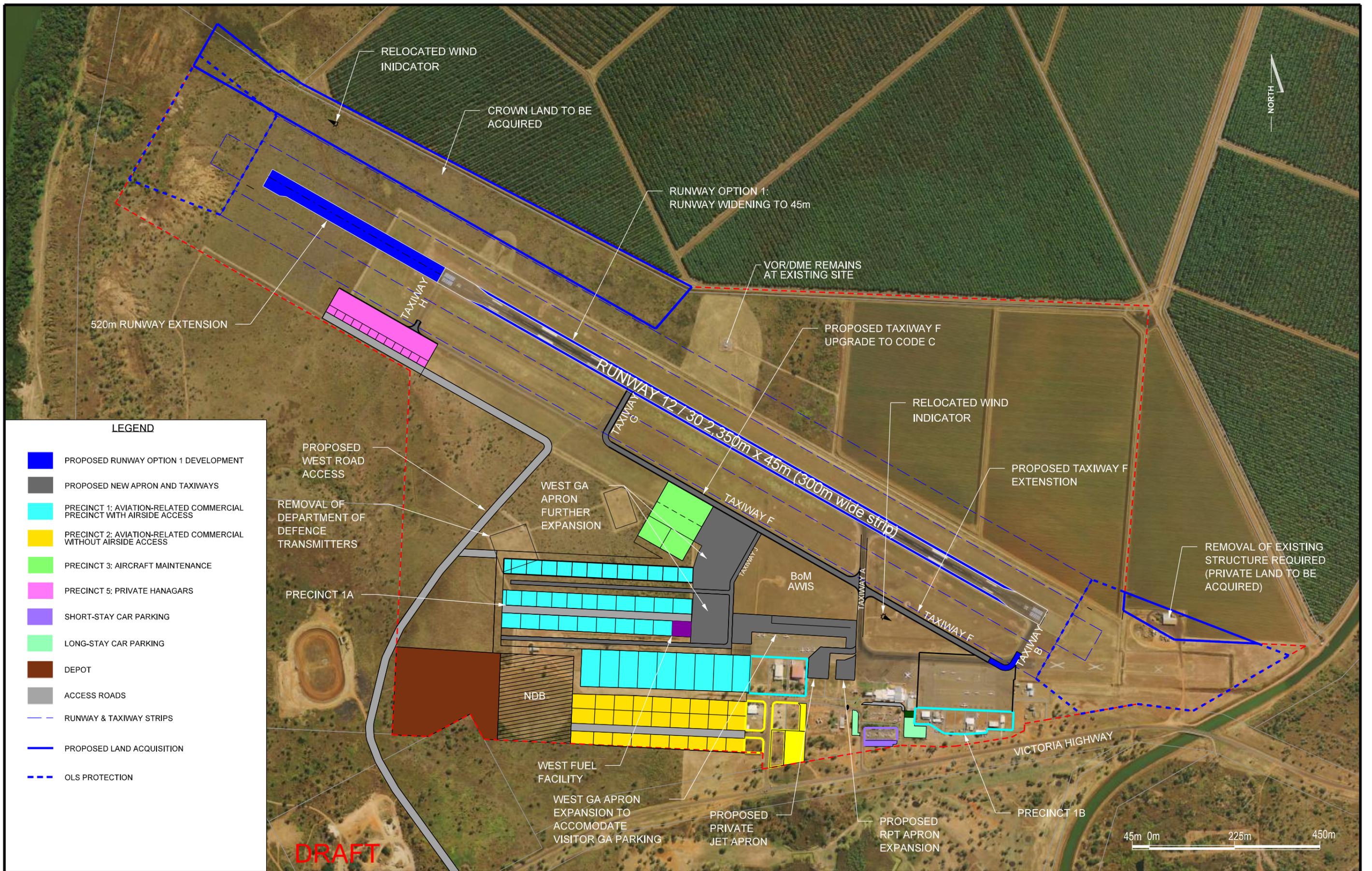
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Designer: JSS	Approved: BJH		
Scale: 1:50,000	Date: 31/05/2012		

0 1 221 of 447



**DRAFT**

						Project: EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN		Client: SHIRE OF WYNDHAM EAST KIMBERLEY					
				CBD HOUSE, Level 3 120 WICKHAM STREET FORTITUDE VALLEY QLD 4006 A.C.N. 126 939 768		Title: EXISTING FACILITIES		Draftsperson: JSS Designer: JSS		Checked: BJH Approved: BJH		Sheet Size: A3 Drawing No.: B11337A002	
						Telephone: (07) 3250 9900 Facsimile: (07) 3250 9001 Email: mail@lar.net.au		Scale: 1:5000		Date: 31/05/12		0 1 222 of 447	
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Ordinary Council Meeting Public Minutes													



**LEGEND**

- PROPOSED RUNWAY OPTION 1 DEVELOPMENT
- PROPOSED NEW APRON AND TAXIWAYS
- PRECINCT 1: AVIATION-RELATED COMMERCIAL PRECINCT WITH AIRSIDE ACCESS
- PRECINCT 2: AVIATION-RELATED COMMERCIAL WITHOUT AIRSIDE ACCESS
- PRECINCT 3: AIRCRAFT MAINTENANCE
- PRECINCT 5: PRIVATE HANAGARS
- SHORT-STAY CAR PARKING
- LONG-STAY CAR PARKING
- DEPOT
- ACCESS ROADS
- RUNWAY & TAXIWAY STRIPS
- PROPOSED LAND ACQUISITION
- OLS PROTECTION

No.	Date	By	Amendment	Checked
0	20/07/12	JSS	DRAFT	BJH

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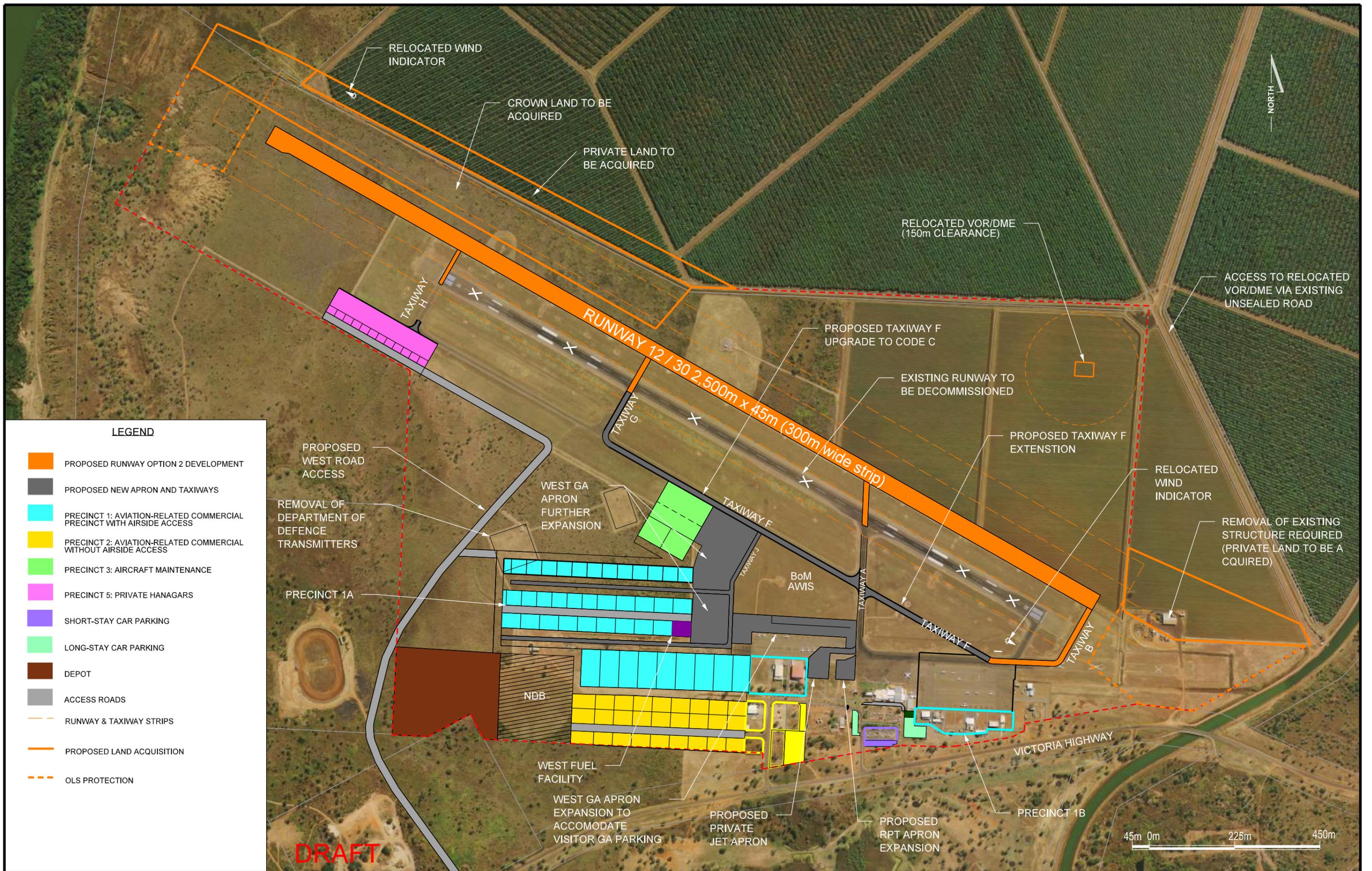
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Title: PROPOSED DEVELOPMENT CONCEPT STAGE 1 - RUNWAY OPTION 1 2012 - 2022

Client: SHIRE OF WYNDHAM EAST KIMBERLEY

Draftsperson: JSS	Checked: BJH	Sheet Size: A3	Drawing No.: B11337A003A
Designer: JSS	Approved: BJH	Scale: 1:4,500	Date: 20/07/2012

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**LEGEND**

- PROPOSED RUNWAY OPTION 2 DEVELOPMENT
- PROPOSED NEW APRON AND TAXIWAYS
- PRECINCT 1: AVIATION-RELATED COMMERCIAL PRECINCT WITH AIRSIDE ACCESS
- PRECINCT 2: AVIATION-RELATED COMMERCIAL WITHOUT AIRSIDE ACCESS
- PRECINCT 3: AIRCRAFT MAINTENANCE
- PRECINCT 5: PRIVATE HANAGARS
- SHORT-STAY CAR PARKING
- LONG-STAY CAR PARKING
- DEPOT
- ACCESS ROADS
- RUNWAY & TAXIWAY STRIPS
- PROPOSED LAND ACQUISITION
- OLS PROTECTION

No.	Date	By	Amendment	Checked
0	20/07/12	JSS	DRAFT	BJH


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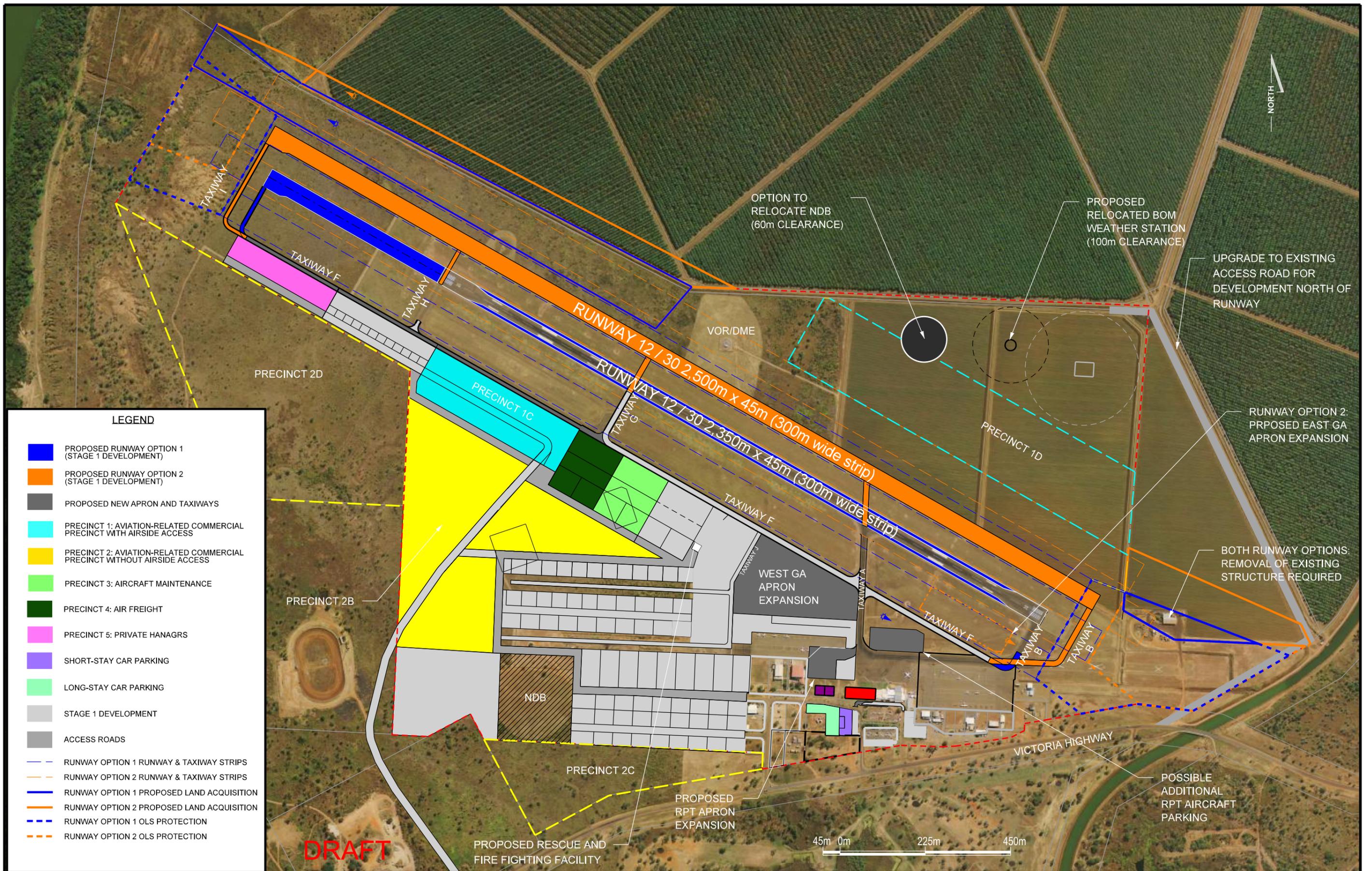
The Association of Consulting Engineers Australia  
21 August 2012

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EMAIL mail@rar.net.au

**Project:** EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN

**Title:** PROPOSED DEVELOPMENT CONCEPT STAGE 1 - RUNWAY OPTION 2 2012 - 2022

<b>SHIRE OF WYNDHAM EAST KIMBERLEY</b>			
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Designer: JSS	Approved: BJH	Scale: 1:4,500	Date: 20/07/2012
		0	224 of 447



**LEGEND**

- PROPOSED RUNWAY OPTION 1 (STAGE 1 DEVELOPMENT)
- PROPOSED RUNWAY OPTION 2 (STAGE 1 DEVELOPMENT)
- PROPOSED NEW APRON AND TAXIWAYS
- PRECINCT 1: AVIATION-RELATED COMMERCIAL PRECINCT WITH AIRSIDE ACCESS
- PRECINCT 2: AVIATION-RELATED COMMERCIAL PRECINCT WITHOUT AIRSIDE ACCESS
- PRECINCT 3: AIRCRAFT MAINTENANCE
- PRECINCT 4: AIR FREIGHT
- PRECINCT 5: PRIVATE HANAGRS
- SHORT-STAY CAR PARKING
- LONG-STAY CAR PARKING
- STAGE 1 DEVELOPMENT
- ACCESS ROADS
- RUNWAY OPTION 1 RUNWAY & TAXIWAY STRIPS
- RUNWAY OPTION 2 RUNWAY & TAXIWAY STRIPS
- RUNWAY OPTION 1 PROPOSED LAND ACQUISITION
- RUNWAY OPTION 2 PROPOSED LAND ACQUISITION
- RUNWAY OPTION 1 OLS PROTECTION
- RUNWAY OPTION 2 OLS PROTECTION

DRAFT

No.	Date	By	Amendment	Checked
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2	31/05/12	JSS	FINAL DRAFT	BJH
1	29/05/12	JSS	FINAL DRAFT	BJH
0	08/05/12	JSS	DRAFT	BJH

No.	Date	By	Amendment	Checked

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Project: EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN

Title: PROPOSED DEVELOPMENT CONCEPT STAGE 2 2022 - 2037

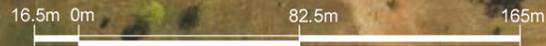
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0 1 26 of 47



**DRAFT**



**LEGEND**

	PROPOSED DEVELOPMENT - STAGE 1 (2021-22)
	PROPOSED DEVELOPMENT - STAGE 2 (2036-37)

No.	Date	By	Amendment	Checked
2	31/05/12	JSS	FINAL DRAFT	BJH
1	29/05/12	JSS	FINAL DRAFT	BJH
0	08/05/12	JSS	DRAFT	BJH

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21 August 2012

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Project: EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN

Title: PASSENGER TERMINAL AREA CONCEPT

Client: **SHIRE OF WYNDHAM EAST KIMBERLEY**

Draftsperson: JSS	Checked: BJH	Sheet Size: A3	Drawing No.: B11337A005
Designer: JSS	Approved: BJH	0	1 of 447
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21 August 2012

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Project: EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN

Title: EXISTING OBSTACLE LIMITATION SURFACE

Client: <b>SHIRE OF WYNDHAM EAST KIMBERLEY</b>			
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Designer: CMF	Approved: BJH	0	227 of 447
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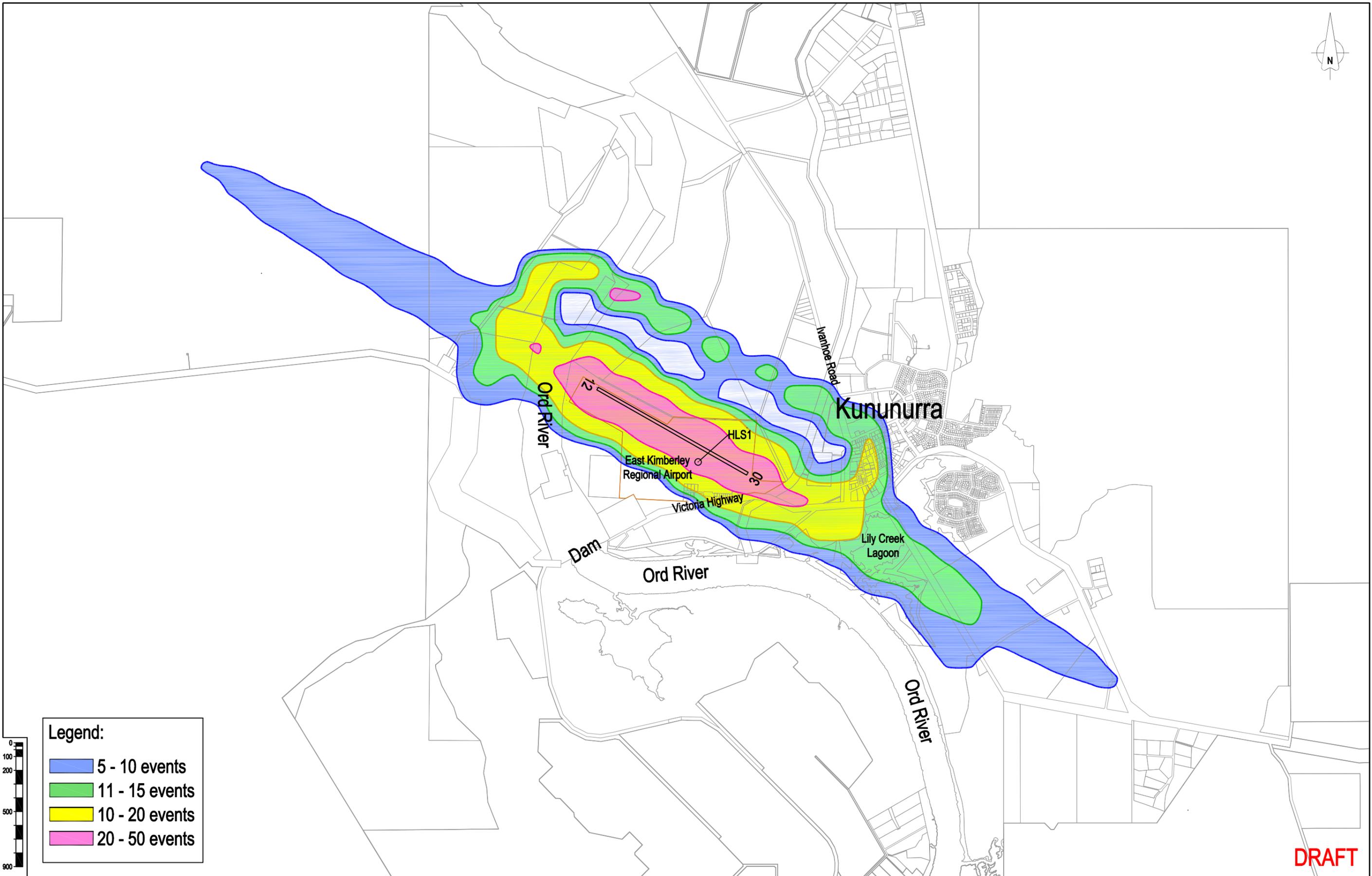

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 21 August 2012

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Project: EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN  
 Title: FUTURE OBSTACLE LIMITATION SURFACE

Client: <b>SHIRE OF WYNDHAM EAST KIMBERLEY</b>			
Draftsperson: CMF	Checked: TAB	Sheet Size: A1	Drawing No.: B11337-A-007
Designer: CMF	Approved: BJH	0	228 of 447
Scale: 1:75,000	Date: 06.05.12		



**Legend:**

- 5 - 10 events
- 11 - 15 events
- 10 - 20 events
- 20 - 50 events

Project:  
**EAST KIMBERLEY REGIONAL AIRPORT**  
**MASTER PLAN**  
 Ordinary Council Meeting Public Minutes

Client:  
 SHIRE OF WYNDHAM EAST KIMBERLEY

Title:  
 2036/37 N70 NOISE CONTOURS (24 HRS)

21 August 2012

**REHBEIN**

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 Fortitude Valley QLD 4006  
 PO Box 112, Fortitude Valley QLD 4006  
 LAR (SEC) Pty Ltd  
 A.C.N. 126 699 788  
 A.B.N. 77 126 699 788

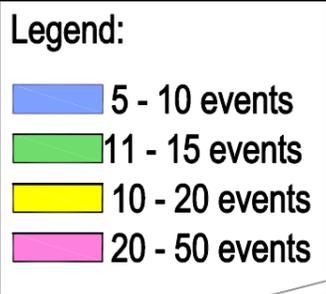
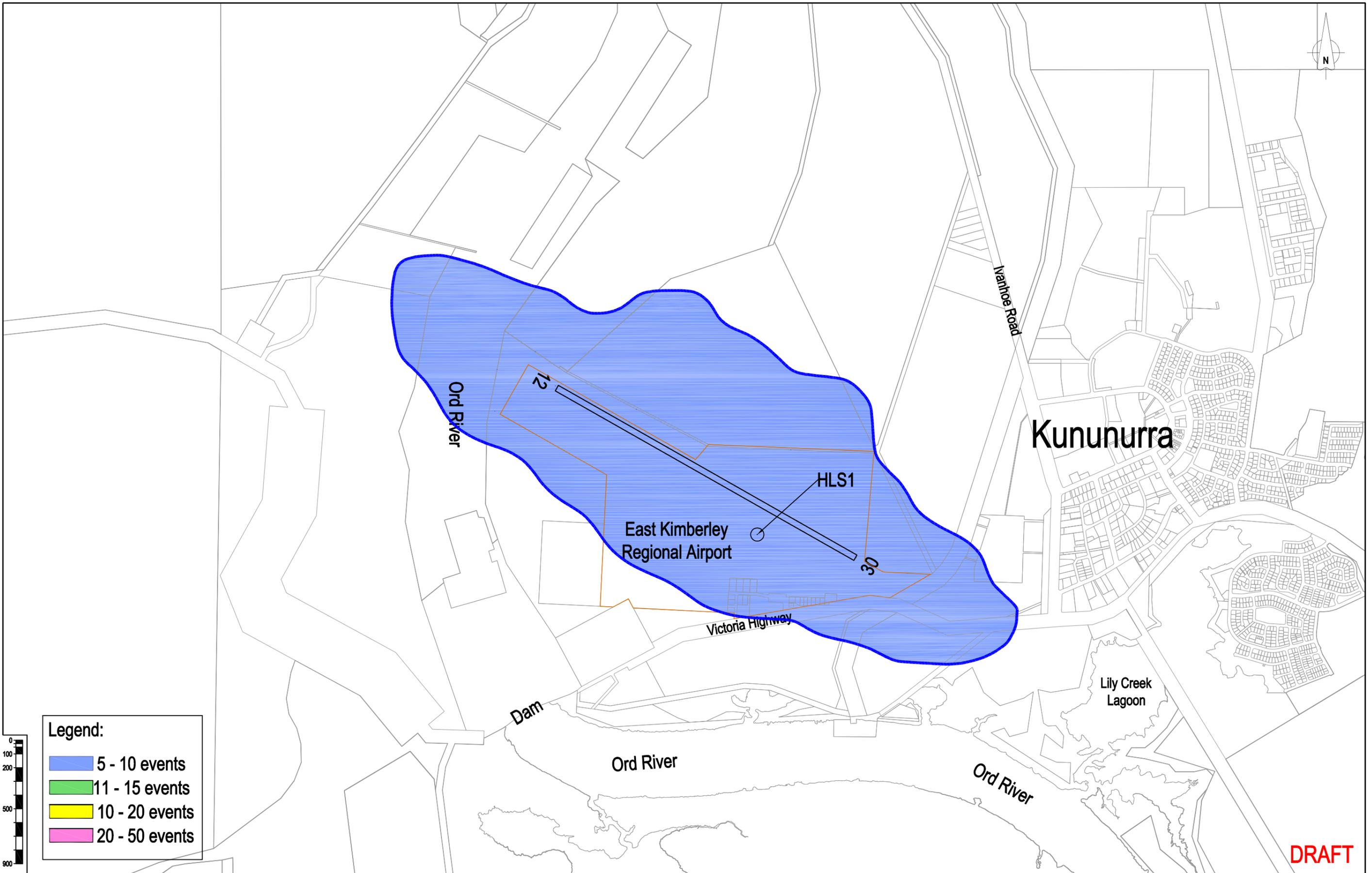
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Drawn: MMW      Checked: JSS      Approved: BJH

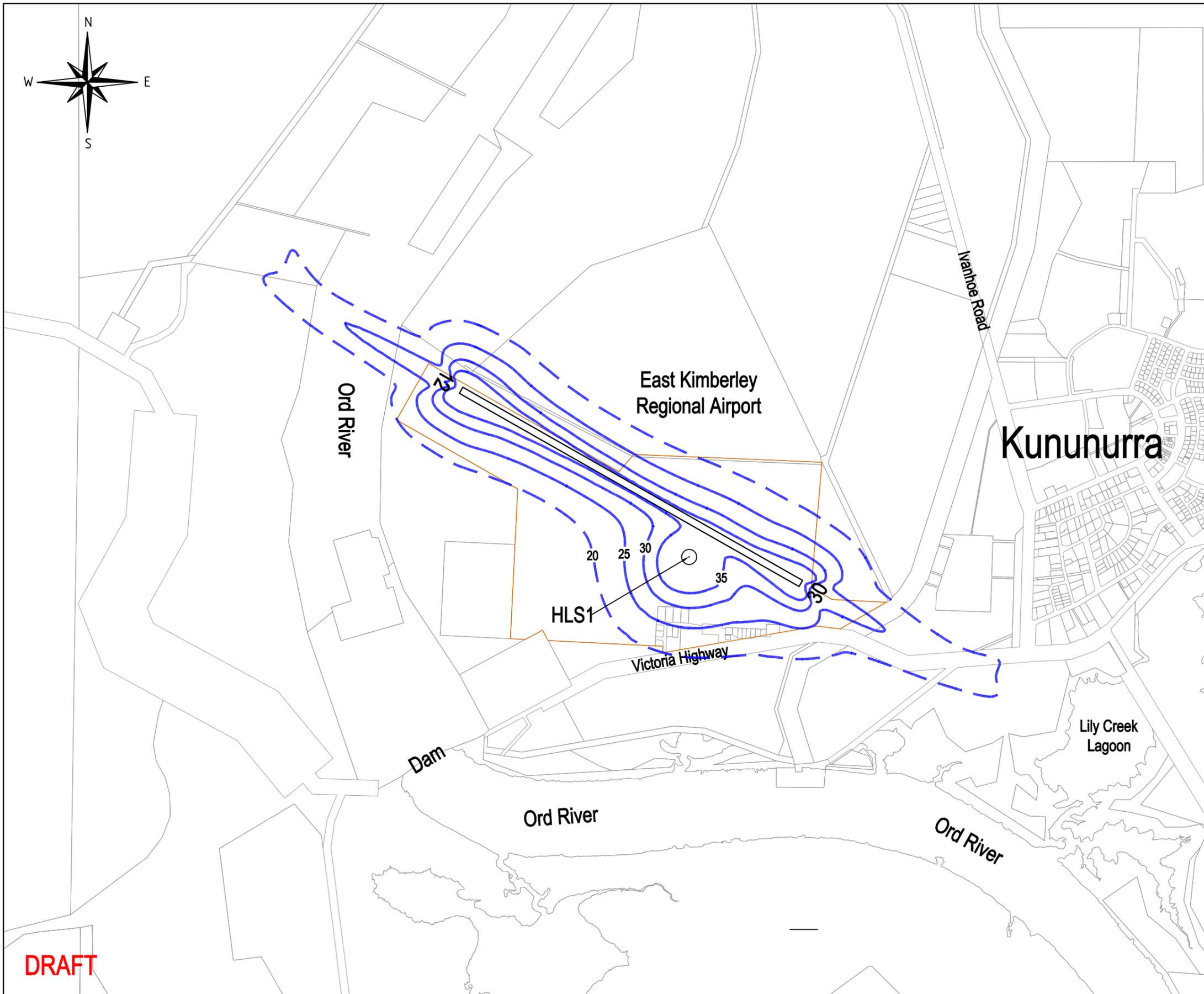
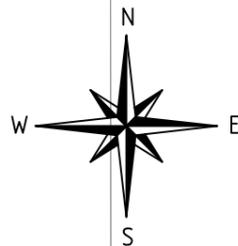
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Rev.	Date	Sheet No.

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<p>Project:</p> <h2 style="margin: 0;">EAST KIMBERLEY REGIONAL AIRPORT MASTER PLAN</h2> <p style="font-size: small;">Ordinary Council Meeting Public Minutes</p>	<p>Client:</p> <p>SHIRE OF WYNDHAM EAST KIMBERLEY</p> <p>Title:</p> <p>2036/37 N60 NOISE CONTOURS (19:00 - 07:00)</p> <p style="text-align: right;">21 August 2012</p>	<p style="text-align: center;"><b>REHBEIN</b></p> <p style="font-size: x-small;">CBD House, level 3 Wickham Street Fortitude Valley QLD 4006 PO Box 112, Fortitude Valley QLD 4006 L&amp;R (SEC) Pty Ltd A.C.N. 126 630 788 A.B.N. 77 126 630 788</p> <p style="font-size: x-small;">TELEPHONE (07) 3250 9000 FACSIMILE (07) 3250 9001 EMAIL <a href="mailto:mail@r.net.au">mail@r.net.au</a></p> <p style="text-align: right; font-size: x-small;">  The Association of Consulting Engineers Australia         </p>	<p>Drawing No:</p> <p style="font-size: large;"><b>B11337A009</b></p> <p>Scale: 1:25,000 @ A3</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 5%;">0</td> <td style="width: 15%;">08/05/2012</td> <td style="width: 10%;">Rev.</td> <td style="width: 10%;">Date</td> <td style="width: 15%;">File Ref: 201337A009R0</td> <td style="width: 5%; text-align: center;">A3</td> </tr> </table>	0	08/05/2012	Rev.	Date	File Ref: 201337A009R0	A3
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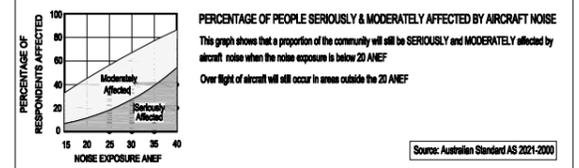


**LAND USE COMPATIBILITY ADVICE FOR AREAS IN THE VICINITY OF AUSTRALIAN AIRPORTS**

Shall be read in conjunction with **AS 2021 - 2000**  
Acoustics - Aircraft noise intrusion - Building siting and construction

Building Type	ANEF zone of site		
	Acceptable	Conditionally acceptable	Unacceptable
Home, home unit, flat, caravan park	Less than 20 ANEF (note 1)	20 to 25 ANEF (note 2)	Greater than 25 ANEF
Hotel, motel, hostel	Less than 25 ANEF (note 1)	25 to 30 ANEF	Greater than 30 ANEF
School, university	Less than 20 ANEF (note 1)	20 to 25 ANEF (note 2)	Greater than 25 ANEF
Hospital, nursing home	Less than 20 ANEF (note 1)	20 to 25 ANEF	Greater than 25 ANEF
Public building	Less than 20 ANEF (note 1)	20 to 30 ANEF	Greater than 30 ANEF
Commercial building	Less than 25 ANEF	25 to 35 ANEF	Greater than 35 ANEF
Light industrial	Less than 30 ANEF	30 to 40 ANEF	Greater than 40 ANEF
Other industrial	Acceptable in all ANEF Zones		

- NOTES:
- The actual location of the 20 ANEF contour is difficult to define accurately, mainly because of variation in aircraft flight paths. Because of this, the procedure of Clause 2.3.2 of the Standard may be followed for building sites outside but near to the 20 ANEF contour.
  - Within 20 ANEF to 25 ANEF, some people may find that the land is not compatible with residential or educational uses. Land use authorities may consider that the incorporation of noise control features in the construction of residences or schools is appropriate. (See Exposure - Response graph below)
  - There will be cases where a building of a particular type will contain spaces used for activities which would generally be found in a different type of building (e.g. an office in an industrial building). In these cases Table 2.1 of the Standard should be used to determine site acceptability, but internal design noise levels within the specific spaces should be determined by Table 3.3 of the Standard.
  - The Standard does not recommend development in unacceptable areas. However, where the relevant planning authority determines that any development may be necessary within existing built-up areas designated as unacceptable, it is recommended that such development should achieve the required ANR determined according to Clause 3.2 of the Standard. For residences, schools, etc., the effect of aircraft noise on outdoor areas associated with the buildings should be considered.
  - In no case should new development take place in greenfield sites deemed unacceptable because such development may impact airport operations.



Category	Item	Value	Unit
RWY	11	100.00	%
	12	100.00	%
	13	100.00	%
	14	100.00	%
	15	100.00	%
	16	100.00	%
	17	100.00	%
	18	100.00	%
	19	100.00	%
	20	100.00	%
	21	100.00	%
	22	100.00	%
	23	100.00	%
	24	100.00	%
	25	100.00	%

GENERAL NOTE:  
1. WHERE FIGURES HAVE BEEN ROUNDED DISCREPANCIES MAY OCCUR BETWEEN TOTALS AND THE SUMS OF COMPONENT ITEMS.  
2. TERRAIN WAS NOT INCLUDED IN THE MODEL.

**AERODROME DATA**  
LOCATION IDENTIFIER: YPKU (WAC 3108)  
MAGNETIC VARIATION - 3° EAST  
ELEVATION - 145 FEET

**AERODROME REFERENCE POINT**  
LATITUDE - S 15° 46.7'  
LONGITUDE - E 128° 42.4'

**COORDINATE SYSTEM**  
AGDM

**INN SOFTWARE:**  
VERSION: FAA INN v7.0c  
DATE OF RUN:  
11 APRIL 2012

SCALE 1: 12,500 @ A1



**DRAFT**

<p>THIS DESIGN AND DRAWING IS UNCONTROLLED. NO PART OF THIS DRAWING MAY BE REPRODUCED, STORED IN A RETRIEVAL SYSTEM OR TRANSMITTED IN ANY FORM WITHOUT THE WRITTEN PERMISSION OF REHBEIN AIRPORT CONSULTING PTY LTD</p>				<p>No. Date By Amendment Checked</p>	
1	11/04/2012	MMW	DRAFT		BJH
0	04/04/2012	MMW	DRAFT		BJH

**REHBEIN**  
Airport Consulting

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ABN. 71128936788

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FACSIMILE (07) 5250 9001  
EMAIL: [ral@rehbein.net.au](mailto:ral@rehbein.net.au)

21 August 2012

Project:  
**EAST KIMBERLEY REGIONAL AIRPORT  
AUSTRALIAN NOISE EXPOSURE FORECAST (ANEF)**

Title:  
**EAST KIMBERLEY REGIONAL AIRPORT 2036/37 ANEF**

Client:  
**SHIRE OF WYNDHAM EAST KIMBERLEY**

Draftsperson:  
**MMW**

Checked:  
**JSS**

Designers:  
**MMW**

Approved:  
**BJH**

Scale:  
**1:12,500 @ A1**

Date:  
**11/04/2012**

Sheet Size:  
**A1**

Figure No.  
**B11382FIG008**

1281 of 447

## East Kimberley Regional Airport Master Plan – Draft Report Stakeholder Consultation – Comments Received

Organisation	Representative	Comments Received	Response
Northern Airport Services	Frank Rodriguez, Managing Director	<p>The following comments are in line with my knowledge while working for Ansett Australia at Kununurra Airport since 1975 and as the current ground handling agent for the Airlines and Charter companies. These comments relate directly to our area of operations.</p> <p><b>4.1.1 – Historical Aviation Activity.</b> Passenger Traffic. Yes the reason for the high annual passenger numbers during 1993 -1996 was due to Argyle Diamonds being at its peak and commuting all their management in and out of Kununurra on a daily charter basis. This was operated by Skywest airlines at the time who had a Twin Otter based in Kununurra. Ansett also had 3 additional direct flights from Perth to Kununurra as well as daily flights via Broome and to Darwin. I think the maximum number of annual RTP passengers in and out of Kununurra reached 86,000 to 90,000 during that time. The rest would have been charter passenger to the Argyle mine.</p> <p>The decline from 1996 – 1999 was due to the gradual withdrawal of all Argyle Diamonds management who were relocated to Perth. This also saw a decline of RPT flights into Kununurra. In 1998 Ansett contracted Airnorth to take on some of these dwindling routes between Darwin Kununurra and Broome with the E120 Brasilia aircraft. This wasn't too popular with the traveling public and a lot of people drove these routes by car. Airnorth continued with the Brasilia once Ansett collapsed in 2001.</p>	Additional explanation for historical passenger traffic patterns added in Section 4.1.1.
		<p><b>5.4.1 &amp; 2 – Passenger Terminal Facilities.</b> My comments are in relation to current and future back office operational requirements which I think is not</p>	SWEK to liaise with terminal stakeholders to ensure immediate requirements are met. Minor

Organisation	Representative	Comments Received	Response
		<p>mentioned in this draft apart from future whole terminal expansion. Any short term changes appears to relate to passenger requirements only. It should be noted that our current working back office space, while being redesigned and refurnished and is workable, is similar or less space than we have had for the past 25 years. The total amount of staff and flight crews currently utilizing this area on most days and during busy flights can reach up to 14. This can be very crowded and will only increase over the coming years. While the airlines are happy to continue paying for the current amount of space, additional space will need to be planned within the near future to allow for current and any increase in operations. The back office currently caters for load control, staff amenities such as staff kitchen, fridges and hot water urns for overnight aircraft, work desks, finance area, stationery cupboards, staff meeting room, Managers office, crew flight planning office, staff safety gear, delayed and rush baggage area, 4 computers and printers, photo copier and radio equipment.</p> <p>Any future terminal expansion to the West should also include staff toilets, showers and lockers separate to the general public areas.</p> <p>Note, I believe similar back office operational space problems “may” have occurred at the Karratha Airport new terminal expansion a few years ago.</p>	<p>reconfiguration of the terminal may be required in the near future. Section 5.4.1 has been updated to reflect this.</p>
		<p>9.1.2 – Assumptions &amp; Exclusions. In reference to your comments that Lessees / Owners of subdivided sites within the precincts will provide their own septic tank. Regardless who pays for it, Immediate planning needs to be considered for a toilet dump to be included for overnight RPT and Charter aircraft.</p>	<p>Requirement for toilet dump noted in Section 8.2.11.</p>

Organisation	Representative	Comments Received	Response
		As mentioned above, these comments are in relation to our back office operational area only and I appreciate that this may not be included in your planning scope. Your comments regarding future GSE and freight areas is on target.	Noted – No action required.
<b>Airservices Australia</b>	Richard Holding – Site Manager	There is a bit of confusion regarding our buildings and equipment.  Drawing B11337A005 correctly shows the location of the Airservices Satellite Ground Station SGS. This equipment is still in use and provides air ground communications on 122.4 MHz and Sita information on 131.55MHz.	Noted – No action required.
		Page 82 section 8.2.8 refers to the Airservices Transmitter, I believe this is a reference to the SGS?	Section 8.2.8, Page 82: Reference updated.
		Page 30 section 3.5.3 second paragraph - when I first read it I thought it was referring to the SGS building mentioned in the previous sentence. I believe the disused building you are referring to is called the Flight Service Building. It may pay to name this building to prevent confusion in the future.	Section 3.5.3, page 30: Building name updated and building context clarified.
	Peter Hay, Aviation Relations Manager, WA	Peter advised that Airservices Australia (AsA) would like to undertake a detailed review of the Master Plan to ensure there are no issues with regard to proposals made that impact Airservices Australia infrastructure as well as ensure that the appropriate systems and navigational aids are also planned for the future by AsA to support the forecast operations at EKRA. This review process generally takes 8 weeks to complete.	Noted. No action required  This is a new approach by Airservices Australia (AsA) to regional airport master plans. It would be beneficial to allow AsA to review this Master Plan with the appropriate context with regard to AsA facility requirements and likely timings. However, given the timing of this request it may be appropriate for the AsA review to be undertaken following endorsement of the Master Plan. AsA should also be

Organisation	Representative	Comments Received	Response
			involved in the detailed planning of any of the facilities that are likely to impact on AsA facilities.
Private hangar owner/tenant	Steve Guerinoni	<p><b>Item 3.5.9 Airport Manager's House.</b> I believe it is still a valuable asset ( demolishing and replacement cost \$ 950,000 ? ) plus it is essential for the Security of the Airport. When a new Airport manager house is built it should be in another location and this house go to another SWEK airport staff member. The removal and cost estimates are not show in Table 5 or Table 9, so hopefully the proposal does not proceed .</p>	<p>Noted. No action required The relative cost benefit of removing the airport manager's house would need to be reviewed as part of the study for the long-term car parking facility which is proposed to be located at this location.</p>
		<p><b>Item 5.5.4 East GA Apron.</b> Perhaps my suggestion is too specific for a Master Plan but , Relocating Gate 7 to the unused pedestrian gate near the large water storage/ fire tanks. This is to create a larger buffer, between visitors and hangers and will make the access point closer to the visitors aircraft bays.</p>	<p>This issue is currently being addressed by another SWEK project. However, the Master Plan proposes that visitor GA aircraft parking be relocated to the western GA apron, therefore there should be little or no demand for pedestrian access via Gate 7.</p>

### Attachment 3 – Table II: EKRA 2012/13 Planning Studies



**Table II: EKRA 2012/13 Planning Studies**

Infrastructure Development	Planning Study Elements
Runway development to Code 4 capability	Existing runway pavements study followed by a detailed planning study for the selected runway option
Taxiway F Extension	Taxiway F extension planning study
West GA Area Development	West GA expansion and reorganisation to accommodate relocated GA visitor parking, private jet apron and to include the initial stages of development of Precinct 1A and 2A
Car Parking and Road Access	Secure and unsecure long-stay parking, implementation of short-stay parking restrictions as well as review of road access arrangements

Draft

### 12.4.3 Ord Surface Water Allocation Plan 2011

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Department of Water
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Jacinta Thompson, Strategic Planning Officer
<b>REPORTING OFFICER:</b>	Nick Kearns, Director Community Development
<b>FILE NO:</b>	GR.09.1

#### **PURPOSE**

For Council to note the release of the draft Ord River Surface Water Allocation Plan and provide a response.

#### **BACKGROUND**

The Ord River Surface Water Allocation Plan is an update to the Ord River Water Management Plan (DoW 2006) and describes how the Department of Water (DoW) will manage water allocation and licensing in the plan area. This includes securing 865GL of surface water for current irrigation and future expansion of the Ord Irrigation area, defining dam release rules and specifying restriction policies for hydropower, irrigation and environmental water during drought periods.

A copy of the Ord River Surface Water Allocation Plan has been previously provided to Councillors.

#### **STATUTORY IMPLICATIONS**

There are no statutory implications associated with this report.

#### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

#### **FINANCIAL IMPLICATIONS**

There are no financial implications associated with this item.

#### **STRATEGIC IMPLICATIONS**

This report aligns with Council's focus on Economic Development, Key Result Area 3, in and Governance, Key Result Area 5, in the *Shire of Wyndham East Kimberley Strategic Plan 2008*.

- Advocate for opportunities unique to the region
- Promote and facilitate economic opportunities
- Effective partnerships with the public and private sectors
- Leadership and advocacy on issues relevant to the region
- Council has effective relationships with external organisations

#### **COMMUNITY CONSULTATION**

Advice has been sought from the Ord Irrigation Cooperative (OIC) and the Ord-East Kimberley Expansion Project (OEKEP). The OEKEP has indicated that the current Ord

Allocation Plan may not provide sufficient water allocation to enable the development of irrigated agriculture to its full potential. They are of the view that there is a possibility that sugar cane may emerge as a major crop across the valley, and if that were to happen, there may not be a sufficient allocation.

OEKEP has suggested an interagency workshop to develop and scope out investigative work aimed at increasing the water allocation for irrigated agriculture. The OEKEP have not however provided a formal response on the Plan.

The OIC have not yet provided a formal response to DoW and did not provide any preliminary comments for council to consider.

## **COMMENT**

The volumes of water allocated to each area of the Ord Irrigation Area have not changed from the plan released in 2006. It is important to note that the allocations are calculated using an average of approximately 16ML of water per hectare at a 95% reliability of supply. The type of crops planted across the irrigation area will impact on the volume of water that is available for future development. However, this plan allows for the full expansion of the Irrigation area including the 14000 hectares in the Northern Territory. If a greater volume of allocated water is required, it is likely that reliability will need to decrease.

The volume of water allocated to self-supply pumpers is 15GL and approximately 60% of that volume has already been allocated. No additional allocations are set aside for further rural residential or residential developments such as Valentine Falls estate.

There is likely to be an announcement by the State Government at the end of September 2012 regarding the distribution of land in the Goomig, Knox Plain and Ord West bank land areas. The Shire President John Moulden and Chief Executive Officer Gary Gaffney have been briefed about that process. Councillors may want to consider the potential impact that the announcement may have on the community.

Taking on board the suggestions of the OEKEP, and having regard to the potential needs for the area with regards to both agriculture and urban development, it is suggested that Council submit to the Department of Water on the following basis:

- That consideration be given to additional consultation on the Plan be given to the broader community, to better explain the Plan with respect to the need for environmental water requirements and how restrictions will apply in drought periods.
- That an interagency working group be formed, along the lines of that suggested by the OEKEP to discuss the current allocated volumes and the implications for the types of crops planted in the valley and to ensure greater flexibility to anticipated future expanded agricultural opportunities, including to look at options, such as to raise the height of the Ord Dam spillway.
- That there is consideration for allocation of water supply for expansion of urban and rural-residential land consistent with the Shire's planning strategies.
- To review new release rules and the potential impacts that may be caused to tour boat operators – to ensure that water is available for tourism operations, particularly during the dry season months.

A copy of the draft response is provided at Attachment 1.

## **ATTACHMENTS**

The associated attachment will be provided under separate cover.

Attachment 1 – Draft response to Department of Water

Attachment 2 – Letter from Ord Irrigation

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council notes Ord River Surface Water Allocation Plan and writes to the Department of Water and submits on the basis of the following:

- That consideration be given to additional consultation on the Plan be given to the broader community, to better explain the Plan with respect to the need for environmental water requirements and how restrictions will apply in drought periods.
- That an interagency working group be formed, along the lines of that suggested by the OEKEP to discuss the current allocated volumes and the implications for the types of crops planted in the valley and to ensure greater flexibility to anticipated future expanded agricultural opportunities, including to look at options, such as to raise the height of the Ord Dam spillway.
- That there is consideration for allocation of water supply for expansion of urban and rural-residential land consistent with the Shire's planning strategies.
- To review new release rules and the potential impacts that may be caused to tour boat operators – to ensure that water is available for tourism operations, particularly during the dry season months.

## **MOTION**

**Moved: Cr J Parker**

**That Council holds over a decision on this report and agrees to hold a Special Council Meeting on 10 September 2012 in respect to letter written by Ord Irrigation Co-operative to Department of Water.**

## **COUNCIL DECISION**

**Minute No. 9865**

**Moved: Cr J Parker**

**Seconded: Cr R Addis**

**That Council holds over a decision on this report and agrees to hold a Special Council Meeting on 10 September 2012 in respect to letter written by Ord Irrigation Co-operative to Department of Water.**

Attachment 1 – Draft response to Department of Water

22 August 2012

Our Ref: GR.09.1  
Your Ref: WT9482  
Enquiries: Nick Kearns

Ian Loh  
Water Allocation Planning Branch  
Department of Water  
PO Box 6822  
Perth WA 6842

Dear Mr Loh

**Ord Surface Water Allocation Plan**

Thank you for the opportunity to comment on the Ord River Surface Water Allocation Plan.

The Council of the Shire of Wyndham East Kimberley is supportive of opportunities to promote and provide opportunities for economic and social growth in the East Kimberley. It is generally considered that this Plan demonstrates the availability of water to support that growth and therefore, the Council is supportive of the Plan but provides the following comments by way of a submission:

1. The Council is of the view that additional public information sessions should be held to better explain the plan to the community and other key stakeholders how it affects economic development in the East Kimberley, particularly in relation to the need for environmental water requirements and how restrictions may apply in drought periods.
2. That an interagency working group be formed to discuss the current allocated volumes and the implications for the types of crops planted in the valley, particularly as that would impact upon flexibility to raise crops and in anticipation of future expanded agricultural opportunities. The Shire suggests options to increase future water supply such as to raise the height of the spillway at the Ord Dam.
3. The Council would like to see further volumes of water allocated to expansion of urban and rural-residential land consistent with Shire strategies.
4. The Council is also concerned about the new release rules and the potential impacts that may be caused to tour boat operators and would encourage the Department of Water to ensure that water is available for tourism operations, particularly during the dry season months.

Should you have any further enquiries please do not hesitate to contact Nick Kearns, Director Community Development on (08) 9168 4100 or email [nick.kearns@swek.wa.gov.au](mailto:nick.kearns@swek.wa.gov.au).

Yours sincerely

Gary Gaffney  
Chief Executive Officer

## Attachment 2 – Letter from Ord Irrigation



Phone (08) 9168 3300  
Mobile 0408 683 300  
PO Box 573  
Kununurra WA 6743

Chris Gunby  
Department of Water  
PO Box 625  
Kununurra, WA 6743

Ord Irrigation Co-operative Ltd ("OIC") would like to congratulate the Department of Water ("the Department") on releasing the draft Ord Surface Water Allocation Plan ("the Plan") for public comment.

OIC is supportive of Agricultural Expansion and the Departments plan to secure and make available reliable water supplies for a strong and expanding irrigation industry. OIC is also an advocate for the environment and believes that the proposed plan will support a healthy lower Ord River environment.

As a large stakeholder in the region, the OIC would however like to raise several constructive objections in relation to the Plan, as currently proposed. There is the possibility that some sections of the Plan, if left in the final version, could be highly detrimental to OIC members and the community. In its current state, it is in our opinion highly probable that certain aspects of the Plan would stifle the possibility of future water trade, as well as potentially amount to a direct failure to comply with the directives of the National Water Initiative (NWI).

Western Australia signed the Intergovernmental Agreement on a National Water Initiative (NWI) in 2006 and has been continually working towards (and releasing documentation periodically) which evidences its intention to implement transparent objectives in the water industry/market closely aligned with the principles set out in the NWI. As such, the OIC questions why a document has now been released by the Department which clearly doesn't comply with the key principles in the NWI. With a view to clarification on this point, can the Department please confirm that its intention is to breach the NWI?

Unfortunately, it would appear in the Plan that some key aspects of the NWI are not being complied with. The OIC has noted in previous correspondence the Department's error in interchangeably using "entitlements" and "allocation" which has continued into the present plan - these are entirely different concepts with entirely different legal meanings. The OIC strongly believes that the Department needs to correct the Plan and use the unambiguous legal concepts of entitlement and allocation clearly and based on the standard definitions and usages in line with the NWI. For the avoidance of doubt;

**Entitlements:** such as, in our opinion, the OIC water licence refers to a perpetual entitlement to exclusively access a share of water. The allocation of water to an entitlement is to be made consistent with the requirements and rules set out in the relevant water plan. Entitlements are to:

- specify the essential characteristics of the water product;
- be exclusive;
- be able to be traded, given, bequeathed or leased;
- be able to be subdivided or amalgamated;
- be mortgageable (and in this respect have similar status as freehold land when used as collateral for accessing finance) *a number of OIC shareholders have mortgage water entitlements to support irrigation activities*;
- be enforceable and enforced; and
- be recorded in publicly-accessible reliable water registers that foster public confidence and state unambiguously who owns the entitlement, and the nature of any encumbrances on it (NICWER).

Entitlements also;

- clearly indicate the responsibilities and obligations of the entitlement holder consistent with the *water plan* relevant to the source of the water;
- only be able to be cancelled at Ministerial and agency discretion where the responsibilities and obligations of the entitlement holder have clearly been breached (*the OIC has never breached our License conditions*); and
- be able to be varied, for example to change extraction conditions, where mutually agreed between the government and the entitlement holder (*OIC does not mutually agree to "recoup" entitlements*);

**Allocation:** The percentage of the entitlement volume that is available for diversion by irrigators in the current water year in regulated rivers that use an "annual accounting" system.

OIC notes these key objectives of the NWI in relation to entitlements and allocations which are to:

- enhance security and certainty of water access
- ensure they possess clear and **nationally**-compatible characteristics
- ensure entitlements clearly assign the risks arising from future changes to the consumptive pool, including unregulated growth in interception through land-use change.

In light of the above, the OIC requests the following constructive concerns are noted and requests the Plan be amended accordingly;

1. The Plan makes reference to "recouping" unused entitlements. It also makes reference to its intention to "match water entitlements with water use" and promotes

confusion by interchangeably misusing the terms "entitlement" and "allocation". Moreover, the Department has seemingly expressed that should the OIC not be able to show reasonable use for 335GL that the water shall be returned to the consumptive pool where it will be further allocated to third parties. The OIC asserts that the Department cannot legally "recoup" a water entitlement such as exist pursuant to its 335GL licence.

The OIC water entitlement is owned legally by the OIC and beneficially by its members and was fundamentally and unequivocally entered into on the basis of representations made by the Department as to the intended permanency of this water entitlement. It is a right to water which is volumetrically determined pursuant to variations in allocation and has, on occasion, been borrowed against accordingly by OIC members. This was done on the basis of representations made by the Department to OIC and its members as to permanent water entitlements in line with the NWI. Banks have consequently lent money to OIC members and have an equitable interest in some of the water entitlements. The 335GL water entitlement was issued on the basis of representations by the Department of permanent water entitlements in line with the objectives of the NWI. Certain aspects of the Plan appear to be a breach of these NWI objectives and undermine the unambiguous representations made by the Department to the OIC and its members as to permanent water entitlements in line with the NWI. The OIC again requests clarification on whether through this Plan it is the Department's intention to breach the NWI as signed by the Western Australian Government?

The level of entitlement was originally based on a sugarcane agricultural industry and conveyancing water to support a flow-through-irrigation system. Whilst the Sugar Industry is not currently active, there are very strong possibilities that the industry will re-surface in future years. Shareholders of the cooperative have purchased these entitlements and they are recorded in the National Irrigation Corporations Water Entitlement Register (NICWER).

The volume of entitlement ensures that growers have flexibility in their cropping possibilities from season to season and has been calculated from crop water uses in the region. These water entitlements are separate from land titles and can be permanently or temporarily traded as required. This promotes regional investment, water trading and certainty among water users. In addition, the OIC has systems and software in place that supports these practices.

2. OIC notes from the Plan that the department references "During the past three years the cooperative has taken less than 200GL/yr from Lake Kununurra....." OIC sees this comment as completely inappropriate and irrelevant for an allocation plan, particularly as the comments fail to reflect the ongoing re-development of OIC irrigation land in line with new industries. The closure of the Sugar Industry had resulted in 50% of the irrigation land being progressively re-introduced to irrigated cropping over the past 4-5 years and the recent water use in no way will reflect the next 10 years as the land is being used for silviculture, horticulture or agriculture.

3. The Department comments in the plan "OIC.....supplies water to 63 irrigation cooperative members". Individual members are irrelevant to the plan and the OIC

requests that the Department only refers to 111 individual properties, comprising a total area of 15363Ha and a combined 335GL water entitlement.

5. OIC notes that the Department has also separately allocated entitlement to Packsaddle expansion and the East Bank expansion, both of these developments are within the deliverable area of the Stage 1 infrastructure and therefore water entitlement is available from OIC's existing water entitlement. OIC has sold additional water from our entitlement to support new irrigation land that is within the boundary of our license.

6. OIC is extremely supportive of the water and its control to support traditional Aboriginal laws and customs. OIC agrees that this will be difficult to manage, and recognises the environmental benefits associated with periodically reducing flows to below environmental flow requirements; provided this doesn't impact irrigation capacity.

The OIC is also supportive of the annual lowering of the Lake to be able to perform critical asset management practices.

7. OIC is supportive of the social water aspect of the Plan, however it must be noted that the environmental water for many years has been well above the minimum environmental flow requirements and that recreational users and locals may view these as "Normal" conditions. This has already been seen in recent months with conservative environmental flows and changes in downstream geographic's impacting local fishing businesses in the lower Ord River. It should be noted that these businesses make no financial contributions to water storage or transfer and so the OIC would find it unacceptable if any water was intentionally released to satisfy their requirements that impacted negatively on irrigator's allocation.

8. OIC also notes that the Plan currently makes reference to a 104GL entitlement that is currently being requested by Carlton Plain development and requests clarification on the impact to the consumptive pool and the Main Ord subarea. The Carlton Plain development is not listed in the water demand chart, and OIC seeks clarification as to whether this 104GL is included in the 865GL total water availability for irrigation purposes, and a more detailed analysis of hectares to entitlements. The OIC feels 865GL will not support the Stage 1, Main Ord future expansion and the Tarrara-Carlton subarea. OIC is concerned the Department is going to drastically under-allocate inflexible water entitlements to new developments.

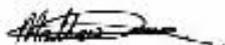
It is requested that the Department note that any uncompensated "recouping" or any alteration whatsoever by the Department of the 335GL water entitlement of the OIC will be vigorously defended by the OIC in every available forum and notes that in adopting this policy, the Department would, in the opinion of OIC, fundamentally breach its obligations under the NWI as signed by the Western Australian government.

From a social perspective, the water entitlement is a key factor in land values, cropping systems, irrigator profitability and some of the current suggestions set out in the Plan threaten entire communities and are, in the view of OIC and its members, morally and publicly unacceptable practices by a government entity. Despite the above, there are many aspects of the Plan which the OIC is positively in support of

and we thank the Department for allowing this forum to comment on the proposed Plan. The OIC looks forward to seeing its comments incorporated into the next stage in the development of the Plan.

If you wish to discuss this further please do not hesitate to call me.

Yours sincerely,

A handwritten signature in black ink, appearing to read 'Mathew Dear', with a horizontal line underneath.

Mathew Dear  
General Manager

## 12.5 CHIEF EXECUTIVE OFFICER

### 12.5.1 Use of the Common Seal

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Debbie McCallum, Officer
<b>REPORTING OFFICER:</b>	Gary Gaffney, Chief Executive Officer
<b>FILE NO:</b>	GN.02.1

#### **PURPOSE**

For Council to receive this report on the application of the Shire of Wyndham East Kimberley Common Seal for the period 12 July to 16 August 2012.

#### **STATUTORY IMPLICATIONS**

*Local Government Act 1995*

Council's Standing Order Local Law makes reference to the application of the Common Seal.

#### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

#### **FINANCIAL IMPLICATIONS**

There are no financial implications associated with this item.

#### **STRATEGIC IMPLICATIONS**

This report aligns with Council's focus on Governance, Key Result Area 5, in Council's Strategic Plan.

#### **COMMENT**

The following documents have had the Shire of Wyndham East Kimberley Common Seal applied:

<b>Date of Use</b>	<b>Document</b>
25/07/2012	Deed of Agreement between Marc and Mandy Watson and Kangaroo Haven Inc and SWEK
7/08/2012	Annual Monitoring and Audit Compliance Report (1 Oct 2010 - 30 Sept 2011 Kununurra Waste Disposal Policy Facility

## **ATTACHMENTS**

There are no attachments associated with this report.

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council receives the report on the application of the Shire of Wyndham East Kimberley Common Seal for the period 12 July to 16 August 2012.

## **COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr D Ausburn**

**That Council receives the report on the application of the Shire of Wyndham East Kimberley Common Seal for the period 12 July to 16 August 2012**

**Carried Unanimously 6/0**

## 12.5.2 Delegated Authority Report

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Debbie McCallum, Governance Officer
<b>REPORTING OFFICER:</b>	Gary Gaffney, Chief Executive Officer
<b>FILE NO:</b>	PL.02.1

### **PURPOSE**

To report to Council on the use of Delegated Authority by Officers for the period 1 July to 31 July 2012.

### **BACKGROUND**

Use of Council approved Delegated Authority by Officers is reported to Council on a monthly basis.

The attached tables outline use of Delegated Authority by relevant Officers for the above period.

### **STATUTORY IMPLICATIONS**

Local Government Act 1995 - Sect 5.46

5.46. Register of, and records relevant to, delegations to CEO's and employees.

- 3) The CEO is to keep a register of the delegations made under this Division to the CEO and to employees.
- 4) At least once every financial year, delegations made under this Division are to be reviewed by the delegator.
- 5) A person to whom a power or duty is delegated under this Act is to keep records in accordance with regulations in relation to the exercise of the power or the discharge of the duty.

### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

### **FINANCIAL IMPLICATIONS**

There are no financial implications associated with this item.

### **STRATEGIC IMPLICATIONS**

This report aligns with Council's focus on Governance, Key Result Area 5, in the *Shire of Wyndham East Kimberley Strategic Plan 2008*.

**COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

**COMMENT**

The attached report outlines the use of Delegated Authority by relevant Council Officers for endorsement by Council.

**ATTACHMENTS**

Attachment 1 – Delegated Authority Report

**VOTING REQUIREMENT**

Simple Majority

**OFFICER'S RECOMMENDATION**

That Council receives the Delegated Authority Report for the period 1 July to 31 July 2012.

**COUNCIL DECISION**

**Moved: Cr J Parker  
Seconded: Cr D Ausburn**

**That Council receives the Delegated Authority Report the period 1 July to 31 July 2012.**

**Carried Unanimously 6/0**

**Attachment 1 – Delegated Authority Report**

**COMMUNITY QUICK GRANTS DELEGATED AUTHORITY APPROVALS – 1 July – 31 July 2012**

<b>Approval Date</b>	<b>Organisation</b>	<b>Purpose of Quick Grant</b>	<b>Total Project Cost</b>	<b>Amount Requested</b>	<b>Amount Approved</b>
11/07/2012	Ord Pistol Club	Restoration of Berm	\$10,000.00	\$500.00	\$500.00 In Kind
26/07/2012	Kununurra Tennis Club Inc	Community Tennis Nights	\$1,000.00	\$500.00	\$500.00
27/07/2012	Kununurra Gymnastics Club	Coaching Clinics	\$1600.00	\$500.00	\$330.00 In Kind remainder \$220.00 in cash

**CERTIFICATE OF DESIGN COMPLIANCE (Form BA03) ISSUED AS THE CERTIFIER FOR THE PERMIT ISSUING AUTHORITY –  
01 to 31 July 2012**

<b>APPLIC NO</b>	<b>DATE APPLIC RECVD</b>	<b>DATE ISSUED</b>	<b>ASSESS NO.</b>	<b>CERT No</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>
100415	4/05/2012	5/07/2012	A7284P	BA4115	Lot 27(90) Egret Close Kununurra	Class 10A Non-habitable shed (wildlife rehabilitation)
100450	15/05/2012	5/07/2012	A7102P	BA4117	Lot 116 (2) Salacca Loop Kununurra	Class 10A - Non-habitable Storage Units
100551	27/06/2012	11/07/2012	A5766P	BA4151	Lot 132 Whimbrel Road Kununurra	Class 10B - Swimming Pool (below ground)
73206	19/06/2012	11/07/2012	A2121P	BA4153	Lot 1738 (73a) Casuarina Way Kununurra	Conversion of existing Class 10A Shed to Habitable Class 1A building
100559	29/06/2012	17/07/2012	A1940P	BA4164	Lot 1929 (39) Eucalyptus Close Kununurra	Class 10B - swimming pool (below ground)
100557	29/06/2012	18/07/2012	A6264P	BA4166	"Ngunulum Community" (Cockatoo Springs) Community Lot 3 King Loc 374 Victoria Highway Via Kununurra	Class 10A Tropical Roof
100580	9/07/2012	18/07/2012	A7233P	BA4177	Lot 377 (18) Tamarind Meander Kununurra	Class 1A dwelling with attached Class 10A carport and outdoor living

**CERTIFICATE OF DESIGN COMPLIANCE (Form BA03) ISSUED AS THE CERTIFIER FOR THE PERMIT ISSUING AUTHORITY –  
01 to 31 July 2012 continued**

<b>APPLIC NO</b>	<b>DATE APPLIC RECVD</b>	<b>DATE ISSUED</b>	<b>ASSESS NO.</b>	<b>CERT No</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>
100553	5/07/2012	20/07/2012	A222P	BA4180	Lot 836 (49) Great Northern Highway Wyndham	Class 10B - roof mounted solar array
100574	5/07/2012	20/07/2012	A152P	BA4182	Lot 640 (45) Gully Road Wyndham	Class 10B - roof mounted solar array
100554	5/07/2012	20/07/2012	A247P	BA4184	Lot 831 (54) Koolama Street Wyndham	Class 10B - roof mounted solar array
100552	5/07/2012	23/07/2012	A1673P	BA4188	Lot 1708 Great Northern Highway Wyndham	Class 10B - roof mounted solar array
100575	5/07/2012	24/07/2012	A74P	BA4191	Lot 988 (Meter 1) Koolama Street Wyndham	Class 10B - roof mounted solar array
100572	5/07/2012	24/07/2012	A74P	BA4193	Lot 989 (Meter 2) Koolama Street Wyndham	Class 10B - roof mounted solar array
100495	28/05/2012	25/07/2012	A576P	BA4200	Lot 473 Crossing Falls Road Kununurra	Class 1A addition to existing dwelling
100581	11/07/2012	25/07/2012	A2467P	BA4202	Lot 24 Weaber Plain Road Kununurra	Class 10B - Swimming Pool (below ground)

**CERTIFICATE OF CONSTRUCTION COMPLIANCE (Form BA17) ISSUED AS THE CERTIFIER FOR THE PERMIT ISSUING AUTHORITY  
01 to 31 July 2012**

<b>APPLIC NO</b>	<b>DATE ISSUED</b>	<b>ASSESS NO.</b>	<b>CERT No</b>	<b>BUILDING PERMIT NO</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>
728428	30/07/2012	A7093P	BA4224	044/2011	Lot 104 Kentia Way Kununurra	Class 7B/8 Commercial Production & Storage Shed and Class 4 Caretakers residence

**BUILDING PERMITS (Form BA04) ISSUED AS THE PERMIT ISSUING AUTHORITY – 01 to 31 July 2012**

<b>LIC#</b>	<b>DATE RECEIVED</b>	<b>DATE LICENCED</b>	<b>OWNER</b>	<b>BUILDER</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>	<b>NEW/ ADD</b>	<b>EST. VALUE</b>	<b>COMMENT / PROCESSING TIME – WORKING DAYS</b>
067/2012	4/05/2012	5/07/2012	Marc & Mandy Watson	Marc & Mandy Watson	Lot 27(90) Egret Close Kununurra	Class 10A Non-habitable shed (wildlife rehabilitation)	New	\$15,000.00	
068/2012	15/05/2012	5/07/2012	Porsha Holdings Electrical Services Pty Ltd	T J Burnett (Proprietor/ Director- Porsha Holdings)	Lot 116 (2) Salacca Loop Kununurra	Class 10A - Non-habitable Storage Units	New	\$240,000.00	
069/2012	27/06/2012	11/07/2012	Ty Rankin	Ty Rankin	Lot 132 Whimbrel Road Kununurra	Class 10B - Swimming Pool (below ground)	New	\$22,750.00	9/25 assess days
070/2012	19/06/2012	11/07/2012	Jamie & Natasha Short	Jamie & Natasha Short	Lot 1738 (73a) Casuarina Way Kununurra	Conversion of existing Class 10A Shed to Habitable Class 1A building	Add	\$15,000.00	15/25 assess days

<b>LIC#</b>	<b>DATE RECEIVED</b>	<b>DATE LICENCED</b>	<b>OWNER</b>	<b>BUILDER</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>	<b>NEW/ ADD</b>	<b>EST. VALUE</b>	<b>COMMENT / PROCESSING TIME – WORKING DAYS</b>
071/2012	29/06/2012	17/07/2012	David Muller & Melissa Miller	Kununurra Pools & Spas	Lot 1929 (39) Eucalyptus Close Kununurra	Class 10B - swimming pool (below ground)	New	\$30,000.00	11/25 assess days
072/2012	29/06/2012	18/07/2012	"Ngunulum Community" (Cockatoo Springs) MG Corporation	CKC Nominees Pty Ltd t/as Ecoefficient Homes	"Ngunulum Community" (Cockatoo Springs) Community Lot 3 King Loc 374 Victoria Highway Via Kununurra	Class 10A Tropical Roof	New	\$ 47,850.00	12/25 assess days
073/2012	9/07/2012	19/07/2012	Darren Fulcher	Darren Fulcher t/as DE Carpenters	Lot 377 (18) Tamarind Meander Kununurra	Class 1A dwelling with attached Class 10A carport and outdoor living	New	\$ 380,000.00	7/25 assess days
074/2012	5/07/2012	20/07/2012	Ross McDonald & Rhonda McDonald	Trent Ross Electrical	Lot 836 (49) Great Northern Highway Wyndham	Class 10B - roof mounted solar array	Add	\$ 5,650.00	10/25 assess days
075/2012	5/07/2012	20/07/2012	Michelle McKittrick	Trent Ross Electrical	Lot 620 (45) Gully Road Wyndham	Class 10B - roof mounted solar array	Add	\$ 5,650.00	10/25 assess days

LIC#	DATE RECEIVED	DATE LICENCED	OWNER	BUILDER	LOCATION	DESCRIPTION	NEW/ ADD	EST. VALUE	COMMENT / PROCESSING TIME – WORKING DAYS
076/2012	5/07/2012	20/07/2012	Desmond Daniel & Lorre Daniel	Trent Ross Electrical	Lot 831 (54) Koolama Street Wyndham	Class 10B - roof mounted solar array	Add	\$5,650.00	10/25 assess days
077/2012	5/07/2012	23/07/2012	Jocelyn Layton	Trent Ross Electrical	Lot 1708 Great Northern Highway Wyndham	Class 10B - roof mounted solar array	Add	\$5,650.00	11/25 assess days
078/2012	16/07/2012	24/07/2012	Jossam Holdings Pty Ltd ATF Hale Trust	CKC Nominees Pty Ltd t/as Ecoefficient Homes	Lot 18 (4) Bluegrass Avenue Kununurra	Class 1A dwelling with attached Class 10A carport and outdoor living	New	\$392,000.00	Certified 5/10 assess days
079/2012	5/07/2012	24/07/2012	Anglican Diocese of North West Australia	Trent Ross Electrical	Lot 988 (Meter 1) Koolama Street Wyndham	Class 10B - roof mounted solar array	Add	\$5,650.00	12/25 assess days
080/2012	5/07/2012	24/07/2012	Anglican Diocese of North West Australia	Trent Ross Electrical	Lot 989 (Meter 2) Koolama Street Wyndham	Class 10B - roof mounted solar array	Add	\$5,650.00	12/25 assess days

LIC#	DATE RECEIVED	DATE LICENCED	OWNER	BUILDER	LOCATION	DESCRIPTION	NEW/ ADD	EST. VALUE	COMMENT / PROCESSING TIME – WORKING DAYS
081/2012	28/05/2012	25/07/2012	Patricia Handasyde & John Moulden	Patricia Handasyde & John Moulden	Lot 473 Crossing Falls Road Kununurra	Class 1A addition to existing dwelling	Add	\$30,000.00	
082/2012	11/07/2012	25/07/2012	Chopperfarm Pty Ltd	Chopperfarm Pty Ltd	Lot 24 Weaber Plain Road Kununurra	Class 10B - Swimming Pool (below ground)	Add	\$30,000.00	9/25 assess days
								\$1,236,500.00	

**DEMOLITION PERMITS (Form BA06) ISSUED AS THE PERMIT ISSUING AUTHORITY – 01 to 31 July 2012**

<b>DEMO PERMIT</b>	<b>DATE APPLIC RECVD</b>	<b>DATE LICENCED</b>	<b>ASSESS NO.</b>	<b>OWNER</b>	<b>ADDRESS</b>	<b>BUILDER/ DEMOLITION CONTRACTOR</b>	<b>BUILDER ADDRESS</b>	<b>DEMO CLASS (1, 2 or 3)</b>	<b>WAD CLASS/ LIC NO</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>
DP 018/ 2012	29/06/2012	4/07/2012	A5665P	BA4114	State Lands Department	PO Box 630 Kununurra	Nateis Contracting Pty Ltd	5 Spence Court Leeming	Class 1	WAD 248	London Loc 21 Kalumburu Community Lot 39 (Reserve 46596 - Education)
DP 019/ 2012	2/07/2012	11/07/2012	A260P	BA4160	Cambridge Gulf Limited	PO Box 238 Kununurra	KGC Enterprises	PO Box 701 Kununurra	WARA	WARA 141	Lot 2 Barytes Road Kununurra
DP 020/ 2012	5/07/2012	23/07/2012	A865P		Department of Finance, Building Management & Works Division	PO Box 4 Kununurra	SilviCare Pty Ltd	PO Box 36 Kununurra WA 6743	Class 2	WAD 275 / WARA 232	Lot 1270 (Crown Reserve 28410) Minderoo Street Wyndham

**OCCUPANCY PERMITS (Form BA10 and BA12) ISSUED – 01 to 31 July 2012**

<b>OCCUP PERMIT NO</b>	<b>DATE</b>	<b>BUILDING PERMIT / LICENCE NO</b>	<b>ASSESS NO.</b>	<b>Receipt No</b>	<b>Building Class</b>	<b>OWNER</b>	<b>BUILDER</b>	<b>BRB / OB NO</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>
BA4225	30/07/2012	044/2011	A7093P		7B/8 & 4	Lerch Holdings	Lennox Wildman	S.1198	Lot 104 Kentia Way Kununurra	Class 7B/8 Commercial Production & Storage Shed and Class 4 Caretakers residence

**SIGN LICENCES ISSUED – 01 to 31 July 2012**

<b>SIGN LIC#</b>	<b>DATE APPLIC RECVD</b>	<b>DATE LICENCED</b>	<b>ASSESS NO.</b>	<b>OWNER</b>	<b>ADDRESS</b>	<b>BUILDER/ CONTRACTOR</b>	<b>BUILDER ADDRESS</b>	<b>LOCATION</b>	<b>DESCRIPTION</b>	<b>Planning Approval / Delegation</b>
SL 02/ 2012	2/07/2012	2/07/2012	A2148	ROAD RESERVE	ROAD RESERVE	Reef Tour - Tura New Music	C/- PO Box 1379 Kununurra	Lot 2446 Coolibah Drive Kununurra "Post Office Access Road"	Temporary Banner Sign	N/A exempt sign
SL 03/ 2012	2/07/2012	2/07/2012	A6806	ROAD RESERVE	ROAD RESERVE	Reef Tour - Tura New Music	C/- PO Box 1379 Kununurra	Road Reserve adjacent to Loc 567 Ivanhoe Road Kununurra	Temporary Banner Sign	N/A exempt sign
SL 04/ 2012	12/06/2012	11/07/2012	A2148	ROAD RESERVE	ROAD RESERVE	Kimberley Writers Festival	C/- PO Box 666 Kununurra	Lot 2446 Coolibah Drive Kununurra "Post Office Access Road"	Temporary Banner Sign	N/A exempt sign

THERE WERE NO FORMAL NOTICES RAISED PURSUANT TO THE BUILDING ACT 2011 ISSUED – 01 to 31 July 2012  
THERE WERE NO BUILDING PERMITS (BA04) AND CERTIFICATES OF DESIGN COMPLIANCE (BA03)  
ISSUED BY EXTERNAL PERMIT ISSUING AUTHORITIES UNDER BUILDING ACT 2011 S.7(02)  
RELATING TO PROPERTIES WITHIN THE SHIRE OF WYNDHAM EAST KIMBERLEY. – 01 to 31 July 2012

### 12.5.3 Kimberley Regional Collaborative Group – Regional Business Plan 2012 – 2015 and Strategic Community Plan 2012-2021

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Gary Gaffney Chief Executive Officer
<b>REPORTING OFFICER:</b>	Gary Gaffney, Chief Executive Officer
<b>FILE NO:</b>	N/A

#### **PURPOSE**

The purpose of this report is to present to Council the Kimberley Regional Collaborative Group Strategic Community Plan 2012 – 2021 and a Regional Business Plan 2012 – 2021 for endorsement.

#### **BACKGROUND**

The Kimberley Regional Collaborative Group has prepared a Strategic Community Plan 2012 – 2021 and a Regional Business Plan 2012 – 2021. These plans were commissioned to assist and guide the Regional Collaborative Group for three year period. The plan was endorsed by the Regional Collaborative Group at their meeting of July 2012 and is now referred to each relevant Local Government for consideration and then forwarded to the Department of Local Government.

The Strategic Community Plan and the Regional Business are based on five overarching key result areas:

- Governance
- Natural Environment
- Built Environment
- Community
- Economy

These result areas then have goals and outcomes associated with them.

The difference between the Business Plan and the Strategic Community Plan 2012- 2021 is due to its nature, the community plan has a more strategic longer term outlook and includes performance measurements.

Both of these documents are a requirement of the Department of Local Government as part of the Integrated Reporting Framework.

The documents should be reviewed annually .

#### **STATUTORY IMPLICATIONS**

There are no statutory implications associated with this report.

#### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

## **FINANCIAL IMPLICATIONS**

It is anticipated that some of the actions items in the Regional Business Plan may have associated financial impacts. The endorsement of the Business Plan does not commit Council to funding these actions and any financial consideration would be considered as part of Councils budget process.

## **STRATEGIC IMPLICATIONS**

It is important that alignment of the Shire of Wyndham East Kimberley Strategic Community Plan and the Kimberley Regional Group Strategic Plan is maintained for the benefit of our Council.

It should be noted that the direction for funding by Royalties for Regions is focussed on collaboration through regional projects and it is imperative that our Council shares the benefits of potential funding.

The strategic implications of this report are overarching compliance to the *Strategic Planning Process*.

This report aligns with Council's focus on Economic Development, Key Result Area 3, in the *Shire of Wyndham East Kimberley Strategic Plan 2008*.

- Leadership and advocacy on issues relevant to the region

## **COMMUNITY CONSULTATION**

The Shire of Wyndham East Kimberley has been consulted and participated in the Regional Business Plan and Strategic Community Plan.

## **ATTACHMENTS**

Attachment 1 – Strategic Community Plan 2012 - 2021

Attachment 2 – Strategic Business Plan 2012 -2021

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council:

- 1) Endorses the Regional Collaborative Group Regional Business Plan 2012-2015 and Strategic Community Plan 2012-21; and
- 2) Advises the Regional Collaborative Group that Council reserves the right to consider any financial commitment to implementing the Regional Business Plan as part of its normal budgeting process.

**COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr D Ausburn**

**That Council:**

- 1) Endorses the Regional Collaborative Group Regional Business Plan 2012-2015 and Strategic Community Plan 2012-21; and**
- 2) Advises the Regional Collaborative Group that Council reserves the right to consider any financial commitment to implementing the Regional Business Plan as part of its normal budgeting process.**

**Carried Unanimously 6/0**



# Kimberley

## Strategic Community Plan 2012-2021

### Kimberley Regional Group



## Acknowledgement

The Shires of Broome, Wyndham East Kimberley, Halls Creek and Derby West Kimberley wish to acknowledge funding provided by the Department of Local Government and Department of Regional Development and Lands through Royalties for Regions to support this project.



Department of **Local Government**  
Department of **Regional Development and Lands**



The Strategic Community Plan has been cooperatively developed and endorsed by the member Local Governments of the Kimberley Regional Group.



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## Disclaimer

Information provided in this Plan was gathered in good faith and is believed to be reliable and accurate at the time of publication.

The Shires of Broome, Wyndham East Kimberley, Halls Creek and Derby West Kimberley advise users of this information to take responsibility for their own assessment and to verify all representations, statements and information that concern the conduct of business that involves monetary or operational consequences.

The development of this Plan for the Kimberley Regional Group has been facilitated by:



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Web:	www.cammanagementsolutions.com

## 1. Foreword

### Message from the Chairperson



The Kimberley Regional Group's Strategic Community Plan is a document that identifies aspirations and sets priorities for our communities at a Kimberley-wide level.

Each participating Council (Broome, Derby West Kimberley, Halls Creek and Wyndham East Kimberley) has prepared, or is in the process of preparing, a Strategic Community Plan which aims to define their aspirations and priorities at the Council level.

This Plan strives to identify commonalities shared by the four Kimberley Councils, and I hope it will serve to unify us by shining light on the things which bind us as "The Kimberley".

This Strategic Plan should be seen as a constantly evolving roadmap for the Kimberley. It is the result of a lot of work by Elected Members and Executive Staff from all the Kimberley Councils and I want to thank everyone involved in its creation for their contributions.

We believe we have captured the essence of the region's aspirations, and have reflected these in our desired outcomes.

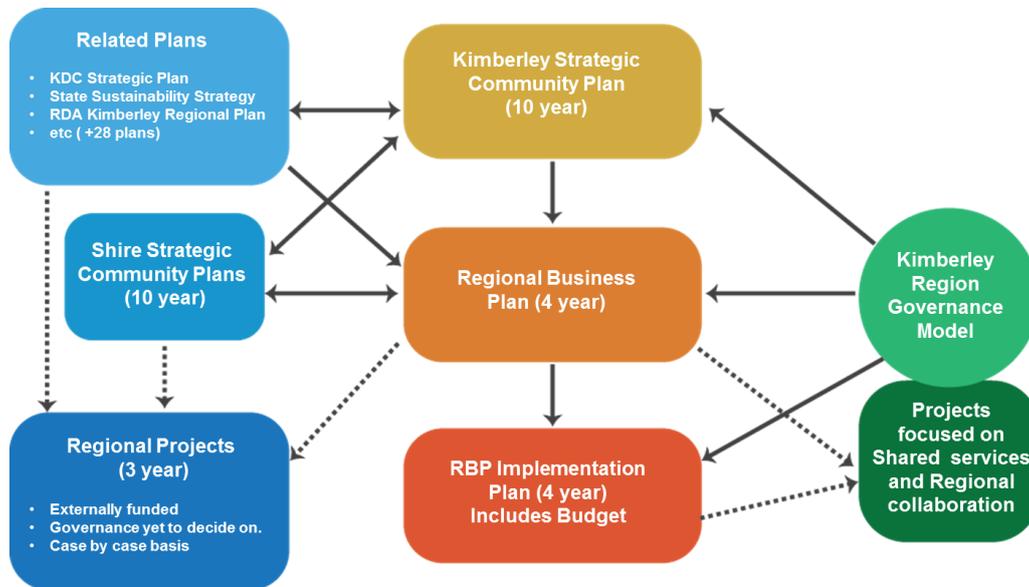
**John Moulden**

**Chairperson**

## 2. About this Plan

The Kimberley Strategic Community Plan is the primary strategic document in the Regional Planning Framework, outlined in the figure below:

**Figure 1: Regional Planning Framework**



The Kimberley Strategic Community Plan will be the primary driver for regional collaboration initiatives by the member Kimberley Shires, setting out the Vision, Goals, Outcomes and Strategies to guide and inform all related regional plans.

The Kimberley Strategic Community Plan was developed by the Kimberley Regional Group with direct reference to key stakeholder planning documents, including the four Member Shire's Strategic Community Plans and thirty one related regional, state and national plans and strategies with direct relevance to the role of Local Government in the Kimberley Region.

The Regional Business Plan provides the major actions, responsibilities and timeframes over a four year period to deliver on the strategies and outcomes in the Kimberley Strategic Community Plan.

The Regional Business Plan Implementation Plan provides detailed planning and budget information for priority projects focused on shared services and regional collaboration initiatives.

The Regional Planning Framework also recognises the identification and development of regional projects that has been driven through the individual Shire strategic planning processes and/or the plans and strategies of major stakeholders.

The diagram below outlines the structure of the Kimberley Strategic Community Plan and shows the critical linkage level with the Regional Business Plan:

**Figure 2: Kimberley Strategic Community Plan Structure**



Through a clear understanding of the current situation across the five strategic areas for Local Government:

1. Governance
2. Natural Environment
3. Built Environment
4. Community
5. Economy

A clear picture or Vision for the future, and a clear understanding of the Mission of the Kimberley Regional Group, it is possible to identify the goals and outcomes that must be achieved.

This provides the framework for the identification of strategies that reflect the role and responsibilities of the Kimberley Region governance model.

The strategies are the critical level of connection from the Kimberley Strategic Community Plan to the Regional Business Plan, and drive the selection of priority actions and initiatives.



### 3. Vision

Maintain and enhance the rich diversity and liveability of the Kimberley for its people and the world

### 4. Mission

Working collaboratively for the benefit of people and the land of the Kimberley region

### 5. Values

- Equity for the interests of each Shire
- Respecting the diversity of people, landscape and culture
- Integrity, openness and trust
- Commitment to Kimberley outcomes
- Working proactively to achieve our collective vision
- Valuing success
- Innovative

## 6. Regional Profile

### Geography

The Kimberley region covers an area of 419,558km<sup>2</sup> which is one sixth of Western Australia's total land area. It is one of the nine regions of Western Australia and is located in the northern part of the State. The Kimberley is bordered by the Indian Ocean in the west, the Timor Sea to the north, the Great Sandy and Tanami Deserts to the south, and the Northern Territory to the east.



The distance from Perth city to the nearest town, Broome, is 2,415km. The distance from Broome to Wyndham is 1055km via the Great Northern Highway.

The Kimberley is located north of the Tropic of Capricorn, so the climate is tropical monsoon, defined by a distinct wet season (November to April) and a dry season (May to October).

Significant geographic features of the Kimberley include:

- Most of the northern portion is characterised by savannah style vegetation with mature trees and grasslands. Rivers to the north are commonly lined with paperbarks and pandanus
- The coast is typically steep cliffs in the north but flatter in the south with both subject to high tides
- There are no mountains that reach above 1,000 metres
- The southern end of the Kimberley beyond the Dampier Peninsula is flatter with dry tropical grassland and is used for cattle ranching
- In parts of the Kimberley such as the valleys of the Ord and Fitzroy Rivers, the soils are useable cracking clays, whilst elsewhere they are lateritic Orthents
- The largest protected areas are the Prince Regent Nature Reserve and the Drysdale River National Park

The following figure shows a general reference map of the Kimberley region:

**Figure 3: Kimberley Region Map**



Source: Department of Regional Development and Lands, 2009

## Population

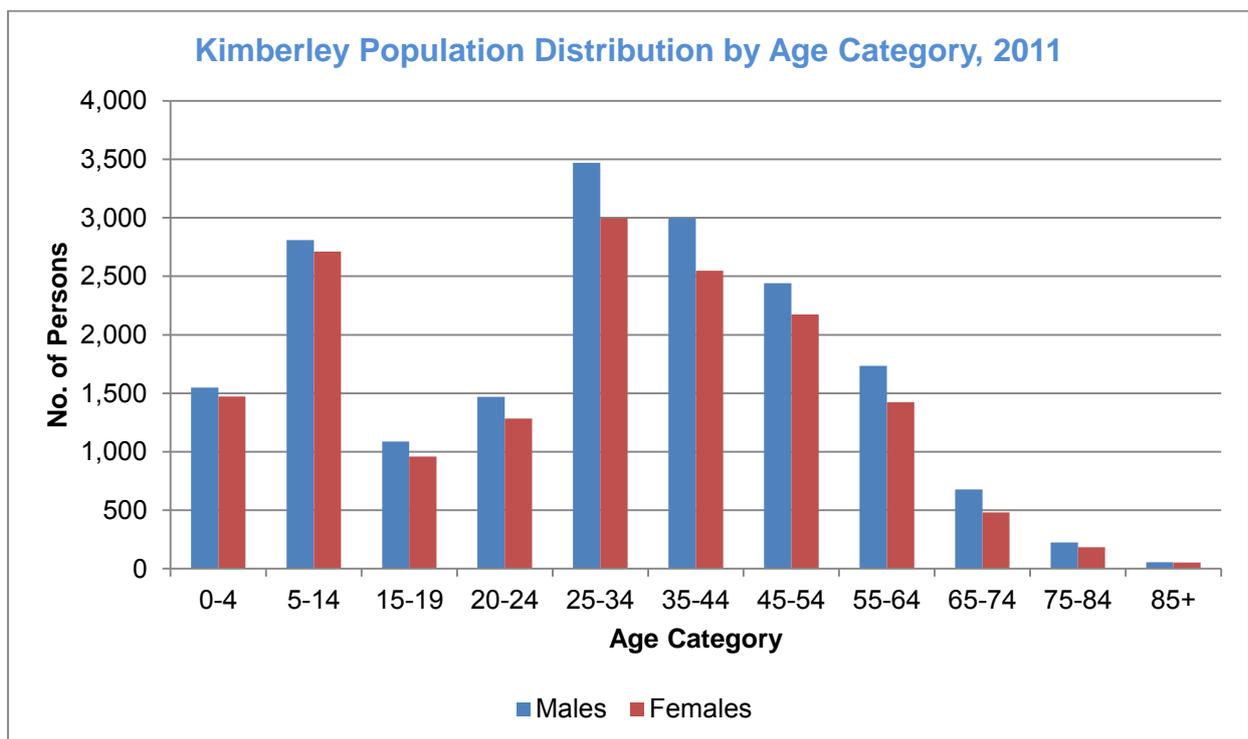
The relatively small total population of 34,794 persons and correspondingly large geographic area of 419,558km<sup>2</sup> in the Kimberley region, equates to a population density of 0.08 persons/km<sup>2</sup> (ABS 2011). This compares with the State population density of 0.89 persons/km<sup>2</sup> and that of Australia at 2.78 persons/km<sup>2</sup> in 2011.



The median age of the Kimberley population in 2011 was 30 years, compared to the State median age of 36 years. The population in 2011 comprised 53.2% males and 46.8% females.

In the same year, there were 8,541(24.6%) people who were below 15 years old and 2,952(8.5%) who were 60 years and above.

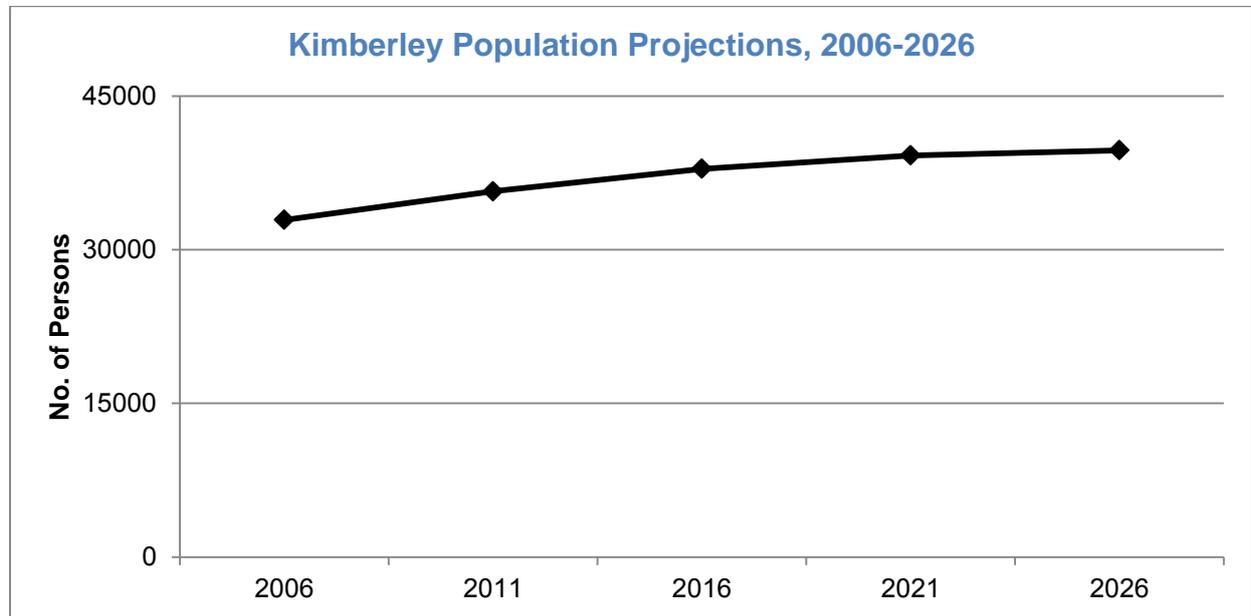
**Figure 4: Kimberley Age Breakdown, 2011**



Source: ABS – Census of Population and Housing, 2011

The Kimberley is expected to have a steady growth over the next 15 years. The Department of Planning & Infrastructure projections of the Kimberley population are displayed in the line graph below:

**Figure 5: Kimberley Population Projections, 2006-2026**



Source: Western Australia Planning Commission, Forecast Profile for the Kimberley Planning Region, 2012

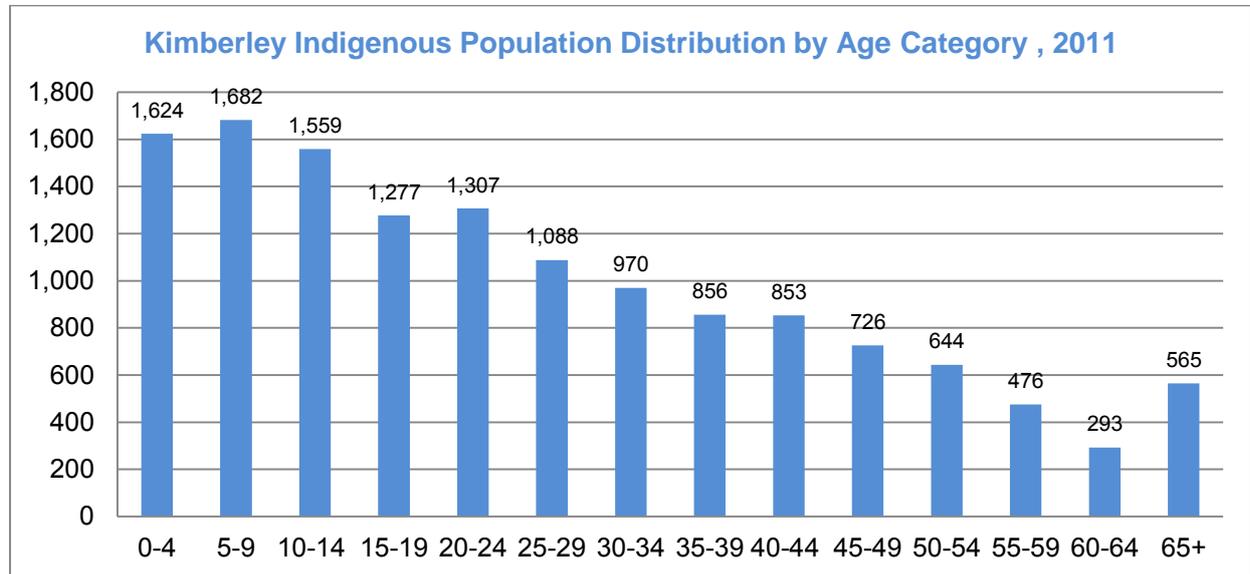
There is a strong Indigenous representation in the Kimberley. The 2011 Census indicated that 40.0% of the total population in the Kimberley were Indigenous people. This compares with 3.1% of the population State-wide. According to the 2011 ABS census, Indigenous residents in the Kimberley had a median age of 22



years and were made up of 6,738 males and 7,182 females. The 2011 Census also indicated that 7,449(53.5%) of the Indigenous population of the Kimberley were less than 25 years of age and 858(6.2%) were 60 years and above.

The Kimberley Indigenous population distribution by age category for is displayed in the graph below:

**Figure 6: Kimberley Indigenous Population by Age Category, 2011**



Source: ABS – Census of Population and Housing, 2011

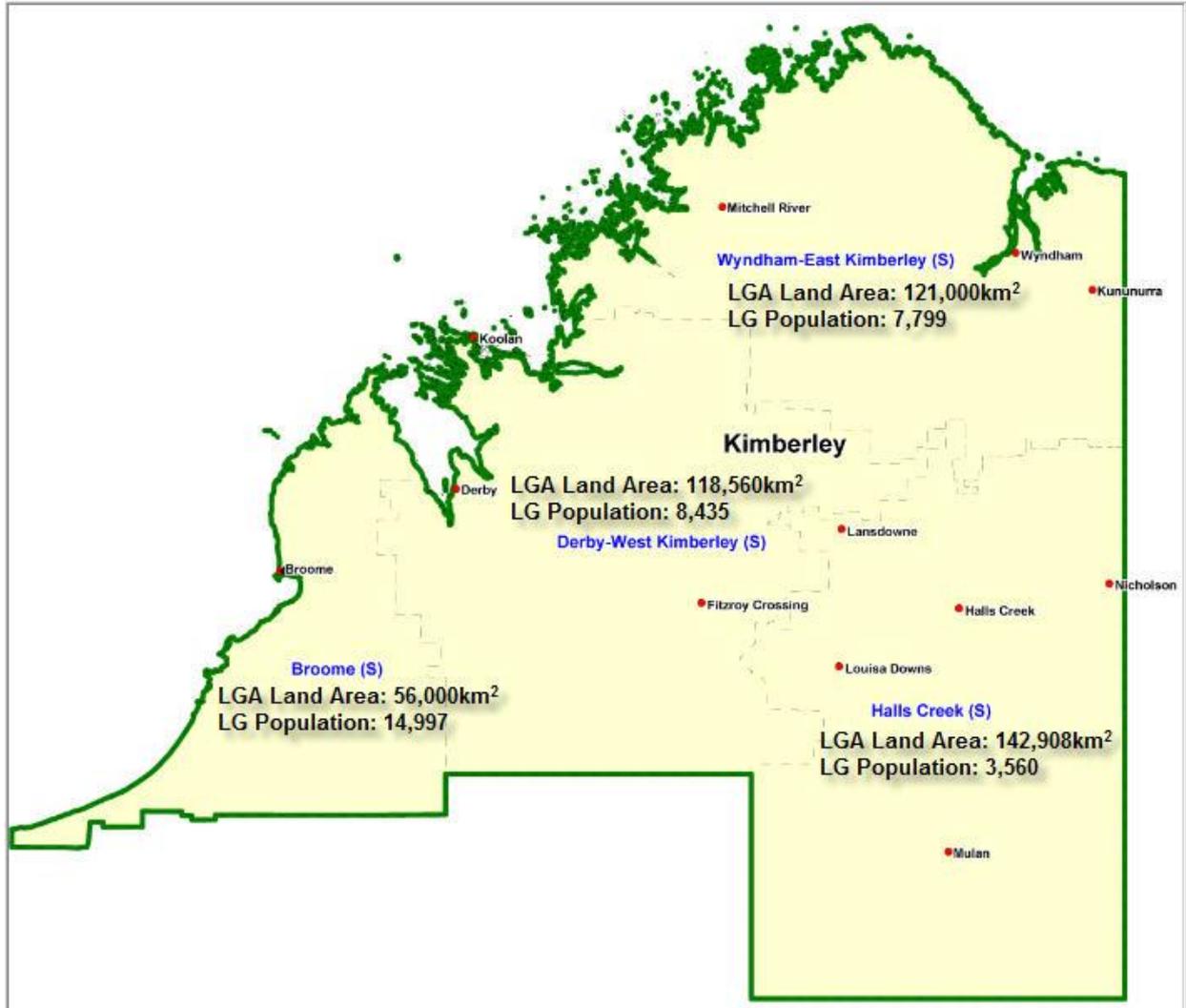
It is forecast that the characteristics of the Kimberley population will alter significantly over subsequent generations. From the macro-economic perspective there will be substantial challenges for Local, State and Commonwealth governments in achieving balanced development from economic, cultural and community perspectives.

The following summarises the population growth for each Shire in recent years: (ABS 2011)

- Shire of Broome – 14,997 people in 2011 and of these 51.1% were male and 48.9% were female. Aboriginal and Torres Strait Islander people made up 29.1% of the population. Average annual growth rate for the 5 years to 2011 of 1.16%.
- Shire of Derby West Kimberley - 8,435 people in 2011 and of these 58.9% were male and 41.1% were female. Aboriginal and Torres Strait Islander people made up 48.1% of the population. Average annual growth rate for the 5 years to 2011 of 2.94%.
- Shire of Halls Creek - 3,560 people in 2011 and of these 51.2% were male and 48.8% were female. Aboriginal and Torres Strait Islander people made up 78.2% of the population. Average annual growth rate for the 5 years to 2011 of 1.98%.
- Shire of Wyndham East Kimberley – 7,799 people in 2011 and of these 51.9% were male and 48.1% were female. Aboriginal and Torres Strait Islander people made up 34.8% of the population. Average annual growth rate for the 5 years to 2011 of 1.79%.

The land area and population for each of the four Local Government Areas is represented in the figure below.

**Figure 7: Kimberley Local Government Areas**



Source: ABS – National Regional Profile, Kimberley, 2011

## Labour Force

The labour force varies significantly between the Local Government Areas. The Shire of Broome, with a larger total population has a proportionately higher number of people in the labour force. The table below presents the labour force for each Local Government Area from 2006 to 2011.

**Table 1: Kimberley Labour Force, 2006-2011**

Local Government Area/Region	Labour Force, June Quarter					
	2006	2007	2008	2009	2010	2011
<b>KIMBERLEY</b>	17,539	17,667	16,471	15,819	17,375	18,159
<b>Broome (S)</b>	7,983	8,042	7,617	7,357	8,080	8,453
<b>Derby-West Kimberley (S)</b>	3,918	3,947	3,627	3,467	3,808	3,980
<b>Halls Creek (S)</b>	1,604	1,615	1,387	1,293	1,420	1,484
<b>Wyndham-East Kimberley (S)</b>	4,034	4,063	3,840	3,702	4,067	4,242

Source: Department of Education, Employment and Workplace Relations, 2006-2011

There is also significant variance between Local Government Areas in the unemployment rate. The most recent statistics indicate an unemployment rate of around 5.8% across the Kimberley. The unemployment rate for the 6 years to 2011 by June Quarter for each Shire is shown in the table below. The Shire of Halls Creek has consistently recorded the highest unemployment rate across the Shires.

**Table 2: Kimberley Unemployment, 2006-2011**

Local Government Area/Region	Unemployment, June Quarter					
	2006	2007	2008	2009	2010	2011
<b>KIMBERLEY</b>	5.6%	5.1%	5.4%	6.6%	8.7%	5.8%
<b>Broome (S)</b>	4.1%	4.7%	4.9%	6.0%	7.1%	4.3%
<b>Derby-West Kimberley (S)</b>	4.5%	4.6%	5.0%	6.1%	9.3%	5.8%
<b>Halls Creek (S)</b>	15.9%	9.0%	12.0%	14.6%	18.7%	13.8%
<b>Wyndham-East Kimberley (S)</b>	5.4%	4.8%	4.7%	5.6%	7.7%	6.0%

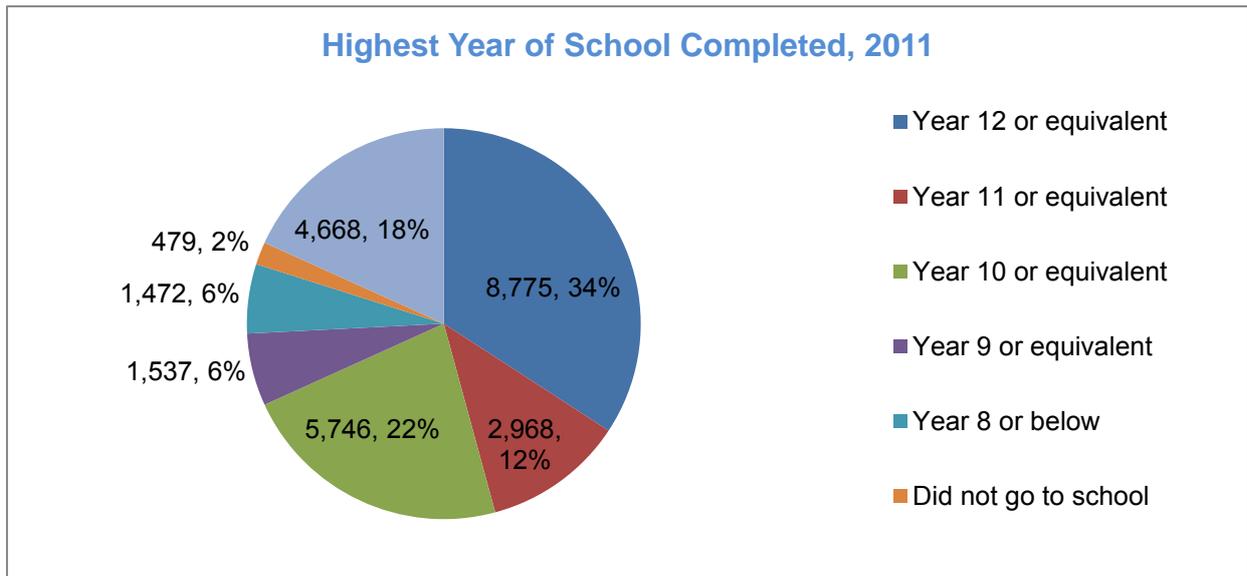
Source: Department of Education, Employment and Workplace Relations, 2006-2011

## Education

In the Kimberley region there were a total of 17,489 school children (68.2%) who had completed Year 10 or above in 2011. This consisted of an equal number of males (8,631) and females (8,858). The highest year of school completed is shown in the figure below:



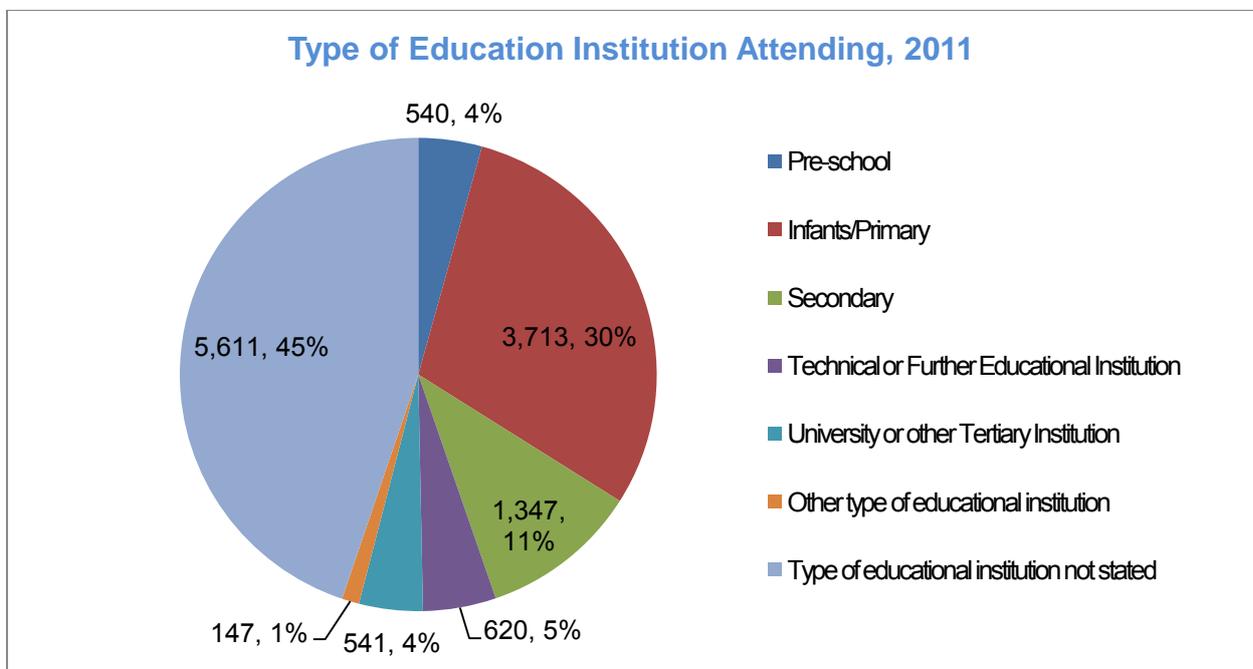
**Figure 8: Kimberley Highest Year of School Completed, 2011**



Source: ABS – Census of Population and Housing, 2011

In terms of the type of education institution in the Kimberley, the major category was infants or primary level, 30% in 2011. The breakdown of the type of education institution in 2011 is shown in the pie chart below:

**Figure 9: Kimberley Type of Education, 2011**

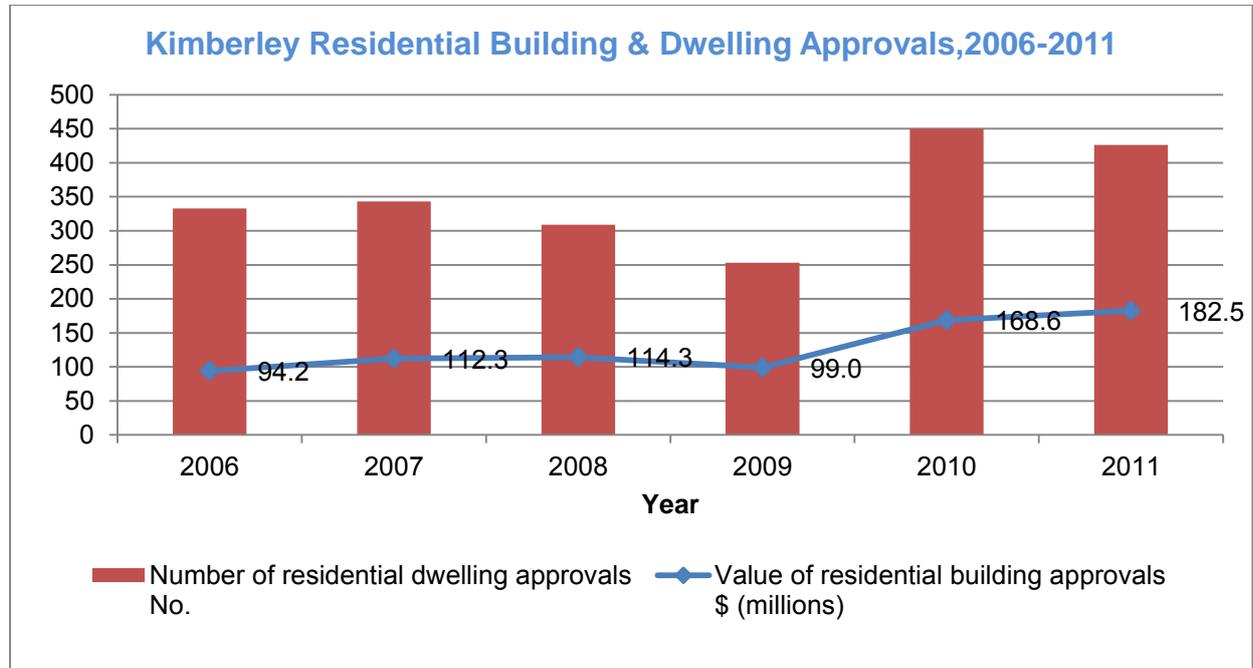


Source: ABS – Census of Population and Housing, 2011

## Housing

The Shires of Wyndham East Kimberley, Halls Creek, Derby West Kimberley and Broome experienced strong growth in building approvals in 2010 and 2011, as shown in the graph below.

**Figure 10: Residential Building & Dwelling Approvals, 2006-2011**



Source: ABS – National Regional Profile, Kimberley, 2006-2011

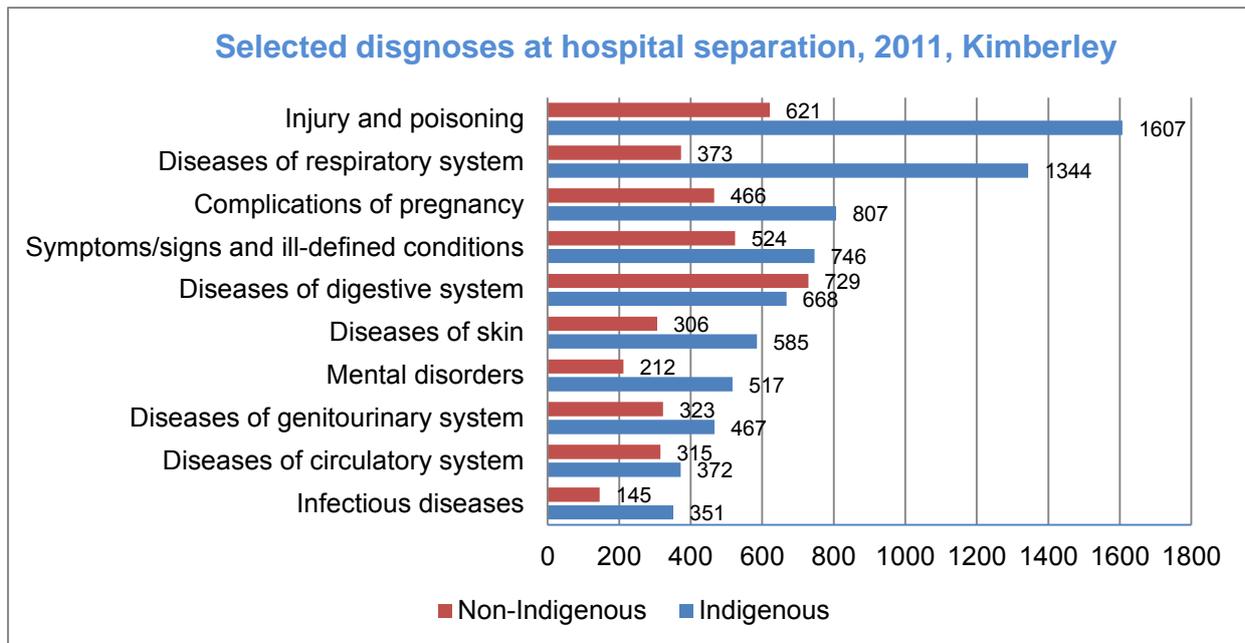
**Health**

There were 23,291 hospital separations for patients residing in the Kimberley region in 2011. About three-quarters of separations (72.5%) were for Indigenous patients. The graph below shows the top ten diagnoses for hospital separations in 2011.



Categories showing significant variation between Indigenous and non-Indigenous separations were Injury and Poisoning, Diseases of the Respiratory System, Complications of Pregnancy, Mental disorders, Diseases of Skin and Infectious Diseases.

**Figure 11: Kimberley Hospital Separations, 2011**



Source: Department of Health – Hospital Separations, Principal Diagnosis by Indigenous Status, 2011

The Kimberley region has the highest incidence of end stage kidney disease (ESKD) and sexually transmitted diseases in Western Australia. Compared to the State smoking is also significantly higher for men and women.

There are higher rates of injury, disease, mortality and morbidity amongst Indigenous people, correlating to their relatively poor socio-economic position.

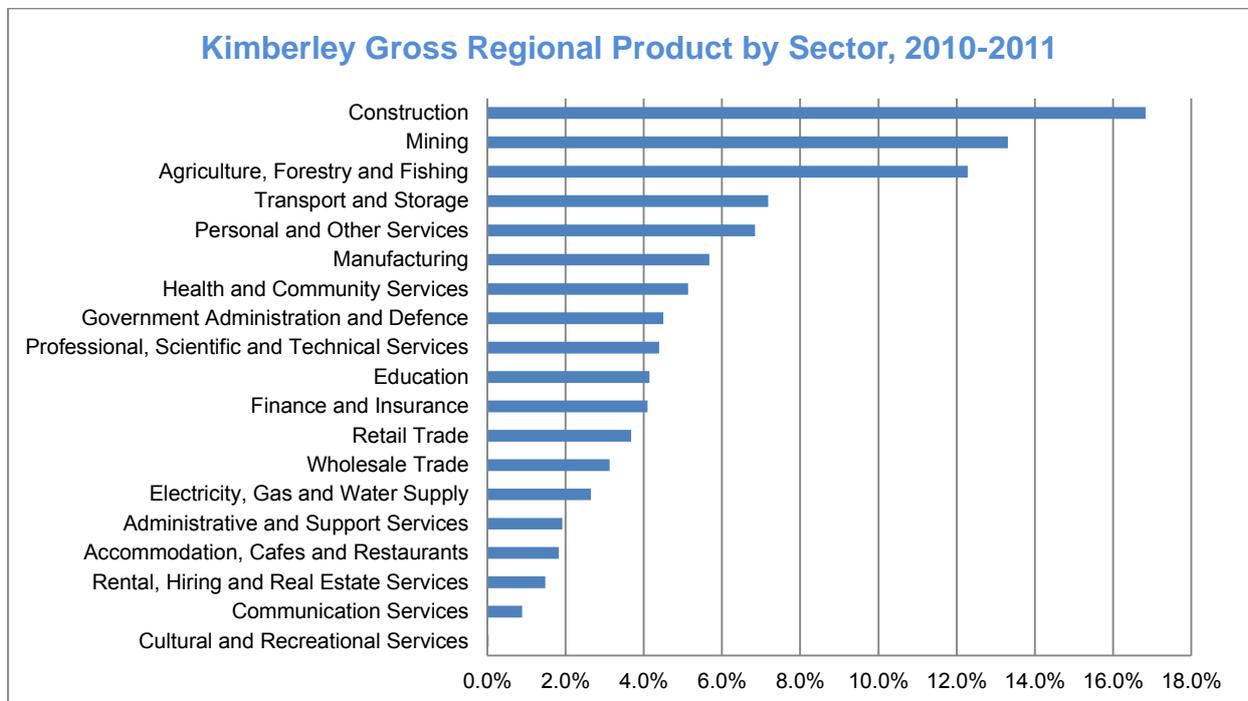
However, there is a significantly lower risk of obesity, lower rates of high blood pressure and lower rates of high cholesterol in the Kimberley region in comparison to the rest of the State.

## Economy

The Kimberley region's Gross Regional Product (GRP) was estimated at \$3.1 billion in 2010-2011. The main industry contributors were Construction, Mining, Agriculture, Forestry and Fishing. The graph below shows GRP by industry sector in 2010-2011.



**Figure 12: Kimberley Gross Regional Product, 2010-2011**



Source: Department of Regional Development and Lands – Kimberley GRP Estimates, 2010-2011

The top five industries in terms of employment from 2006 Census data were:

1. Health Care and Social Assistance (15.0%)
2. Public Administration and Safety (13.5%)
3. Education and Training (8.9%)
4. Retail Trade (8.6%)
5. Accommodation and Food Services (7.1%)

The construction, mining and agricultural sectors totalled 6.8%, 4.3% and 5.2% respectively.

The table below shows the change in employment by industry from 2001 to 2006 in the Kimberley region.

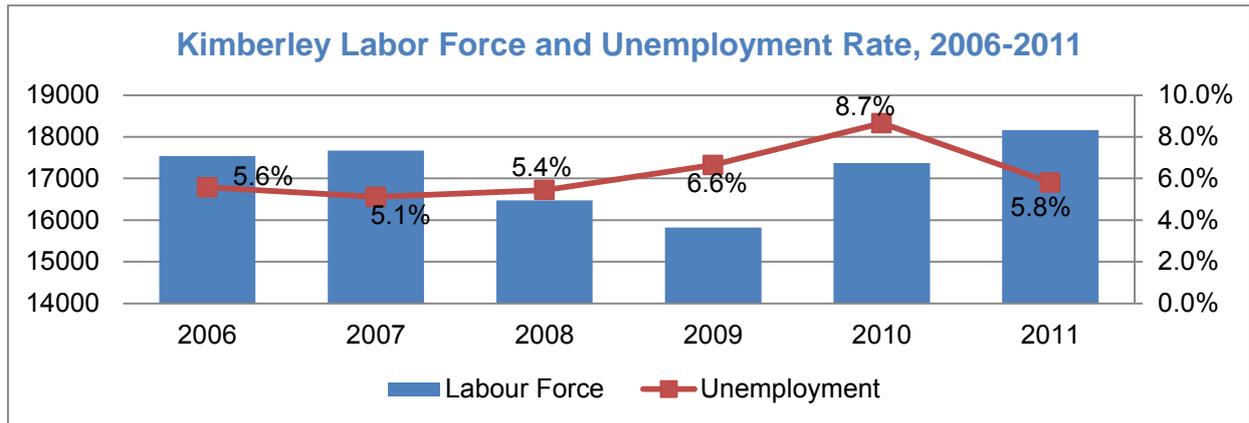
**Table 3: Employment by Industry in the Kimberley**

Industries	% of Total Labour Force	
	2006 Census	2001 Census
Health Care and Social Assistance	15.00%	9.10%
Public Administration and Safety	13.50%	18.20%
Education and Training	8.90%	8.20%
Retail Trade	8.60%	10.40%
Accommodation and Food Services	7.10%	6.70%
Construction	6.80%	5.90%
Other Services	5.30%	7.40%
Agriculture, Forestry and Fishing	5.20%	7.50%
Inadequately described/Not stated	5.20%	3.30%
Transport, Postal and Warehousing	5.10%	5.20%
Mining	4.30%	3.00%
Administrative and Support Services	3.40%	n/a
Manufacturing	3.30%	3.60%
Professional, Scientific and Technical Services	2.10%	n/a
Rental, Hiring and Real Estate Services	1.40%	5.60%
Wholesale Trade	1.20%	2.30%
Electricity, Gas, Water and Waste Services	1.00%	0.80%
Arts and Recreation Services	1.00%	1.50%
Financial and Insurance Services	0.60%	0.70%

Source: ABS – Census of Housing and Population, 2001 and 2006

The Kimberley region's labour force (total number of people employed and unemployed) has exhibited steady growth increasing from 17,539 in June 2006 to 18,159 in June 2011. The number of employed persons in the Kimberley in June 2011 was 17,105 and unemployed persons were 1,054. There was a spike in the unemployment rate from 2008 to 2010 with the onset of the Global Financial Crisis and a decrease in the unemployment rate in 2011 as global economic conditions improved.

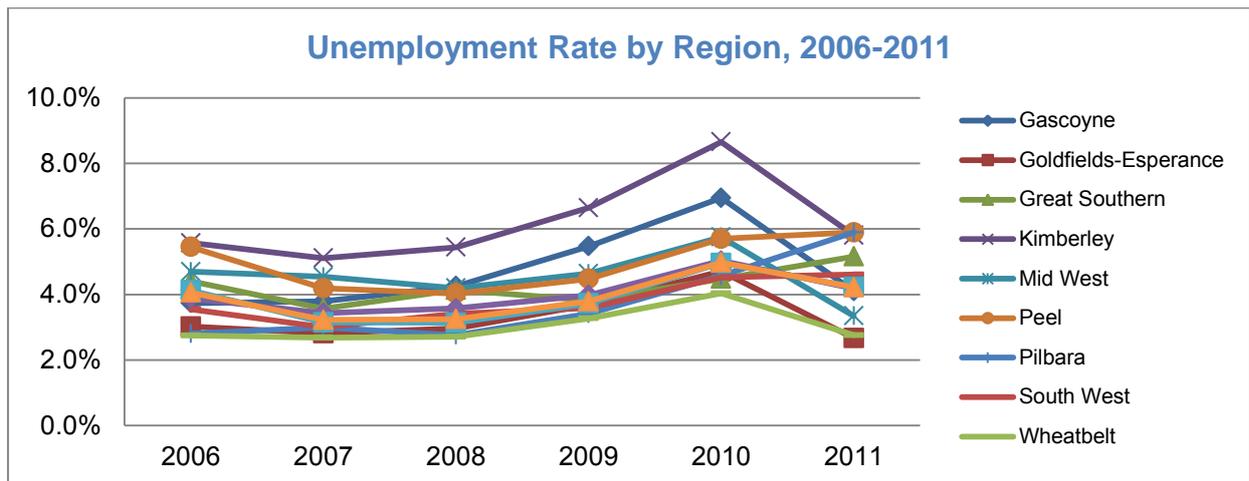
**Figure 13: Kimberley Labour Force and Unemployment Rate, 2006-2011**



Source: Department of Education, Employment and Workplace Relations - Kimberley Labour Force, 2006-2011

The Kimberley region's unemployment rate has been consistently higher than the State's unemployment rate since 2006. The unemployment rate was 5.8% in June 2011, compared to the State rate of 4.2%. The graph below shows a significant drop in the Kimberley region's unemployment rate from 2010 to 2011.

**Figure 14: Unemployment by Region, 2006-2011**

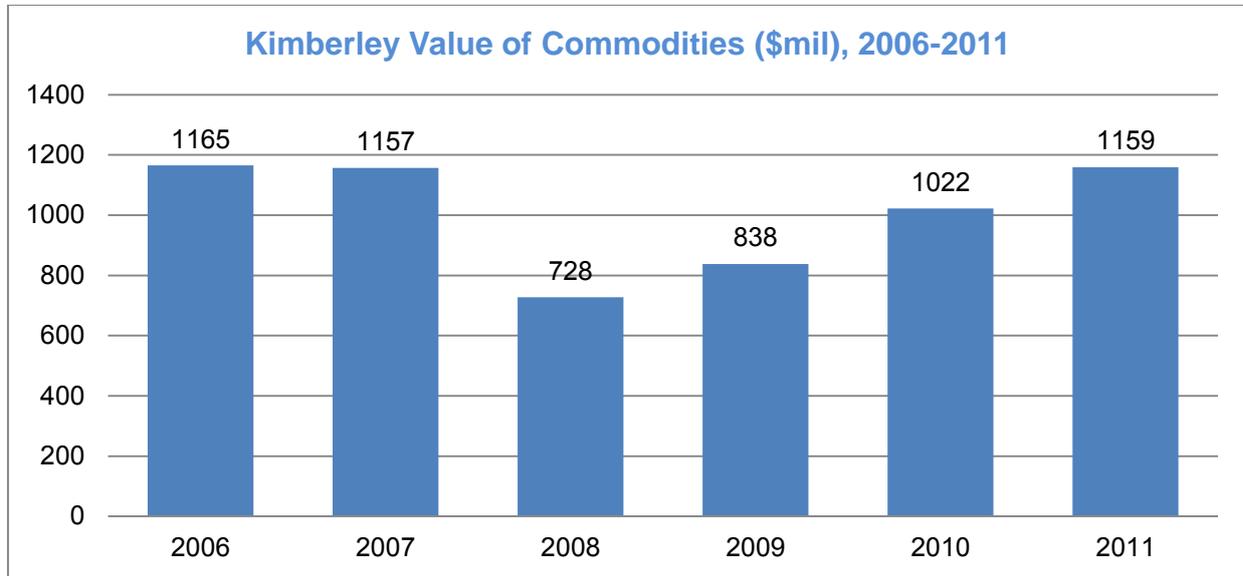


Source: Department of Education, Employment and Workplace Relations - Unemployment by Region, 2006-2011

## Mining

The mining industry is the biggest economic contributor to the Kimberley region, with output valued at \$1,159 million in 2011. The value of commodities, including petroleum, derived from the Kimberley region between 2006 and 2011 is shown in the graph below:

**Figure 15: Kimberley Value of Commodities, 2006-2011**



Source: Department of Mines and Petroleum - Kimberley Value of Commodities, 2006-2011

Diamonds are one of the region’s main mineral products with the Argyle Diamond Mine being the largest supplier of pink diamonds in the world. Diamonds are also mined at Ellendale in the Shire of Derby-West Kimberley. Diamonds, gold and silver valued at \$356 million, made up 30.7% of the mining production in 2011. High grade iron ore is mined at Cockatoo and Koolan Islands near King Sound. The iron ore represented 49.5% of mining production with a value of \$574 million. The remaining 19.7% was from other minerals including nickel, copper, cobalt and crude representing a value of \$229 million in 2011.



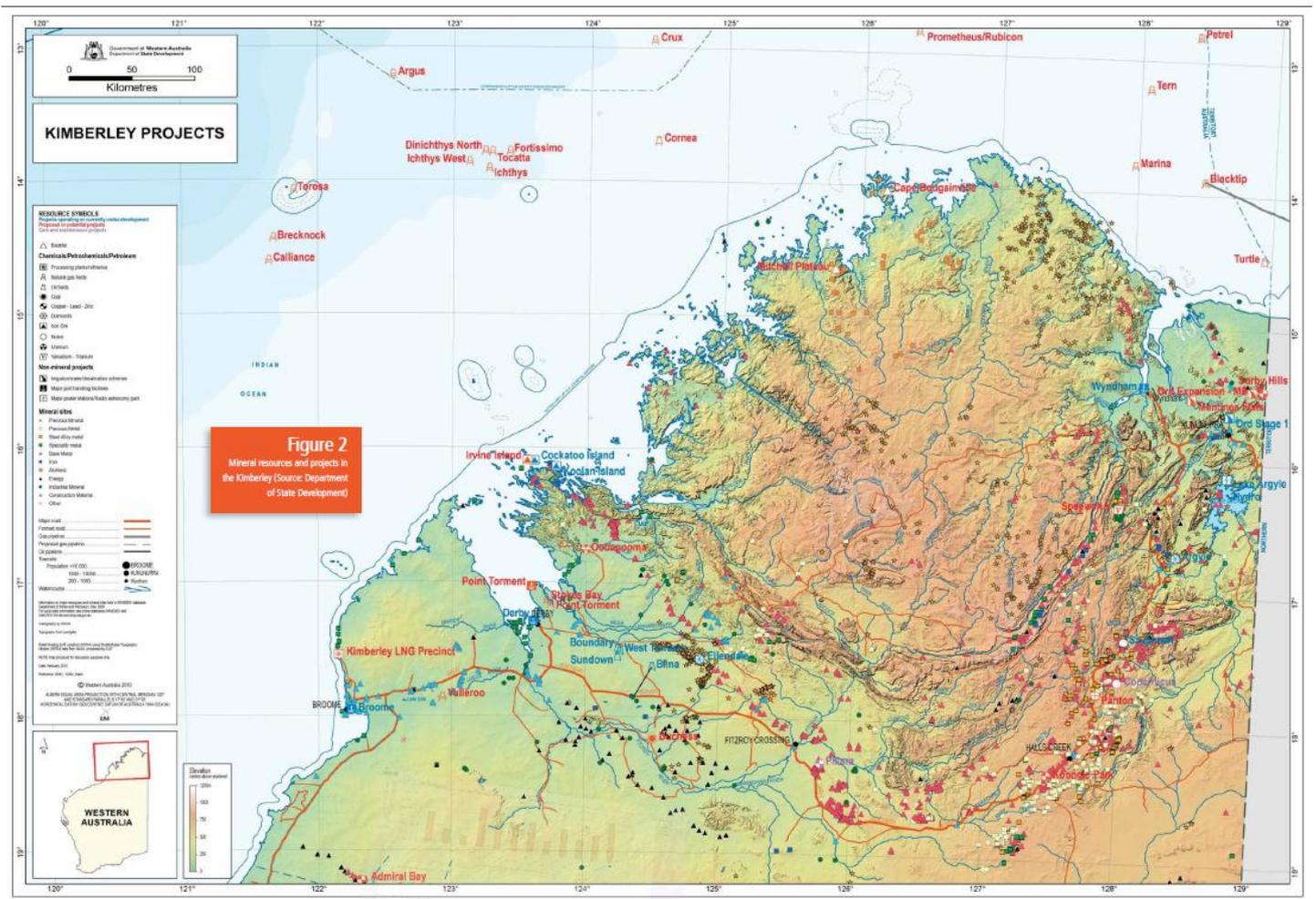
Consistent growth in the Kimberley resources sector over the last 10 years has been a major contributor to the 51 percent increase in Gross Regional Product. That growth is likely to accelerate in the years ahead, primarily driven by the development of the Browse Basin gas resource, in particular the on-shore gas processing and export from the proposed LNG precinct being established in the Kimberley by the state government, with Australian government support. It is estimated that this project will require a construction workforce of 3,500 and an

operational workforce of 520, with substantial spin-offs for small and medium businesses in the region (CME, 2010).

At the Argyle Diamond Mine operation, a decision taken in 2005 to develop underground operations will extend mine life from 2010 to beyond 2018. The Alumina Refinery (Mitchell Plateau) Agreement Act 1971 commits joint venture partners Rio Tinto and Alcoa to the ongoing study of development options, which would be focussed on the Mitchell Plateau and Cape Bougainville tenures. Beyond these major projects, a feasibility study is underway for the Duchess-Paradise thermal coal resource in the Canning Basin (Rey Resources Ltd), with advanced exploration of the largest vanadium deposit in Australia (Niplats Australia Ltd) and a range of base metals prospects underway in the East Kimberley area. There are a wide range of other projects exploring and proving resources in iron ore, platinum, uranium, diamonds, precious metals and industrial metals (CME, 2010).

The following map of the Kimberley region shows the location of mineral, petroleum and gas production and exploration activities.

**Figure 16: Mineral, petroleum and gas projects in the Kimberley region**



## Tourism

Domestic visitors to the Kimberley region in 2010 were estimated at 235,000 persons and international tourists at 40,400 persons. Tourism expenditure in 2010 was estimated to be \$265 million.

In 2010 there were an estimated 2,171 people employed directly in servicing the visitor market.

The tourism industry is expected to remain a key contributor to economic activity in the Kimberley. The expectation of industry operators is that the region is still moving through the development stage and is yet to achieve its full potential.



## Agricultural Commodities

The value of agricultural commodities has fluctuated over the years. In 1994-95, the value was \$71.6 million, gradually increasing to \$126.8 million in 2000-01, before declining to \$95.9 million in 2003-04. In 2009-10, the gross value of Kimberley agricultural commodities was \$195 million. The breakdown of this is shown in the table below:

**Table 4: Kimberley Value of Agricultural Commodities, 2010**

Commodity Label	Value (\$)	% Regional Value
Broadacre crops and cereals	2,689,039	1.40%
Other cereals for grain or seed	890,872	0.50%
Chickpeas	537,753	0.30%
Summer pulses	81,918	0.00%
Winter pulses	24,963	0.00%
Summer oilseeds	37,261	0.00%
Cultivated turf	4,139,471	2.10%
Nurseries undercover	380,805	0.20%
Nurseries outdoor	106,631	0.10%
Cereal cut for hay	652,506	0.30%
Other corps cut for hay	28,229	0.00%
Pasture cut for hay	493,946	0.30%
Total value of vegetables	20,331,630	10.40%
Mangoes	3,687,708	1.90%
All other orchard fruit	5,646	0.00%
Bananas	62,152	0.00%
All other berry or tropical fruit	441,015	0.20%
<b>Total value of crops</b>	<b>34,591,545</b>	<b>17.70%</b>
Cattle and calves slaughterings	93,186,675	47.80%
Cattle and calves - other disposals	67,089,598	34.40%
Sheep and lambs slaughterings	36,052	0.00%
Sheep and lambs - other disposals	32,907	0.00%
<b>Total value of livestock slaughterings and other disposals</b>	<b>160,345,881</b>	<b>82.20%</b>
Wool (shorn)	57,886	0.00%
Wool (other)	3,550	0.00%
<b>Total value of livestock products</b>	<b>61,436</b>	<b>0.00%</b>
<b>Total Value of Kimberley Agricultural Products</b>	<b>194,999,540</b>	<b>100.00%</b>

Source: ABS - Agricultural Resource Management Survey / Department of Regional Development, 2010

## Pastoral Industry

The majority of the Kimberley region’s land area is under pastoral lease, with an estimated 100 cattle stations.

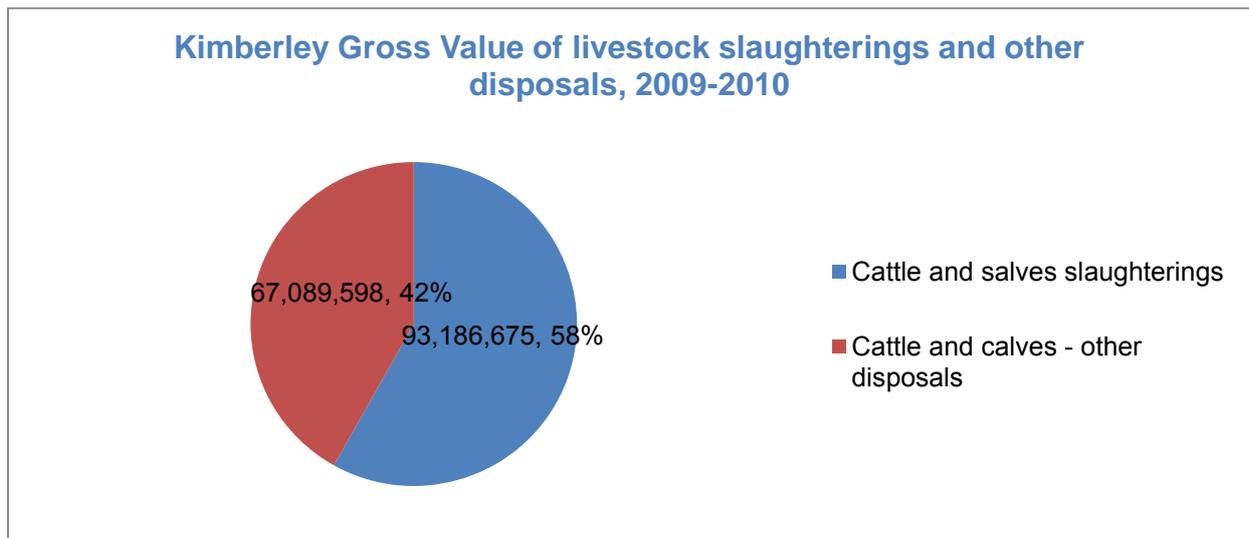
The Kimberley is the closest pastoral zone to Australia’s biggest export market, Indonesia. Annual export figures to Indonesia are around 600,000 head of cattle. Approximately 300,000 cattle are exported out of Darwin annually and between 60,000 to 80,000 are exported out of Broome each year.



The suspension of the live cattle trade to Indonesia in 2011 had a significant impact on the Kimberley region.

The value of cattle disposals from the Region was \$160 million in 2009-10, which made up 82% of the total value of agriculture.

**Figure 17: Kimberley Gross Value of Livestock Production, 2009-2010**



Source: ABS - Agricultural Resource Management Survey/Department of Regional Development, 2010

## Horticulture

The Horticulture industry has undergone considerable growth in the East Kimberley region with the 8,000 hectare, \$220 million Ord East Kimberley Expansion Project. In 2009-10, the Department of Agriculture and Food Western Australia estimated the total value of horticulture production from the Ord River Irrigation Area to be more than \$20 million.

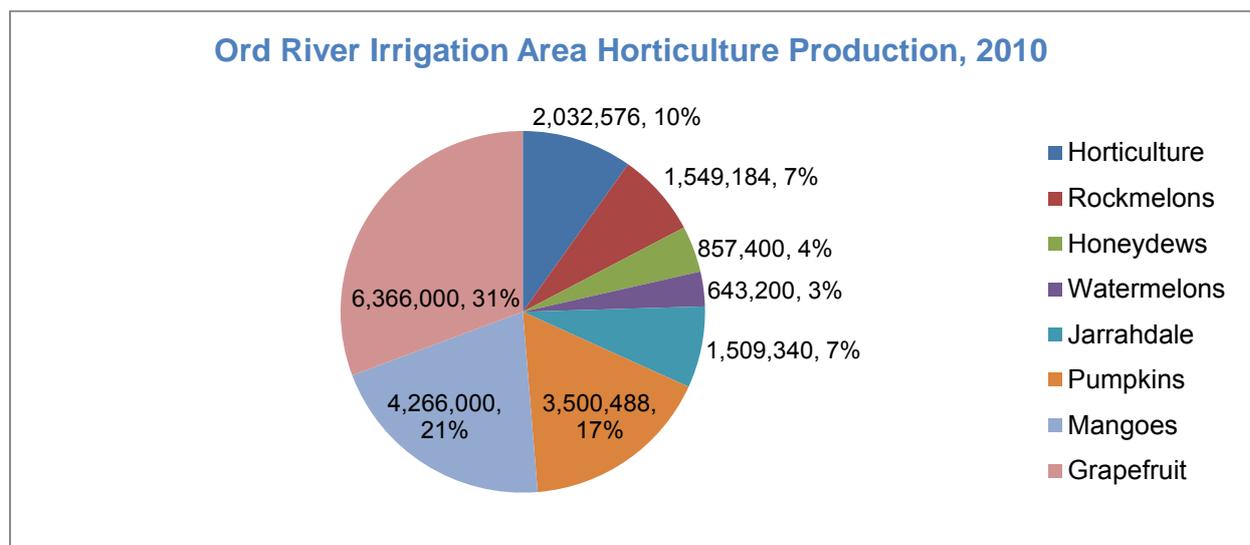


Commercial horticultural production in the West Kimberley occurs on a relatively small scale in the Derby and Broome Shires, with only a few large-scale commercial businesses.

Most of the crops grown in the Kimberley take advantage of the mild to warm 'winter', when they can be grown "out of season" to other Australian production areas

The breakdown of production by horticultural crop is shown in the pie chart below.

**Figure 18: Ord River Irrigation Area Horticulture Production, 2010**



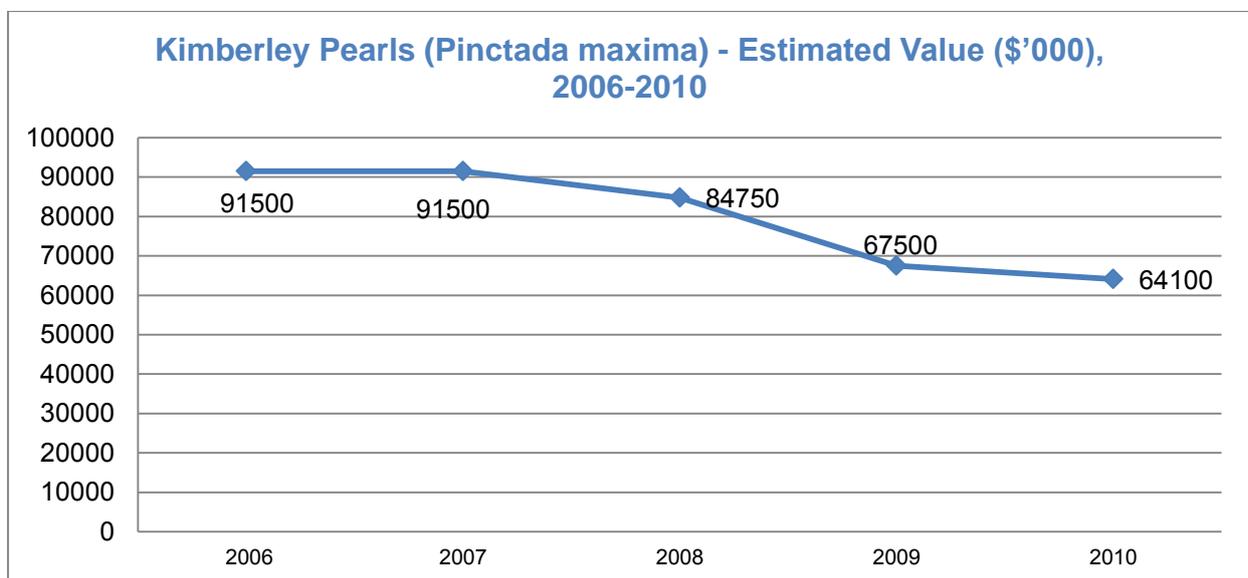
Source: Department of Agriculture and Food Western Australia - ORIA Horticulture Production, 2010

## Aquaculture

The total value of aquaculture in the Kimberley region for 2009-10 was \$67 million. Pearling is an historic industry operating out of Broome with pearl farms along the Kimberley coast. The Kimberley pearl industry peaked at \$91.5 million in 2005-06 and 2006-07, but declined to an estimated \$64.1 million in 2009-10. This decline was caused by the Global Financial Crisis in 2008-09 which has significantly impacted this industry and it was reported that during that time at least 50% of the pearling industry was in 'standby' mode, awaiting improved economic conditions.



**Figure 19: Kimberley Pearls (*Pinctada maxima*) – Estimated Value, 2006-2010**



Source: Department of Fisheries and Department of Regional Development and Lands, Kimberley Pearls (*Pinctada maxima*), 2006-2010

## 7. Strategic Drivers

The Kimberley Regional Group Strategic Planning Workshop was held on February 2012. In this workshop, the following Strategic Drivers or major forces that can shape the future of the Kimberley Region were identified together with an assessment of their relative impact, and what Local Government's regional role could potentially be.

**Table 5: Regional Strategic Drivers**

Strategic Driver	Impact				Local Government Regional Role
	Community	Economy	Environment	Governance	
<b>Housing</b> <ul style="list-style-type: none"> <li>•Affordability, availability, ownership</li> <li>•For key workers</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>•Housing construction facility/project (employment and training, Local Government key worker accommodation)</li> <li>•High level advocacy/facilitation</li> <li>•Land release</li> <li>•Public and private partnership</li> <li>•Building local capacity (regional supply)</li> <li>•Sewerage</li> </ul>
<b>Closing the Gap</b> <ul style="list-style-type: none"> <li>•High % Indigenous population</li> <li>•Accountability for service delivery</li> <li>•Indigenous services – access roads, maintenance (funding)</li> <li>•COAG Agreement</li> <li>•Housing</li> <li>•State planning</li> <li>•ICC/ROC/LIP</li> <li>•Indigenous opportunities and partnerships</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>•Advocate/lobby to influence Local Government service delivery to Aboriginal communities and improving living conditions and life outcomes</li> <li>•Participation in planning and decision making – strong engagement with the community</li> </ul>
<b>Remoteness</b> <ul style="list-style-type: none"> <li>•High level of costs - freight, buildings, insurance premiums</li> <li>•Bureaucracy don't understand</li> <li>•Link to workforce – attracting good people</li> </ul>	High	High	High (built)	High	<ul style="list-style-type: none"> <li>•Advocate for increased funding, subsidies, tax reform, etc.</li> <li>•Social infrastructure (identify and lobby for funding, building, ownership)</li> <li>•Supporting/driving regional preferred suppliers – standards, % weighting</li> </ul>

Strategic Driver	Impact				Local Government Regional Role
	Community	Economy	Environment	Governance	
<b>Tourism</b> <ul style="list-style-type: none"> <li>•Infrastructure</li> <li>•Regional tourism</li> <li>•Tourism marketing and branding</li> <li>•Network coordination</li> </ul>	High	High	High (built) Medium (natural)	High	<ul style="list-style-type: none"> <li>•Access to facilities</li> <li>•Advocacy and marketing</li> <li>•Promoting access</li> <li>•Investigate a differential rating system</li> <li>•Being the driver to enhance visitor's experience in the region (regional branding)</li> <li>•Regional approach to event management</li> <li>•Engaging Aboriginal people in the process</li> <li>•Assisting and facilitating development of Aboriginal tourism enterprises</li> </ul>
<b>Transport</b> <ul style="list-style-type: none"> <li>•Air services and airports</li> <li>•Regional arterial roads and road services</li> <li>•Port development and services</li> </ul>	High	High	High (built) Medium (natural)	High	<ul style="list-style-type: none"> <li>•Advocating for minimum service and infrastructure levels</li> <li>•Advocate and participate in State Transport Strategy</li> <li>•Development of arterial roads, air, and sea networks for the Kimberley region</li> <li>•Advocate for strategic improvements – Tanami road, ports (Point Torment), access to remote airports (RFDS), development of new and extension to existing airports</li> <li>•Promote developer/resource companies contribution to leverage Local Government costs</li> </ul>
<b>Land Tenure</b> <ul style="list-style-type: none"> <li>•Land governance</li> <li>•Pastoral use</li> <li>•Planning scheme, Local Planning strategy and policies</li> <li>•Local contribution</li> <li>•State planning policies</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>•Collaboration on model scheme</li> <li>•High level of advocacy for changes to the Works Planning Act, Mining Act, Indigenous Land Tenure, Pastoral lease and diversification, etc</li> <li>•Land Corporation</li> </ul>
<b>Resource development</b> <ul style="list-style-type: none"> <li>•Oil, gas and minerals</li> <li>•Housing affordability</li> <li>•Availability of staff</li> <li>•Impact on organisation</li> <li>•Impact on the community</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>•Advocate for mining rates on developers</li> <li>•Lobby to increase contribution through State Agreements in relation to community capacity</li> <li>•Lobby for Kimberley Development Commission funding</li> <li>•Advocate for whole of life mine planning</li> </ul>

Strategic Driver	Impact				Local Government Regional Role
	Community	Economy	Environment	Governance	
<b>Water</b> <ul style="list-style-type: none"> <li>• Allocation, supplies</li> <li>• Regional scheme</li> <li>• Water needs to supply development</li> <li>• Water licencing</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>• Regional allocation plan</li> <li>• Advocate for town water supply/security - better allocation of funding</li> <li>• Investigate the mechanism for water licencing/water trading (including sewerage and wastewater)</li> </ul>
<b>Land Use</b> <ul style="list-style-type: none"> <li>• Natural heritage listing</li> <li>• Tourism impact</li> <li>• Resource development</li> <li>• Indigenous control / management of land</li> <li>• Heritage – pastoral, indigenous, culture and historic heritage, archive support records</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>• Investigate the impact of natural heritage listing at the regional level</li> <li>• Develop State of Environment Report at regional level to guide development</li> <li>• Advocate for KRG to be part of the regional planning process.</li> <li>• Kimberley Aboriginal Caring for Country Plan</li> <li>• Conflict between pastoral land and the Department of Planning</li> </ul>
<b>Social Development</b> <ul style="list-style-type: none"> <li>• Health</li> <li>• Senior services</li> <li>• GP services, dental, allied health, St John, mental health</li> <li>• Welfare reform</li> <li>• Appropriate education</li> <li>• Youth development</li> <li>• Employment and training</li> </ul>	High	High	High (built)	High	<ul style="list-style-type: none"> <li>• Identify /lobby/advocate/collaborate for allied and general health services to the region</li> <li>• Advocate for quality of life and health outcomes in the region</li> <li>• Promote welfare reform</li> <li>• Aged care (infrastructure)</li> <li>• Education</li> <li>• Youth development and child care</li> </ul>

Source: KRG Strategic Planning Workshop, February 2012

## 8. Key Regional Issues

In addition to the Strategic Drivers there are a number of Key Regional Issues that were identified as significant in a regional context:

**Table 6: Key Regional Issues**

Key Regional Issues	Description
<b>Labour availability</b>	<ul style="list-style-type: none"> <li>• Workforce - competing demands - salary level and industries</li> </ul>
<b>Power - issue under remoteness</b>	<ul style="list-style-type: none"> <li>• Renewable energy - solar energy efficiency and tidal</li> <li>• Underground</li> <li>• Capacity</li> </ul>
<b>Tax reform</b>	<ul style="list-style-type: none"> <li>• Tax incentive</li> <li>• Zone rebate and allowances (remoteness)</li> <li>• Department of Housing threshold</li> </ul>
<b>Communication – cost associated with issue of remoteness</b>	<ul style="list-style-type: none"> <li>• Optical speed transmission</li> <li>• National Broadband Network</li> <li>• Tower for black spots</li> </ul>
<b>Waste management</b>	<ul style="list-style-type: none"> <li>• Modernise waste management</li> <li>• Recycling management</li> </ul>
<b>Emergency services</b>	<ul style="list-style-type: none"> <li>• Balancing the impact</li> <li>• Fire services</li> <li>• Drivers from the Kelty Report</li> </ul>
<b>Governance</b>	<ul style="list-style-type: none"> <li>• Diversified income</li> <li>• Investment and revenue businesses</li> <li>• Sustainability of the organisation</li> <li>• Freight income stream</li> <li>• Employment – legislative impact</li> </ul>
<b>Resource sharing</b>	<ul style="list-style-type: none"> <li>• Information Technology support</li> <li>• Mobilisation</li> </ul>
<b>Asset Management</b>	<ul style="list-style-type: none"> <li>• Advocacy for Asset Management</li> <li>• Funding for Asset Management</li> <li>• Partner with Aboriginal people</li> <li>• Closing the Gap</li> <li>• Procurement policy</li> </ul>

Source: KRG Strategic Planning Workshop, February 2012

## 9. Emerging Regional Issues

A number of emerging regional issues have also been identified by the Kimberley Regional Group:

**Table 7: Emerging Regional Issues**

Emerging Regional Issues	Stakeholders	Timeframe	Impact			
			Community	Economy	Environment	Governance
<b>Defence White Paper – Broome RAN Naval Base</b>	<ul style="list-style-type: none"> <li>• Department of Defence</li> <li>• Community of Broome</li> <li>• Chambers of Commerce</li> <li>• Aust Government</li> </ul>	10 years	<ul style="list-style-type: none"> <li>• Housing affordability</li> </ul>	<ul style="list-style-type: none"> <li>• Housing sector</li> <li>• Business development</li> <li>• Commercial centre</li> </ul>		<ul style="list-style-type: none"> <li>• Capacity issues for local Shire to manage development; dependent on development timeframes</li> </ul>
<b>Defence White Paper - RAAF Curtin Airport lease changes</b>	<ul style="list-style-type: none"> <li>• Department of Defence</li> <li>• Shire of Derby/West Kimberley</li> <li>• Community</li> <li>• Resource Sector</li> </ul>	1-5 years	<ul style="list-style-type: none"> <li>• Need to retain regular and reliable Regular Public Transport services</li> </ul>	<ul style="list-style-type: none"> <li>• Cost of relocation to Derby Airport</li> <li>• Resources sector</li> </ul>		
<b>Derby - Oil, Gas, Coal and other resource development</b>	<ul style="list-style-type: none"> <li>• Resources sector</li> <li>• Shire of Derby/ West Kimberley</li> <li>• Community</li> <li>• WA Government</li> </ul>	2-10 years	<ul style="list-style-type: none"> <li>• Social and community infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Transport infrastructure</li> <li>• Support Infrastructure</li> </ul>		
<b>Ord River Irrigation Scheme Stage 3</b>	<ul style="list-style-type: none"> <li>• WA Government</li> <li>• NT Government</li> <li>• Aust Government</li> <li>• Miriwung Gajerrong Corporation</li> <li>• Local Govt WA</li> <li>• Local Govt NT</li> </ul>	5-10 years	<ul style="list-style-type: none"> <li>• Social and Community infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Transport infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Aust and State Government requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Cross border jurisdiction changes required</li> <li>• Native title issues</li> </ul>
<b>Bonaparte Gulf – Mining, Oil and Gas exploration</b>	<ul style="list-style-type: none"> <li>• WA Government</li> <li>• NT Government</li> <li>• Aust Government</li> <li>• Local Govt WA</li> <li>• Local Govt NT</li> </ul>	5-10 years	<ul style="list-style-type: none"> <li>• Infrastructure upgrades and housing requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Major airport and port development</li> </ul>	<ul style="list-style-type: none"> <li>• Aust and State Government requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Cross border jurisdiction changes required</li> <li>• Native title issues</li> </ul>

Source: Kimberley Regional Group, June 2012

## 10. Goals, Outcomes & Strategies

The table below provides an overview of the Region’s Strategic Goals and Outcomes:

KRA	Goals	Outcomes
<b>Governance</b>	<b>A collaborative group demonstrating strong Regional Governance</b>	<ul style="list-style-type: none"> <li>• Effective and efficient governance, protocols and systems</li> <li>• Secure funding for regional priorities</li> <li>• Effective engagement with Aboriginal governance structures</li> <li>• Business efficiency and improved services through collaboration</li> <li>• Recognition of Kimberley Local Government issues and opportunities at the Regional, State, National and International levels</li> <li>• Alignment and integration of regional and local priorities for Member Councils</li> </ul>
<b>Natural Environment</b>	<b>Responsible management of the environment</b>	<ul style="list-style-type: none"> <li>• Improved institutional fire management regimes</li> <li>• Integrated waste management</li> <li>• Secure quality water supply</li> <li>• Reuse of waste water</li> </ul>
<b>Built Environment</b>	<b>Improved and secure transport, communications, community and essential services</b>	<ul style="list-style-type: none"> <li>• Attractive townscapes supporting communities</li> <li>• Improved regional arterial road network, ports and airports</li> <li>• Adequate land supply</li> <li>• High standard of infrastructure planning for Indigenous communities</li> <li>• Reliable and adequate power and communications</li> </ul>
<b>Community</b>	<b>A vibrant community based on equity, inclusion and opportunity for all</b>	<ul style="list-style-type: none"> <li>• Innovative and joined up approach to Housing development, ownership and design through community participation</li> <li>• Improved regional outcomes in education and health</li> <li>• Best practice in community services</li> <li>• Advocate for Improved range and standard of municipal services to Aboriginal communities</li> </ul>
<b>Economy</b>	<b>A sustainable and diverse economy</b>	<ul style="list-style-type: none"> <li>• Generational advantage that captures the wealth for the region</li> <li>• Improved outcomes in employment</li> <li>• A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the regions assets (human, environment and built)</li> </ul>



**Shire of Derby /  
West Kimberley**



## Governance

*Goal 1: A collaborative group demonstrating strong regional governance*

**Outcome 1.1 Effective and efficient governance, protocols and systems**

**Strategy 1.1.1 Develop/negotiate effective business processes and systems for the Kimberley Regional Group**

**Strategy 1.1.2 Establish a governance structure for the Kimberley Regional Group**

**Strategy 1.1.3 Develop and implement a regional Procurement Strategy**

**Outcome 1.2 Secure funding for regional priorities**

**Strategy 1.2.1 Develop business cases for regional priorities**

**Outcome 1.3 Effective engagement with Aboriginal governance structures**

**Strategy 1.3.1 Develop relationships and consultation processes with Aboriginal groups at a regional level**

**Outcome 1.4 Business efficiency and improved services through collaboration**

**Strategy 1.4.1 Promote and facilitate mutually beneficial, cost efficient shared services arrangements**

**Outcome 1.5 Recognition of Kimberley Local Government issues and opportunities at the regional, state, national and international levels**

**Strategy 1.5.1 Engage and collaborate with stakeholders to champion the interests of the Kimberley Regional Group and the region**

**Outcome 1.6 Alignment and integration of regional and local priorities for Member Councils**

**Strategy 1.6.1 Ensure two-way communication between the Kimberley Regional Group and Member Councils enabling local and regional perspective and priorities to be considered**



## Natural Environment

*Goal 2: Responsible management of the environment*

**Outcome 2.1 Secure quality water supply**

**Strategy 2.1.1 Advocate for improved water supply (quality and quantity) where required**

**Outcome 2.2 Integrated waste management**

**Strategy 2.2.1 Develop and implement a regional Integrated Waste Management Strategy and operational plan**

**Outcome 2.3 Improved institutional fire management regimes**

**Strategy 2.3.1 Lobby for a Northern Fire Authority Department**

**Outcome 2.4 Reuse of waste water**

**Strategy 2.4.1 Advocate for affordable and improved waste water management that considers innovative technology**



## Built Environment

*Goal 3: Improved and secure transport, communications, community and essential services*

**Outcome 3.1 Attractive townscapes supporting communities**

**Strategy 3.1.1 Develop partnerships with relevant State agencies to progress town enhancement projects across the region recognising the unique character of each individual town**

**Outcome 3.2 Improved regional arterial road network, ports and airports**

**Strategy 3.2.1 Develop and provide input to regional transport priorities, policies and plans**

**Outcome 3.3 Adequate land supply**

**Strategy 3.3.1 Advocate and investigate land development alternatives in partnership with Local Government and relevant stakeholders**

**Outcome 3.4 High standard of infrastructure planning for Indigenous communities**

**Strategy 3.4.1 Lobby the Western Australian Planning Commission (WAPC) for improvement and involvement by Shires into Community Layout Planning process**

**Outcome 3.5 Reliable and adequate power and communications**

**Strategy 3.5.1 Advocate for improved Information and Communication Technology (ICT) infrastructure for the region**

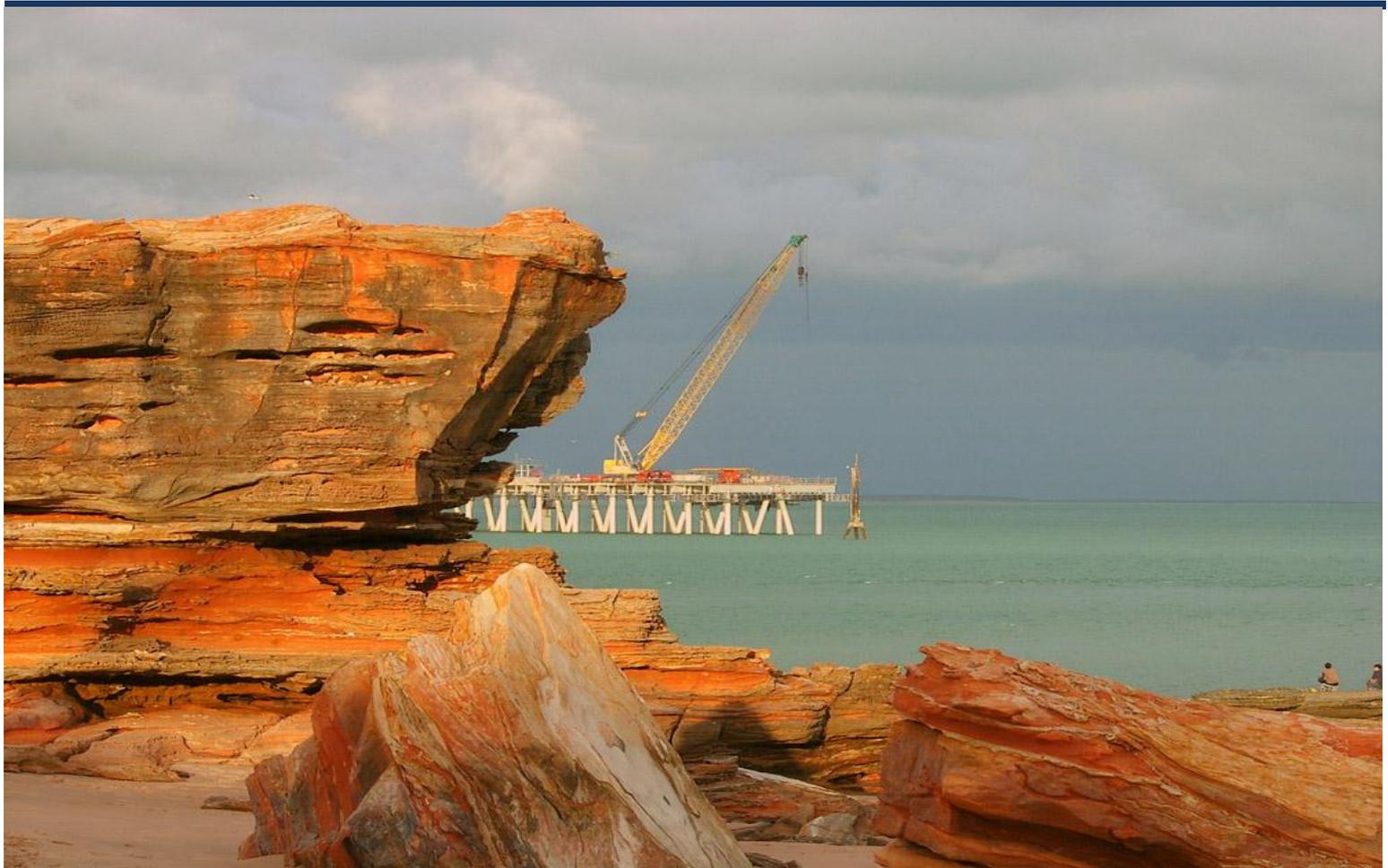
**Strategy 3.5.2 Request that the Kimberley Regional Planning Committee ensure that the Kimberley infrastructure development plan adequately forecasts utility provision for strategic needs of Kimberley Local Governments**



## Community

*Goal 4: A vibrant community based on equity, inclusion and opportunity for all*

- Outcome 4.1 Innovative and joined up approach to housing development, ownership and design through community participation**
- Strategy 4.1.1 Encourage appropriate housing design, style and appropriate management for the local environment**
  - Strategy 4.1.2 Promote a planned approach to manage population growth and access to a choice of housing options**
- Outcome 4.2 Improved regional outcomes in education and health**
- Strategy 4.2.1 Advocate for improved regional health services and infrastructure**
  - Strategy 4.2.2 Advocate for improved regional education and training opportunities**
- Outcome 4.3 Best practice in community services**
- Strategy 4.3.1 Collaborate with State Government and other agencies to ensure co-ordinated provision of services**
- Outcome 4.4 Advocate for an improved range and standard of municipal services to Aboriginal communities**
- Strategy 4.4.1 Liaise with stakeholder organisations to determine the future of municipal service delivery to Aboriginal communities**



# Economy

*Goal 5: A sustainable and diverse economy*

**Outcome 5.1** Generational advantage that captures the wealth for the region

**Strategy 5.1.1** Support regional partnerships with both private and public sector for future investment in the region

**Outcome 5.2** Improved outcomes in employment

**Strategy 5.2.1** Promote and advocate at the state and national level our region's readiness for investment opportunities

**Strategy 5.2.2** Support and advocate regional employment programs and opportunities

**Outcome 5.3** A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the region's assets (human, environment and built)

**Strategy 5.3.1** Promote tourism opportunities at a regional level

## 11. Strategic Measures

An important part of the Integrated Planning process, is the monitoring and reporting of results achieved in the implementation of the Kimberley Strategic Community Plan.

The following outcome based key performance indicators have been identified to track progress against the outcomes of this Plan. This is a preliminary list of key performance indicators which may be revised when the Plan is reviewed.

**Table 8: Strategic Measures**

KRA	Goals	Outcomes	Key Performance Measures
Governance	A collaborative group demonstrating strong regional governance	Effective and efficient governance, protocols and systems	<ul style="list-style-type: none"> <li>Shire satisfaction with the business process and systems for KRG</li> </ul>
		Secure funding for regional priorities	<ul style="list-style-type: none"> <li>Value of grants secured for regional priorities</li> </ul>
		Effective engagement with Aboriginal governance structures	<ul style="list-style-type: none"> <li>Number of Aboriginal governance bodies with which consultation is undertaken</li> </ul>
		Business efficiency and improved services through collaboration	<ul style="list-style-type: none"> <li>Shire satisfaction with the business process and systems for KRG</li> <li>% change in cost of services by Shire for shared services</li> </ul>
		Recognition of Kimberley Local Government issues and opportunities at the regional, state, national and international levels	<ul style="list-style-type: none"> <li>Progress measured by completion of action in Regional Business Plan</li> </ul>
		Alignment and integration of regional and local priorities for Member Councils	<ul style="list-style-type: none"> <li>Shire satisfaction with the business process and systems for KRG</li> </ul>
Natural Environment	Responsible management of the environment	Secure quality water supply	<ul style="list-style-type: none"> <li>% compliance with the Australian Drinking Water Guidelines for potable water supply in Shire towns</li> </ul>
		Integrated waste management	<ul style="list-style-type: none"> <li>Progress measured by completion of action in Regional Business Plan</li> <li>% of waste diverted from landfill</li> </ul>
		Improved institutional fire management regimes	<ul style="list-style-type: none"> <li>Progress measured by completion of action(s) in Regional Business Plan</li> </ul>
		Reuse of waste water	<ul style="list-style-type: none"> <li>% of waste water that is reused in Shire towns</li> </ul>

KRA	Goals	Outcomes	Key Performance Measures
Built Environment	Improved and secure transport, communications, community and essential services	Attractive townscapes supporting communities	<ul style="list-style-type: none"> <li>• % community satisfaction with town entrances and streetscapes</li> </ul>
		Improved regional arterial road network, ports and airports	<ul style="list-style-type: none"> <li>• Community satisfaction with the road network across the region</li> <li>• Community satisfaction with airports across the region</li> <li>• Industry satisfaction with port facilities across the region</li> </ul>
		Adequate land supply	<ul style="list-style-type: none"> <li>• % change in number of residential dwelling sites across the region</li> </ul>
		High standard of infrastructure planning for Indigenous communities	<ul style="list-style-type: none"> <li>• Progress measured by completion of action(s) in Regional Business Plan</li> </ul>
		Reliable and adequate power and communications	<ul style="list-style-type: none"> <li>• % community satisfaction with Information and Technology infrastructure across the region</li> <li>• % community satisfaction with power supply across the region</li> </ul>
Community	A vibrant community based on equity, inclusion and opportunity for all	Innovative and joined up approach to housing development, ownership and design through community participation	<ul style="list-style-type: none"> <li>• % community satisfaction with housing options in Shire towns</li> </ul>
		Improved regional outcomes in education and health	<ul style="list-style-type: none"> <li>• % community satisfaction with regional health services and facilities</li> <li>• % community satisfaction with regional education and training opportunities</li> </ul>
		Best practice in community services	<ul style="list-style-type: none"> <li>• % community satisfaction with community services across the region</li> </ul>
		Advocate for an improved range and standard of municipal services to Aboriginal communities	<ul style="list-style-type: none"> <li>• Progress measured by completion of action(s) in Regional Business Plan</li> </ul>

KRA	Goals	Outcomes	Key Performance Measures
Economy	A sustainable and diverse economy	Generational advantage that captures the wealth for the region	<ul style="list-style-type: none"> <li>Value of regional investment through public and private sector partnership developments</li> </ul>
		Improved outcomes in employment	<ul style="list-style-type: none"> <li>% change in employment across the region</li> </ul>
		A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the region's assets (human, environment and built)	<ul style="list-style-type: none"> <li>% change in the number of visitors across the region</li> <li>% change in visitor's spending across the region</li> <li>% change in number of nights spent by visitors across the region</li> </ul>

## 12. Stakeholder Alignment

The Plan recognises the importance of partnerships with external stakeholders, including Commonwealth, State and Local Governments to ensure that there is a collaborative approach to providing appropriate services and infrastructure in the Kimberley region. The table below shows the alignment of key stakeholder plans with the strategies of this Plan.

**Table 9: Stakeholder Alignment**

No	Strategies	Related Plan Alignment	Plan Origin
1.1.1	Develop/negotiate effective business processes and systems for the Kimberley Regional Group	None identified	None
1.1.2	Establish a governance structure for the Kimberley Regional Group	None identified	None
1.1.3	Develop and implement a regional Procurement Strategy	Indigenous Economic Development Strategy 2011–2018	Commonwealth
1.2.1	Develop business cases for regional priorities	KDC Strategic Plan 2009	State
1.3.1	Develop relationships and consultation processes with Aboriginal groups at a regional level	Regional Development Policy Framework 2011	State
		Kimberley Science and Conservation Strategy 2011	State
		Indigenous Economic Development Strategy 2011–2018	Commonwealth
		A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011	Local Govt
		National Partnership Agreement on Remote Service Delivery 2008	Commonwealth
1.4.1	Promote and facilitate mutually beneficial, cost efficient shared services arrangements	Gascoyne Strategic Plan 2011	Regional
1.5.1	Engage and collaborate with stakeholders to champion the interests of the Kimberley Regional Group and the region	Pilbara Regional Council Strategic Plan 2011-2014	Regional
1.6.1	Ensure two-way communication between the Kimberley Regional Group and Member Councils enabling local and regional perspective and priorities to be considered	Shire of Broome Strategic Plan 2011 - 2016	Shire
2.1.1	Advocate for improved water supply (quality and quantity) where required	Kimberley Regional Water Plan 2010–2030	State
		Kimberley Science and Conservation Strategy 2011	State
2.2.1	Develop and implement a regional Integrated Waste Management Strategy and operational plan	Western Australian Waste Strategy 2012	State
2.3.1	Lobby for a Northern Fire Authority Department	Regional Plan Kimberley 2011	Commonwealth

No	Strategies	Related Plan Alignment	Plan Origin
2.4.1	Advocate for affordable and improved waste water management considering innovative technology	Kimberley Regional Water Plan 2010–2030	State
3.1.1	Develop partnership with relevant state agencies to progress town enhancement projects across the region recognising the unique character of each individual town	State Planning Strategy 1997	State
3.2.1	Develop and provide input to regional transport priorities, policies and plans	Freight & Logistics Services in the East Kimberley Region 2010	State
		Roads 2025 Regional Road Development Strategy Kimberley, 2007	Local Govt
		Regional Plan Kimberley 2011	Commonwealth
		Local Government Roads and Transport Strategy 2006-2016	Local Govt
		State Planning Strategy 1997	State
3.3.1	Advocate and investigate land development alternatives in partnership with Local Government and relevant stakeholders	Affordable Housing Strategy 2010-2020	State
		State Planning Strategy 1997	State
3.4.1	Lobby the Western Australian Planning Commission (WAPC) for improvement and involvement by Shires into Community Layout Planning process	Indigenous Economic Development Strategy 2011–2018	Commonwealth
		National Partnership Agreement on Remote Service Delivery 2008	Commonwealth
		State Planning Policy 1997	State
3.5.1	Advocate for improved Information and Communication Technology (ICT) infrastructure for the region	None identified	External
3.5.2	Request that the Kimberley Regional Planning Committee ensure that the Kimberley infrastructure development plan adequately forecasts utility provision for strategic needs of Kimberley Local Governments	Western Australian State Sustainability Strategy 2003	State
		Energy 2031 - Strategic Energy Initiative Directions Paper, 2011	State
		A Sustainable Street Lighting Project Proposal for Western Australia 2011	Local Govt
4.1.1	Encourage appropriate housing design, style and appropriate management for the local environment	Affordable Housing Strategy 2010-2020	State
4.1.2	Promote a planned approach to manage population growth and access to a choice of housing options	State Sustainability Strategy – Western Australia 2003	State
4.2.1	Advocate for improved regional health services and infrastructure	Regional Development Policy Framework 2011	State
		Regional Plan Kimberley 2011	Commonwealth
		National Partnership Agreement on Closing the Gap in Indigenous Health Outcomes 2009	Commonwealth

No	Strategies	Related Plan Alignment	Plan Origin
		Western Australian Strategic Plan for Safety and Quality in Health Care 2008-2013	State
		WA Health Operational Plan 2010-2011	State
		WA Health Strategic Intent 2010-2015	State
4.2.2	Advocate for improved regional education and training opportunities	Regional Development Policy Framework 2011	State
		Regional Plan Kimberley 2011	Commonwealth
		National Education Agreement 2009	Commonwealth
4.3.1	Collaborate with State Government and other agencies to ensure co-ordinated provision of services	Shire of Broome Strategic Plan 2011 - 2016	Shire
4.4.1	Liaise with stakeholder organisations to determine the future of municipal service delivery to Aboriginal communities	A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011	Local Govt
5.1.1	Support regional partnerships with both private and public sector for future investment in the region	KDC Strategic Plan 2009	State
5.2.1	Promote and advocate at the state and national level our region's readiness for investment opportunities	KDC Strategic Plan 2009	State
5.2.2	Support and advocate regional employment programs and opportunities	Regional Plan Kimberley 2011	Commonwealth
		Kimberley Tafe Strategic Plan 2010-2012	State
5.3.1	Promote tourism opportunities at a regional level	Regional Plan Kimberley 2011	Commonwealth
		Tourism Western Australia Strategic Plan 2010-2011	State
		Australia's North West Tourism Development Priorities 2010-2015	State

A more detailed version of the Related Plan alignment can be found in Appendix 1.

The following table identifies the top five stakeholder plans in terms of related strategy alignment:

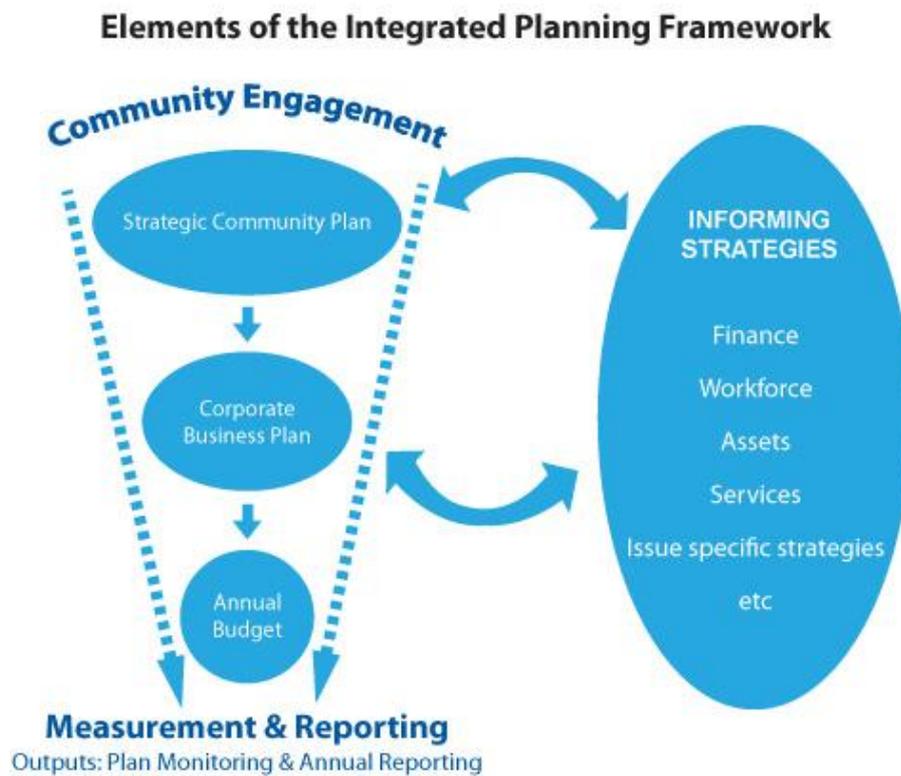
**Table 10: Alignment of Strategies with Key Stakeholder Related Plans**

Key Stakeholder Related Plan	Number of Strategies Aligned
Regional Plan Kimberley 2011	6
Regional Development Policy Framework	3
KDC Strategic Plan	3
Indigenous Economic Development Strategy 2011–2018	3
National Partnership Agreement on Remote Service Delivery	2

### 13. Regional Strategic Plan Implementation

The Kimberley Strategic Community Plan drives the development of the Regional Business Plan, which in turn drives the Implementation Plan and Annual Budget (refer figure below).

**Figure 20: Local Government Integrated Planning Framework**



The progressive linking in of informing strategies will ensure that all components of the Kimberley Regional Group’s operations are integrated into the planning and resourcing framework.

Implementation of the Kimberley Strategic Community Plan is achieved through the development of priority actions and projects as part of the Regional Business Planning and subsequent Implementation Planning processes.

Key performance indicators at the strategic and operational levels provide valuable information on how effectively and efficiently the Kimberley Strategic Community Plan and the Regional Business Plan have been implemented.

## 14. Glossary

ABS	Australian Bureau of Statistics
CEO	Chief Executive Officer
CME	Chamber of Minerals and Energy WA
COAG	Council of Australian Governments
DEC	Department of Environment and Conservation
DLG	Department of Local Government
EOI	Expression of Interest
FaHCSIA	Department of Families Housing Community Services and Indigenous Affairs
FTE	Full Time Equivalent
GIS	Geographic Information System
Govt	Government
IT	Information Technology
KDC	Kimberley Development Commission
KPI	Key Performance Indicator
KRG	Kimberley Regional Group
LG	Local Government
OHS	Occupational Health and Safety
RDA	Regional Development Australia
SDWK	Shire of Derby West Kimberley
SHC	Shire of Halls Creek
SOB	Shire of Broome
SWEK	Shire of Wyndham East Kimberley
WA	Western Australia
WAAMI	Western Australian Asset Management Improvement
WALGA	Western Australian Local Government Association
WAPC	Western Australian Planning Commission

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## 16. Appendix 1: Stakeholder Alignment

### Goal 1: A collaborative group demonstrating strong Regional Governance

#### Outcome 1.1: Effective and efficient governance, protocols and systems

**Strategy 1.1.1: Develop/negotiate effective business processes and systems for the Kimberley Regional Group**

Related Plans	Strategy Alignment
None identified	Not Applicable

**Strategy 1.1.2: Establish a governance structure for the Kimberley Regional Group**

Related Plans	Strategy Alignment
None identified	Not Applicable

**Strategy 1.1.3: Develop and implement a regional Procurement Strategy**

Related Plans	Strategy Alignment
Indigenous Economic Development Strategy 2011–2018	<p><b>Priority 3:</b> Skills Development and Jobs</p> <p><b>Objective 3.4</b> Improve Indigenous employment outcomes through government procurement and service delivery</p> <p><b>Strategy:</b> Reform government procurement to improve Indigenous employment outcomes from government investment</p>

#### Outcome 1.2: Secure funding for regional priorities

**Strategy 1.2.1: Develop business cases for regional priorities**

Related Plans	Strategy Alignment
KDC Strategic Plan 2009	<p><b>Service 2:</b> The Commission will identify, co-ordinate and promote through its major strategies the establishment, improvement and diversification of the region's infrastructure, industries and enterprises.</p> <p><b>Strategy:</b> Facilitate access to Funding for Initiatives in the Region - The Commission will facilitate the allocation of direct grant funding to significant projects and identify and facilitate appropriate funding from other sources.</p>

**Outcome 1.3: Effective engagement with Aboriginal governance structures**

***Strategy 1.3.1: Develop relationships and consultation processes with Aboriginal groups at a regional level***

Related Plans	Strategy Alignment
Regional Development Policy Framework 2011	<p><b>Priority 6:</b> Leadership and Decision-making</p> <p><b>Outcomes:</b></p> <p>Emerging community leaders to be identified and developed and encouraged to participate in local and regional development initiatives.</p> <p>Regional groups to be well represented in decision-making and planning forums on matters that impact upon the wellbeing and development of regional communities and businesses.</p> <p>Strategic planning for regional land use, service delivery and infrastructure provision to be well integrated across and within State, Commonwealth and Local Governments. This integration includes the participation of private and not-for-profit sectors.</p> <p>Indigenous leaders to be encouraged to develop local solutions to regional and Indigenous challenges and to enhance Indigenous representation at all levels of government policy development, program roll-out and service delivery.</p>
Kimberley Science and Conservation Strategy 2011	<p><b>Theme 2:</b> Employing and involving local Aboriginal people and maintaining the Kimberley's rich culture</p> <p><b>Objective:</b> To promote joint management and recognition of Aboriginal customary activities</p> <p><b>Strategy:</b></p> <p>Involve Aboriginal people in managing land and sea country through joint management arrangements as well as in making decisions and being involved in the planning and implementation of land management activities.</p>
Indigenous Economic Development Strategy 2011–2018	<p><b>Priority 1:</b> Strengthening Foundations</p> <p><b>Objective 1.7</b> Support good governance and increase coordinated government</p> <p><b>Strategies:</b></p> <p>Support good corporate governance</p> <p>Increase place based partnerships</p>
A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011	<p><b>Strategy Component 3.5:</b> Governance Development and Community Engagement</p> <p><b>Outcomes:</b></p> <p>Improved community engagement to underpin council decision-making that better reflects local community aspirations, needs and priorities.</p>
National Partnership Agreement on Remote Service Delivery 2008	<p><b>C9 Indigenous engagement principle:</b> Engagement with Indigenous men, women and children and communities should be central to the design and delivery of programs and services.</p>

**Outcome 1.4: Business efficiency and improved services through collaboration**

***Strategy 1.4.1: Promote and facilitate mutually beneficial, cost efficient shared services arrangements***

Related Plans	Strategy Alignment
Gascoyne Strategic Plan 2011	<p><b>Outcome R4.5:</b> Improved collaborative business performance and quality of service delivery.</p> <p><b>Strategy R4.5.1:</b> Promote and facilitate organisational enhancement through the delivery of shared services.</p> <p><b>Strategy R4.5.2:</b> Promote and support the implementation of organisational performance management systems aligned to Strategic Community Plans.</p>

**Outcome 1.5: Recognition of Kimberley Local Government issues and opportunities at the regional, state, national and international levels**

***Strategy 1.5.1: Engage and collaborate with stakeholders to champion the interests of the Kimberley Regional Group and the region***

Related Plans	Strategy Alignment
Pilbara Regional Council Strategic Plan 2011-2014	<p><b>Key Result Area 2:</b> A Voice for the Pilbara</p> <p><b>Strategy 2:</b> Improve collaboration and communication with key regional bodies to champion the interests of the PRC and the Region.</p>

**Outcome 1.6: Alignment and integration of regional and local priorities for Member Councils**

***Strategy 1.6.1: Ensure two-way communication between the Kimberley Regional Group and Member Councils enabling local and regional perspective and priorities to be considered***

Related Plans	Strategy Alignment
Shire of Broome Strategic Plan 2011-2016	<p><b>Strategy 1:</b> Work with agencies and others to ensure affordable and equitable services and infrastructure.</p> <p><b>Outcome 2:</b> Actively participate in the Kimberley Zone of Local Government and the RLG</p> <p><b>Action:</b> Ensure two-way communication between the Zone and Shire of Broome Council exists allowing local perspective and priorities to be considered by Zone and vice versa</p>

**Goal 2: Responsible management of the environment**

**Outcome 2.1: Secure quality water supply**

***Strategy 2.1.1: Advocate for improved water supply (quality and quantity) where required***

Related Plans	Strategy Alignment
Kimberley Regional Water Plan 2010–2030	<p><b>Strategic Objective 1:</b> Understand Kimberley water resources and water systems</p> <p><b>Strategy 1.3:</b> Understand urban and industry water resource demand and use</p> <p><b>Strategic Objective 4:</b> Sustain and grow Kimberley towns and communities</p> <p><b>Strategy 4.1:</b> Drive improvement of water services and source protection in remote communities</p> <p><b>Strategy 4.2:</b> Protect and develop drinking water sources</p>
Kimberley Science and Conservation Strategy 2011	<p><b>Objective:</b> To plan for the future</p> <p><b>Strategy:</b>                      Implement the strategic directions and actions for sustainable management of water resources and water services in the Kimberley to 2030 as outlined in the Kimberley regional water plan.</p>

**Outcome 2.2: Integrated waste management**

***Strategy 2.2.1: Develop and implement a regional Integrated Waste Management Strategy and operational plan***

Related Plans	Strategy Alignment
Western Australian Waste Strategy 2012	<p><b>Strategic objective 3:</b> Develop best practice guidelines, measures and reporting frameworks and promote their adoption.</p> <p><b>Incentive strategies:</b></p> <p>3 f. Fund trials of systems that achieve best practice outcomes to determine their applicability to WA.</p> <p>3 g. Provide funding support to encourage the adoption of systems that achieve best practice outcomes.</p> <p><b>Strategic objective 5:</b> Communicate messages for behaviour change and promote its adoption, and acknowledge the success of individuals and organisations that act in accord with the aims and principles in the Strategy and assist in its implementation.</p> <p><b>Incentive strategies:</b></p> <p>5 e. Develop and support programs and initiatives, including awards, that acknowledge, celebrate and reward excellence in waste avoidance, resource recovery and reduced landfilling behaviours and outcomes and that contribute to the implementation of this Strategy.</p>

**Outcome 2.3: Improved institutional fire management regimes**

***Strategy 2.3.1: Lobby for a Northern Fire Authority Department***

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<b>Goal:</b> More resources and staffing allocated to essential services <b>Strategy:</b> Increase resources to Police, Fire, Ambulance and FESA in the region

**Outcome 2.4: Reuse of waste water**

***Strategy 2.4.1: Advocate for affordable and improved waste water management that considers innovative technology***

Related Plans	Strategy Alignment
Kimberley Regional Water Plan 2010–2030	<b>Strategic objective 1:</b> Understand Kimberley water resources and water systems <b>Strategy 1.2:</b> Understand threats and impacts on water resources  <b>Strategic Objective 4:</b> Sustain and grow Kimberley towns and communities <b>Strategy 4.1:</b> Drive improvement of water services and source protection in remote communities <b>Strategy 4.4:</b> Realise the potential of wastewater <b>Strategy 4.5:</b> Encourage water sensitive communities

**Goal 3: Improved and secure transport, communications, community and essential services**

**Outcome 3.1: Attractive townscapes supporting communities**

***Strategy 3.1.1: Develop partnerships with relevant State agencies to progress town enhancement projects across the region recognising the unique character of each individual town***

Related Plans	Strategy Alignment
State Planning Strategy 1997	<p><b>Community Planning Priorities:</b>                      Develop Liveable Neighbourhoods: Community Design Code firstly for outer and middle suburbs and subsequently for inner suburbs and regional towns.                      Prepare commercial centres strategies for key regional centres to be incorporated in district schemes.</p> <p><b>General Planning Priorities for Regions:</b>                      The State Government assists local governments with the preparation of joint town planning schemes where regional issues cross local government boundaries.</p>

**Outcome 3.2: Improved regional arterial road network, ports and airports**

***Strategy 3.2.1: Develop and provide input to regional transport priorities, policies and plans***

Related Plans	Strategy Alignment
Freight & Logistics Services in the East Kimberley Region 2010	<p><b>Strategic Themes:</b>                      (1) the need for targeted and scalable port development,                      (2) the need for investment in road and freight infrastructure to support the growth across the region</p> <p><b>Key findings</b>                      Need to undertake targeted and scalable port investment as demand grows                      Need to ensure that road networks and related freight infrastructure are enhanced to support the freight task</p>
Roads 2025 Regional Road Development Strategy Kimberley, 2007	Specific development strategies for 9 State Roads and 51 Local Government Roads.

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<p><b>Goals:</b>                      To demonstrate the economic benefits of provision of essential transport routes for resources industries.                      All year access and linkages.</p> <p><b>Strategies:</b>                      Identify what infrastructure upgrade requirements are essential for the on-going expansion of resources industries in the region particularly road networks, ports, and air routes                      Rebuild and upgrade Highway 1                      Lobby for public transport services in towns                      Lobby for affordable inter and intra state transport                      Lobby for support for regular public transport between Asia and the Kimberley region</p>
Local Government Roads and Transport Strategy 2006-2016	<p><b>Objectives:</b>                      Permanent funding arrangements for local government to ensure a well-managed and sustainable local road system that provides appropriate access and levels of service for all communities.                      Transport systems in urban areas that provide a balance between urban amenity, freight efficiency and viable alternatives to the use of private motor vehicles.                      Equitable access, particularly to essential services, through integrated planning and the provision of appropriate transport services for all regional communities.                      Efficient movement of freight vehicles through local communities in a way which recognises community concerns for safety and amenity.                      The long-term financial sustainability of local road and transport systems.</p>
State Planning Strategy 1997	<p><b>8.2.1 Kimberley Region</b>  <b>Strategy:</b> Provide strategic transport linkages within and to the Kimberley.</p>

**Outcome 3.3: Adequate land supply**

***Strategy 3.3.1: Advocate and investigate land development alternatives in partnership with Local Government and relevant stakeholders***

Related Plans	Strategy Alignment
Affordable Housing Strategy 2010-2020	<p><b>Strategy Two - Improve Housing Supply</b>                      Housing strategies will be developed by local government authorities and regional bodies</p> <p><b>Key Reform Areas: 4. Develop Alternative Tenures</b>  <b>Strategy:</b> Alternative tenure arrangements will be explored.</p>
State Planning Strategy 1997	<p><b>Strategies:</b>                      3.4: Coordination of Land Supply                      4.0: Plans for Our Future - The plans will allocate land uses and establish land development priorities in particular regions of the State.</p>

**Outcome 3.4: High standard of infrastructure planning for Indigenous communities**

***Strategy 3.4.1: Lobby the Western Australian Planning Commission (WAPC) for improvement and involvement by Shires into Community Layout Planning process***

Related Plans	Strategy Alignment
Indigenous Economic Development Strategy 2011–2018	<p><b>Objective 1.2</b> Ensure infrastructure investments support economic development</p> <p><b>Strategies:</b>                      Improve essential infrastructure</p>
National Partnership Agreement on Remote Service Delivery 2008	<p><b>Outputs:</b>                      (i) the identification of gaps in priority local infrastructure</p>
State Planning Policy 1997	<p><b>3.2 Aboriginal Communities</b></p> <p><b>Objectives:</b>                      To provide for the recognition of Aboriginal settlements through local planning schemes and strategies.                      To collaboratively plan for the orderly and coordinated development of Aboriginal settlements.</p>

**Outcome 3.5: Reliable and adequate power and communications**

***Strategy 3.5.1: Advocate for improved Information and Communication Technology (ICT) infrastructure for the region***

Related Plans	Strategy Alignment
None identified	Not Applicable

***Strategy 3.5.2: Request that the Kimberley Regional Planning Committee ensure that the Kimberley infrastructure development plan adequately forecasts utility provision for strategic needs of Kimberley Local Governments***

Related Plans	Strategy Alignment
Western Australian State Sustainability Strategy 2003	<b>Objective:</b> Adoption of best practice energy management in the Western Australian community, including government.
Energy 2031 - Strategic Energy Initiative Directions Paper, 2011	<b>Objective:</b> Continue the State Underground Power Program, following review and adjustment to improve cost effectiveness
A Sustainable Street Lighting Project Proposal for Western Australia 2011	<b>Objectives:</b> To enhance Local Governments capacity to buffer the effects of energy pricing increases and street lighting maintenance costs over time To identify the Capital Cost of a replacement program for WA and how this may be appropriately shared by all levels of government for the benefit of the Australian community.

**Goal 4: A vibrant community based on equity, inclusion and opportunity for all**

**Outcome 4.1: Innovative and joined up approach to housing development, ownership and design through community participation**

***Strategy 4.1.1: Encourage appropriate housing design, style and appropriate management for the local environment***

Related Plans	Strategy Alignment
Affordable Housing Strategy 2010-2020	<p><b>Strategy Two - Improve Housing Supply</b>                      Housing strategies will be developed by local government authorities and regional bodies</p> <p><b>Key Complementary Actions</b>                      Work with industry and local government to encourage more diversity in the size, nature and choice of dwellings offered in the marketplace, in large part through demonstration trials and wider choice in local housing strategies.</p> <p>Explore a best practice model for local government development approvals                      Use public/private partnerships to leverage government assets, attract private investment and trial innovative developments.</p>

***Strategy 4.1.2: Promote a planned approach to manage population growth and access to a choice of housing options***

Related Plans	Strategy Alignment
State Sustainability Strategy – Western Australia 2003	<p><b>Vision:</b> A responsive housing system in Western Australia that meets the changing needs, aspirations and choices of all citizens in a sustainable way.</p> <p><b>Objectives:</b>                      Provide the appropriate mix of public housing, affordable private housing and community housing.                      Ensure that this housing provides on-going sustainability benefits.</p> <p><b>Strategies:</b>  <b>5.13:</b> Develop a Community Housing Framework involving standards and accreditation to ensure world best practice in the provision of this housing type and create opportunities for partnerships.  <b>5.14:</b> Use the sustainability agenda to facilitate the community housing sector through its ability to create synergies such as community scale technologies, local government support and ethical investment.</p>

**Outcome 4.2: Improved regional outcomes in education and health**

***Strategy 4.2.1: Advocate for improved regional health services and infrastructure***

Related Plans	Strategy Alignment
<p>Regional Development Policy Framework 2011</p>	<p><b>Priority 3: Health</b>                      Outcomes:                      Health infrastructure and service delivery that reflects the needs of each region.                      Regional delivery of health services coordinated across all levels of government to provide universal access to healthcare for all regional residents.                      Regional patients to receive timely, high quality and continuous health care that meets their needs throughout their lives.                      Older regional residents to have access to health and community support services that promote independent and healthy living.                      Health professionals and allied health workers, who are attracted to live, work and remain in regional areas.</p>
<p>Regional Plan Kimberley 2011</p>	<p><b>Goal:</b> Improved delivery of health services in the region  <b>Strategies:</b>                      Involvement in Regional health committees including District Health Advisory Committee                      Better coordination of all health service providers                      Health precinct with buildings for community health and social care services to deliver services from specific structures and spaces to communities, shared by health and social care organisations  <b>Goal:</b> Ensure a physically and mentally healthy population in the Kimberley  <b>Strategies:</b>                      Improve community participation (i.e. volunteers, players, etc.)                      Improve and / or provide facilities  <b>Goal:</b> Reduce the need for remote community residents to travel to Broome for on-going health needs                      Advocate for the establishment of dialysis unit in the Central Kimberley                      Facilities like in Broome at BRAHMS need to also be out at Fitzroy Crossing and Halls Creek                      Greater use of remote diagnostics and telehealth – Improved telecommunications – Next G mobile and broadband coverage – NBN roll out – Improved and frequent telehealth facilities                      Support family support services (i.e.: Kununurra Neighbourhood House)                      Encourage childcare availability  <b>Goal:</b> Increase the number of private and public medical practitioners                      Influence government to introduce housing and benefit initiatives that encourages location to the region                      Mobile health clinics in 4x4 trucks                      Train regional nurses up to diagnose and prescribe without the need for doctors                      Strategies needed to keep people in the Kimberley and ensure the continuity of programs                      Support medical training in the region to add junior medical resource</p>

Related Plans	Strategy Alignment
	<p>More health professionals (mental health) outside the Government structure</p> <p>Youth (10 to 18) mental health through diversion programs involvement</p> <p>Address other needs i.e. childcare and education standards</p> <p>Encourage local aspiration and training to take on health profession qualifications (school and university, etc.)</p>
National Partnership Agreement on Closing the Gap in Indigenous Health Outcomes 2009	<p><b>Initiative:</b> Primary health care service that can deliver</p> <p>Introduce minimum service standards for all organisations providing primary health care services to Aboriginal and Torres Strait Islander populations.</p> <p>Ensure that primary health care services have the capacity to deliver the coordination and continuity of care necessary to meet the needs of Aboriginal and Torres Strait Islander clients.</p> <p>Expand allied health and acute care services to address the increased referrals for coordinated care by primary health care services.</p> <p>Review and refocus own purpose outlays in primary health care to prioritise core service provision and evidence-based regional priorities.</p>
Western Australian Strategic Plan for Safety and Quality in Health Care 2008-2013	<p><b>Objective C:</b> Expanded patient-centred health service planning and delivery.</p> <p><b>Strategies:</b></p> <p>Ensure consumers, carers and the community are well informed.</p> <p>Improve communication between patients and health care providers.</p> <p>Empower patients/carers/community to be part of health care planning and delivery.</p> <p>Ensure organisational structures and health care providers effectively integrate patients / carers / community in planning and delivering health care.</p> <p><b>Objective D:</b> Equitable access to safe, high quality health care.</p> <p><b>Strategies:</b></p> <p>Identify and apply elements and levers to improve access to health care for all sectors of the community.</p>
WA Health Operational Plan 2010-2011	<p><b>Key Goal 1:</b> Caring for People and the Community</p> <p><b>Priority 1.1:</b> Implement initiatives outlined in WA Strategic Plan for Safety and Quality in Health Care 2008-13</p>
WA Health Strategic Intent 2010-2015	<p><b>Making the best use of funds and resources</b></p> <p>Developing and rolling out a system of Activity Based Funding and Management for our health services and hospitals - providing a clearer link between the dollars we spend and the services we provide to patients and the community.</p> <p>Building and redeveloping hospitals and facilities to house first-class health services for Western Australians closer to where they live.</p>

**Strategy 4.2.2: Advocate for improved regional education and training opportunities**

Related Plans	Strategy Alignment
Regional Development Policy Framework 2011	<p><b>Priority 4: Education</b></p> <p><b>Outcomes:</b></p> <p>The closure of the gap in education achievement and participation between regional and metropolitan students.</p> <p>Improved attendance and retention rates for primary and secondary schooling in regional and remote areas.</p> <p>Standards of achievement in literacy and numeracy for regional students that match national and state averages.</p> <p>Teachers to be attracted to take up and remain in teaching posts in regional areas.</p> <p>A broad range of educational options to be available and accessible to students in regional areas.</p>
Regional Plan Kimberley 2011	<p><b>Goal:</b> Offer relevant and accessible training that is linked to employment</p> <p><b>Strategies:</b></p> <p>Fund Work Readiness programs</p> <p>Introduce case management processes</p> <p>Develop school based trade training, supported by an Industry Trade Training Centre</p> <p>Encourage adult night training for lifelong learning (could be non-vocational)</p> <p>Improve business training services and introduce professional development opportunities</p> <p><b>Goal:</b> Develop mechanisms that better communicate educational offerings, to break down barriers to access</p> <p><b>Strategies:</b></p> <p>Establish linkages with Universities, TAFE and other Kimberley based educational institutions</p> <p>Develop an interactive RDA website to link with these institutions</p> <p>Engage Aboriginal Workforce Development Centre to assist</p> <p><b>Goal:</b> Introduce a more equitable training rebate cost system</p> <p><b>Strategies:</b></p> <p>Make decision and policy makers aware that training rebates are based on Perth costs and need to be reconsidered in light of regional costs</p> <p>Equalise opportunities between country and city (flights to Perth equivalence)</p> <p><b>Goal:</b> Lift educational standards and curriculum diversity in the region</p> <p><b>Strategies:</b></p> <p>Resume discussion through forums such as the Kimberley Economic Forum and ICC's Education Steering Committee</p> <p>Improve the ability of local schools to provide and meet local needs</p> <p>Develop a private school option, that is located on the campus of an educational institution and offers an Outdoor Education focus</p> <p>Develop an Agricultural College for the East Kimberley</p> <p>Encourage education providers to set up shop in Kimberley communities, e.g.: up the Dampier Peninsula to "help them grow their own"</p> <p><b>Goal:</b> To develop mechanisms that address the issues of remoteness through shared</p>

	<p>knowledge and partnerships</p> <p>Develop partnership with Desert Knowledge</p> <p>Develop partnerships and alliances with Indigenous organisations to increase employment</p> <p>Provide access to quality teachers and trainers</p> <p>Provide access for families to early childhood support and education in order for them to access employment and training</p> <p>Enhance training opportunities (flexible ones)</p>
National Education Agreement 2009	<p><b>Objectives:</b></p> <p>(a) all children are engaged in and benefiting from schooling;</p> <p>(b) young people are meeting basic literacy and numeracy standards, and overall levels of literacy and numeracy achievement are improving;</p> <p>(c) Australian students excel by international standards;</p> <p>(d) schooling promotes the social inclusion and reduces the educational disadvantage of children, especially Indigenous children; and</p> <p>(e) young people make a successful transition from school to work and further study.</p>

**Outcome 4.3: Best practice in community services**

***Strategy 4.3.1: Collaborate with State Government and other agencies to ensure co-ordinated provision of services***

Related Plans	Strategy Alignment
Shire of Broome Strategic Plan 2011-2016	<p><b>Strategy 1:</b> Work with agencies and others to ensure affordable and equitable services and infrastructure.</p> <p><b>Outcome 2:</b> Actively participate in the Kimberley Zone of Local Government and the RLG</p> <p><b>Action:</b> Work with State Government agencies to enable co-ordinated provision of services</p>

**Outcome 4.4: Advocate for an improved range and standard of municipal services to Aboriginal communities**

***Strategy 4.4.1: Liaise with stakeholder organisations to determine the future of municipal service delivery to Aboriginal communities***

Related Plans	Strategy Alignment
<p>A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011</p>	<p><b>Goals:</b></p> <p>Role and Expectations of Rural-Remote and Indigenous Local Government                      Local Government Service Delivery to Remote Indigenous Communities</p> <p><b>Outcomes:</b></p> <p>A shared understanding of the role local government can realistically play in delivering local government and essential services to remote Indigenous communities.                      An improved understanding and appreciation of the full cost of providing services to remote Indigenous communities.                      A coordinated inter-government approach to service delivery in remote Indigenous communities.</p> <p><b>Strategies:</b></p> <p>Research the capacity of local government to deliver core municipal and other non-core services to remote Indigenous communities. This would involve:                      An overview of current challenges to service delivery to remote Indigenous communities and likely future funding scenarios                      An assessment of who is available to deliver noncore local government services and at what cost                      Analysis of alternative and innovative service delivery models (e.g. unincorporated areas of NSW, SA Lands Trust, State Government institutions, large Indigenous NGOs) – how do these work, what are their strengths/benefits and weaknesses compared to a system of local government                      Reviewing the progress of the NT reform of local government and continuing challenges                      International comparisons – particularly Canada                      Examining the feasibility of an inter-governmental approach to remote community service delivery (including joint planning, shared service delivery contributions etc.)                      An examination of opportunities to extend resource sector partnerships with Indigenous communities to provide support for the establishment of Indigenous enterprises that can deliver local government services to remote Indigenous communities on an individual or regional basis.</p>

**Goal 5: A sustainable and diverse economy**

**Outcome 5.1: Generational advantage that captures the wealth for the region**

***Strategy 5.1.1: Support regional partnerships with both private and public sector for future investment in the region***

Related Plans	Strategy Alignment
KDC Strategic Plan 2009	<p><b>Service 1:</b> The Commission will provide effective regional development policies and strategies together with accurate and accessible information on the region to facilitate and support the economic and social development of the region.</p> <p><b>Strategy:</b> Strategic Project Development, Support and Coordination - The Commission will facilitate and coordinate support for strategic regional economic development projects using a whole of government approach to ensure optimum allocation and leverage of resources.</p> <p><b>Service 2:</b> The Commission will identify, co-ordinate and promote through its major strategies the establishment, improvement and diversification of the region’s infrastructure, industries and enterprises.</p> <p><b>Strategy:</b> Development Processes, Partnerships and Government Policies - The Commission will establish effective partnerships with key stakeholders to facilitate the achievement of strategic objectives and will monitor and contribute to formal development processes to advance and assist development outcomes.</p>

**Outcome 5.2: Improved outcomes in employment**

***Strategy 5.2.1: Promote and advocate at the state and national level our region’s readiness for investment opportunities***

Related Plans	Strategy Alignment
KDC Strategic Plan 2009	<p><b>Service 1:</b> Strategic Project Development, Support and Coordination - The Commission will facilitate and coordinate support for strategic regional economic development projects using a whole of government approach to ensure optimum allocation and leverage of resources.</p>

**Strategy 5.2.2: Support and advocate regional employment programs and opportunities**

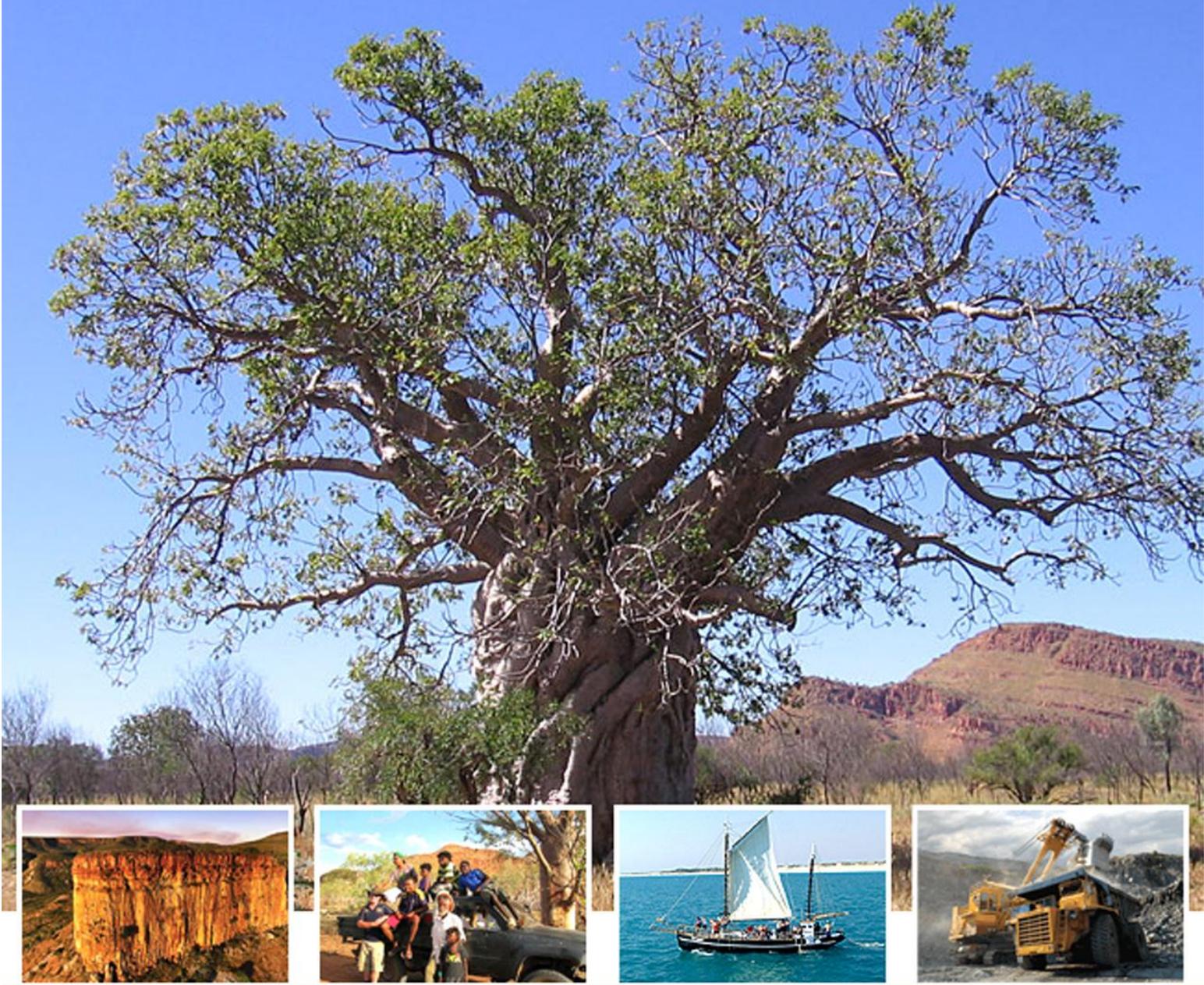
Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<p><b>Goal:</b> Offer relevant and accessible training that is linked to employment</p> <p><b>Strategies:</b>                      Fund Work Readiness programs                      Develop school based trade training, supported by an Industry Trade Training Centre                      Encourage adult night training for lifelong learning (could be non-vocational)                      Improve business training services and introduce professional development opportunities</p> <p><b>Goal:</b> Increase level of participation in the economy</p> <p><b>Strategies:</b>                      More support for workplaces to host, support and train lowly skilled people: – employer readiness programs – alternative education programs</p> <p><b>Goal:</b> Demonstrate the economic impacts of the inadequate supply of housing and slow land release strategies</p> <p><b>Strategies:</b>                      Workers camp accommodation – Make a proper town                      Lobby/advocate for affordable housing and viability of ownership by low income earners to alleviate pressure on State housing</p> <p><b>Goal:</b> Increase the pool of available housing for health care workers</p> <p><b>Strategies:</b>                      Promote discussion for the urgent need to increase housing for health care workers                      Target more remote towns (Fitzroy Crossing and Halls Creek)</p>
Kimberley Tafe Strategic Plan 2010-2012	<p><b>Major Strategy:</b> A skilled workforce</p> <p><b>Strategic Objective:</b> Support industry in the Kimberley region to improve and develop employment opportunities</p>

**Outcome 5.3: A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the region’s assets (human, environment and built)**

**Strategy 5.3.1: Promote tourism opportunities at a regional level**

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<p><b>Goal:</b> Increase level of participation in the economy</p> <p><b>Strategies:</b>                      Ensure links between RDA and tourism                      Support Indigenous tourism at local and regional level (especially in the East Kimberley)                      Support tourism businesses to train and improve infrastructure</p> <p><b>Goal:</b> Provide more cultural focus in tourism in the region</p> <p><b>Strategies:</b>                      Add value to current programmes through membership of tourism associations and contribution to industry forums                      Promote current programmes through the RDA website                      Build capacity of Traditional Owners to become major tourism owners and operators, e.g.: resorts, etc.                      Fund key infrastructure                      Marketing – target international, national and regional                      Enhance facilities i.e. roadside stops</p>
Tourism Western Australia Strategic Plan 2010-2011	<p><b>Strategic Objectives:</b>                      Deliver innovative marketing to make Western Australia famous for its extraordinary experiences and convert consumer preference to visitation.                      Develop, attract and market events that fortify Western Australia’s brand, deliver visitors to the State, activate infrastructure, add social vibrancy and encourage community participation.                      Facilitate tourism infrastructure development to provide access to, and accommodation around, Western Australia’s extraordinary tourism experiences.</p>
Australia's North West Tourism Development Priorities 2010-2015	<p><b>Priorities:</b>                      Establish the Kimberley as a key aviation gateway                      Facilitate the development of accommodation in national and conservation parks and on pastoral stations                      Increase caravan park capacity across the Kimberley region                      Improve the reliability and safety of access to key natural attractions accessible via the Great Northern Highway                      Facilitate the development of indigenous product and interpretation in the region                      Improve and expand visitor amenities along major touring routes                      Improve boating facilities in the Kimberley region                      Facilitate the development of convention/conference facilities in the region                      Ensure tourism related development is incorporated in the rejuvenation of precincts and</p>

Related Plans	Strategy Alignment
	townships Seal key coastal access roads around Broome



# Kimberley

## Strategic Community Plan 2012-2021

### Kimberley Regional Group



## Acknowledgement

The Shires of Broome, Wyndham East Kimberley, Halls Creek and Derby West Kimberley wish to acknowledge funding provided by the Department of Local Government and Department of Regional Development and Lands through Royalties for Regions to support this project.



Department of **Local Government**  
 Department of **Regional Development and Lands**



The Strategic Community Plan has been cooperatively developed and endorsed by the member Local Governments of the Kimberley Regional Group.



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## Disclaimer

Information provided in this Plan was gathered in good faith and is believed to be reliable and accurate at the time of publication.

The Shires of Broome, Wyndham East Kimberley, Halls Creek and Derby West Kimberley advise users of this information to take responsibility for their own assessment and to verify all representations, statements and information that concern the conduct of business that involves monetary or operational consequences.

The development of this Plan for the Kimberley Regional Group has been facilitated by:



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## 1. Foreword

### Message from the Chairperson



The Kimberley Regional Group's Strategic Community Plan is a document that identifies aspirations and sets priorities for our communities at a Kimberley-wide level.

Each participating Council (Broome, Derby West Kimberley, Halls Creek and Wyndham East Kimberley) has prepared, or is in the process of preparing, a Strategic Community Plan which aims to define their aspirations and priorities at the Council level.

This Plan strives to identify commonalities shared by the four Kimberley Councils, and I hope it will serve to unify us by shining light on the things which bind us as "The Kimberley".

This Strategic Plan should be seen as a constantly evolving roadmap for the Kimberley. It is the result of a lot of work by Elected Members and Executive Staff from all the Kimberley Councils and I want to thank everyone involved in its creation for their contributions.

We believe we have captured the essence of the region's aspirations, and have reflected these in our desired outcomes.

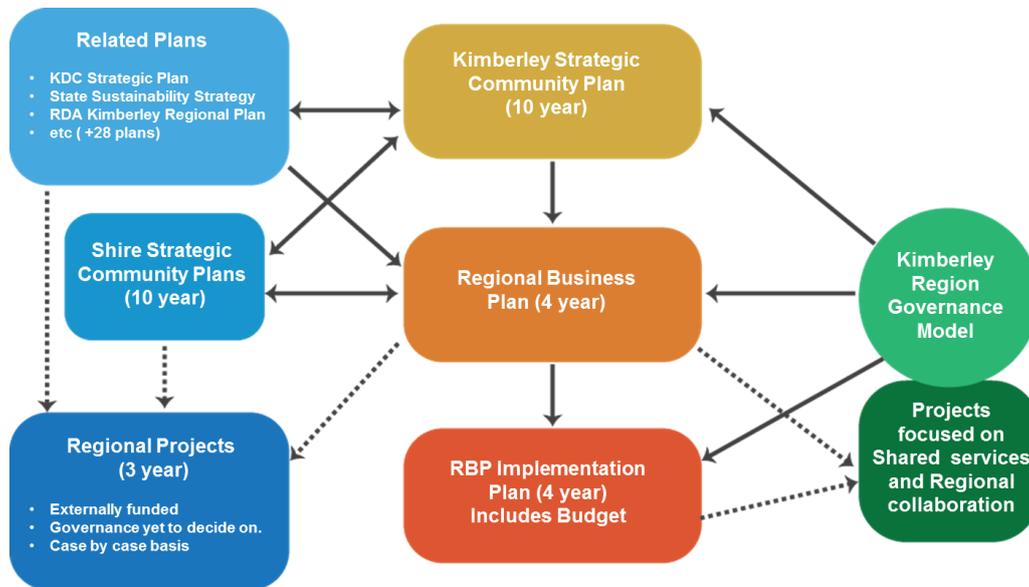
**John Moulden**

**Chairperson**

## 2. About this Plan

The Kimberley Strategic Community Plan is the primary strategic document in the Regional Planning Framework, outlined in the figure below:

**Figure 1: Regional Planning Framework**



The Kimberley Strategic Community Plan will be the primary driver for regional collaboration initiatives by the member Kimberley Shires, setting out the Vision, Goals, Outcomes and Strategies to guide and inform all related regional plans.

The Kimberley Strategic Community Plan was developed by the Kimberley Regional Group with direct reference to key stakeholder planning documents, including the four Member Shire's Strategic Community Plans and thirty one related regional, state and national plans and strategies with direct relevance to the role of Local Government in the Kimberley Region.

The Regional Business Plan provides the major actions, responsibilities and timeframes over a four year period to deliver on the strategies and outcomes in the Kimberley Strategic Community Plan.

The Regional Business Plan Implementation Plan provides detailed planning and budget information for priority projects focused on shared services and regional collaboration initiatives.

The Regional Planning Framework also recognises the identification and development of regional projects that has been driven through the individual Shire strategic planning processes and/or the plans and strategies of major stakeholders.

The diagram below outlines the structure of the Kimberley Strategic Community Plan and shows the critical linkage level with the Regional Business Plan:

**Figure 2: Kimberley Strategic Community Plan Structure**



Through a clear understanding of the current situation across the five strategic areas for Local Government:

1. Governance
2. Natural Environment
3. Built Environment
4. Community
5. Economy

A clear picture or Vision for the future, and a clear understanding of the Mission of the Kimberley Regional Group, it is possible to identify the goals and outcomes that must be achieved.

This provides the framework for the identification of strategies that reflect the role and responsibilities of the Kimberley Region governance model.

The strategies are the critical level of connection from the Kimberley Strategic Community Plan to the Regional Business Plan, and drive the selection of priority actions and initiatives.



### 3. Vision

Maintain and enhance the rich diversity and liveability of the Kimberley for its people and the world

### 4. Mission

Working collaboratively for the benefit of people and the land of the Kimberley region

### 5. Values

- Equity for the interests of each Shire
- Respecting the diversity of people, landscape and culture
- Integrity, openness and trust
- Commitment to Kimberley outcomes
- Working proactively to achieve our collective vision
- Valuing success
- Innovative

## 6. Regional Profile

### Geography

The Kimberley region covers an area of 419,558km<sup>2</sup> which is one sixth of Western Australia's total land area. It is one of the nine regions of Western Australia and is located in the northern part of the State. The Kimberley is bordered by the Indian Ocean in the west, the Timor Sea to the north, the Great Sandy and Tanami Deserts to the south, and the Northern Territory to the east.



The distance from Perth city to the nearest town, Broome, is 2,415km. The distance from Broome to Wyndham is 1055km via the Great Northern Highway.

The Kimberley is located north of the Tropic of Capricorn, so the climate is tropical monsoon, defined by a distinct wet season (November to April) and a dry season (May to October).

Significant geographic features of the Kimberley include:

- Most of the northern portion is characterised by savannah style vegetation with mature trees and grasslands. Rivers to the north are commonly lined with paperbarks and pandanus
- The coast is typically steep cliffs in the north but flatter in the south with both subject to high tides
- There are no mountains that reach above 1,000 metres
- The southern end of the Kimberley beyond the Dampier Peninsula is flatter with dry tropical grassland and is used for cattle ranching
- In parts of the Kimberley such as the valleys of the Ord and Fitzroy Rivers, the soils are useable cracking clays, whilst elsewhere they are lateritic Orthents
- The largest protected areas are the Prince Regent Nature Reserve and the Drysdale River National Park

The following figure shows a general reference map of the Kimberley region:

**Figure 3: Kimberley Region Map**



Source: Department of Regional Development and Lands, 2009

## Population

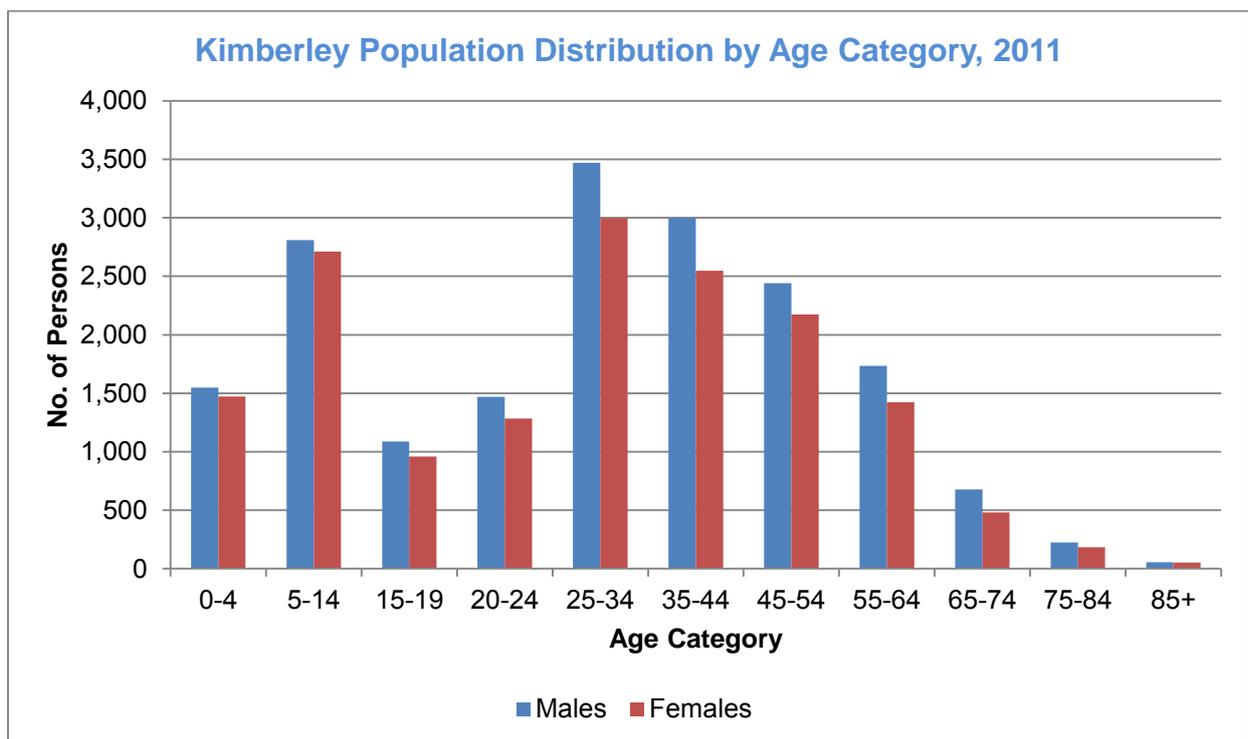
The relatively small total population of 34,794 persons and correspondingly large geographic area of 419,558km<sup>2</sup> in the Kimberley region, equates to a population density of 0.08 persons/km<sup>2</sup> (ABS 2011). This compares with the State population density of 0.89 persons/km<sup>2</sup> and that of Australia at 2.78 persons/km<sup>2</sup> in 2011.



The median age of the Kimberley population in 2011 was 30 years, compared to the State median age of 36 years. The population in 2011 comprised 53.2% males and 46.8% females.

In the same year, there were 8,541(24.6%) people who were below 15 years old and 2,952(8.5%) who were 60 years and above.

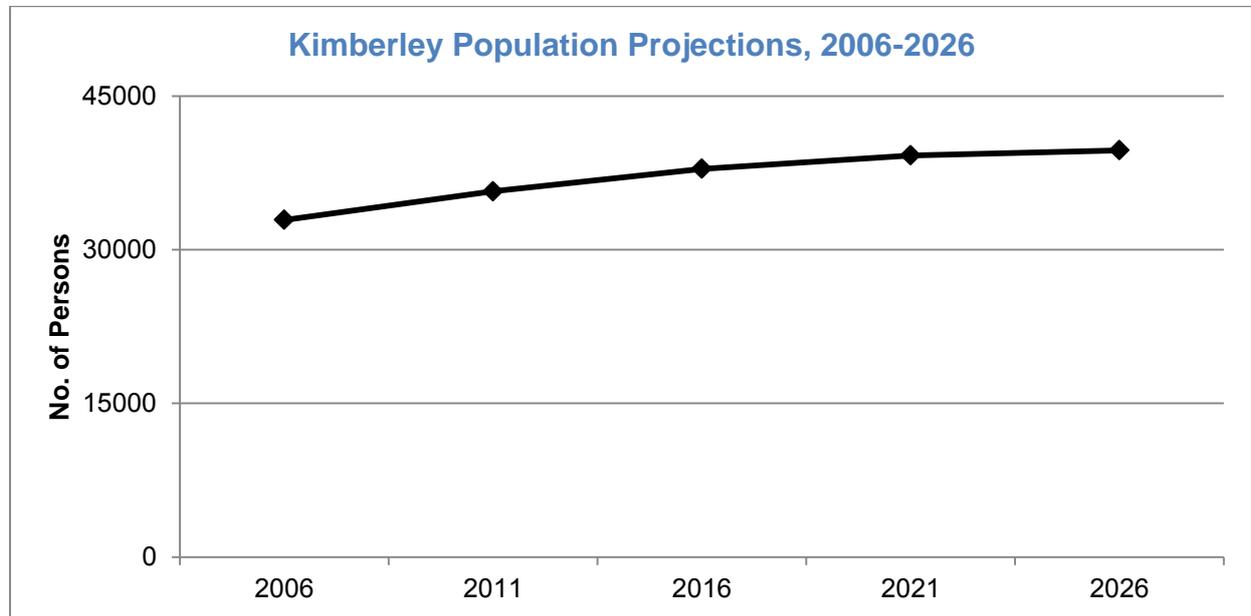
**Figure 4: Kimberley Age Breakdown, 2011**



Source: ABS – Census of Population and Housing, 2011

The Kimberley is expected to have a steady growth over the next 15 years. The Department of Planning & Infrastructure projections of the Kimberley population are displayed in the line graph below:

**Figure 5: Kimberley Population Projections, 2006-2026**



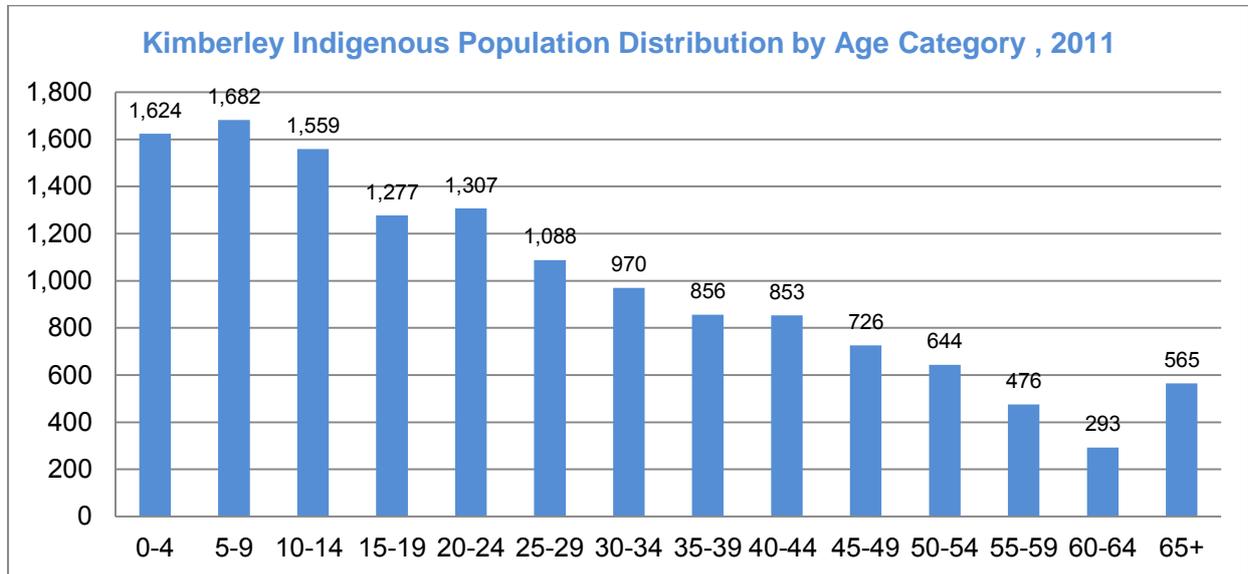
Source: Western Australia Planning Commission, Forecast Profile for the Kimberley Planning Region, 2012

There is a strong Indigenous representation in the Kimberley. The 2011 Census indicated that 40.0% of the total population in the Kimberley were Indigenous people. This compares with 3.1% of the population State-wide. According to the 2011 ABS census, Indigenous residents in the Kimberley had a median age of 22 years and were made up of 6,738 males and 7,182 females. The 2011 Census also indicated that 7,449(53.5%) of the Indigenous population of the Kimberley were less than 25 years of age and 858(6.2%) were 60 years and above.



The Kimberley Indigenous population distribution by age category for is displayed in the graph below:

**Figure 6: Kimberley Indigenous Population by Age Category, 2011**



Source: ABS – Census of Population and Housing, 2011

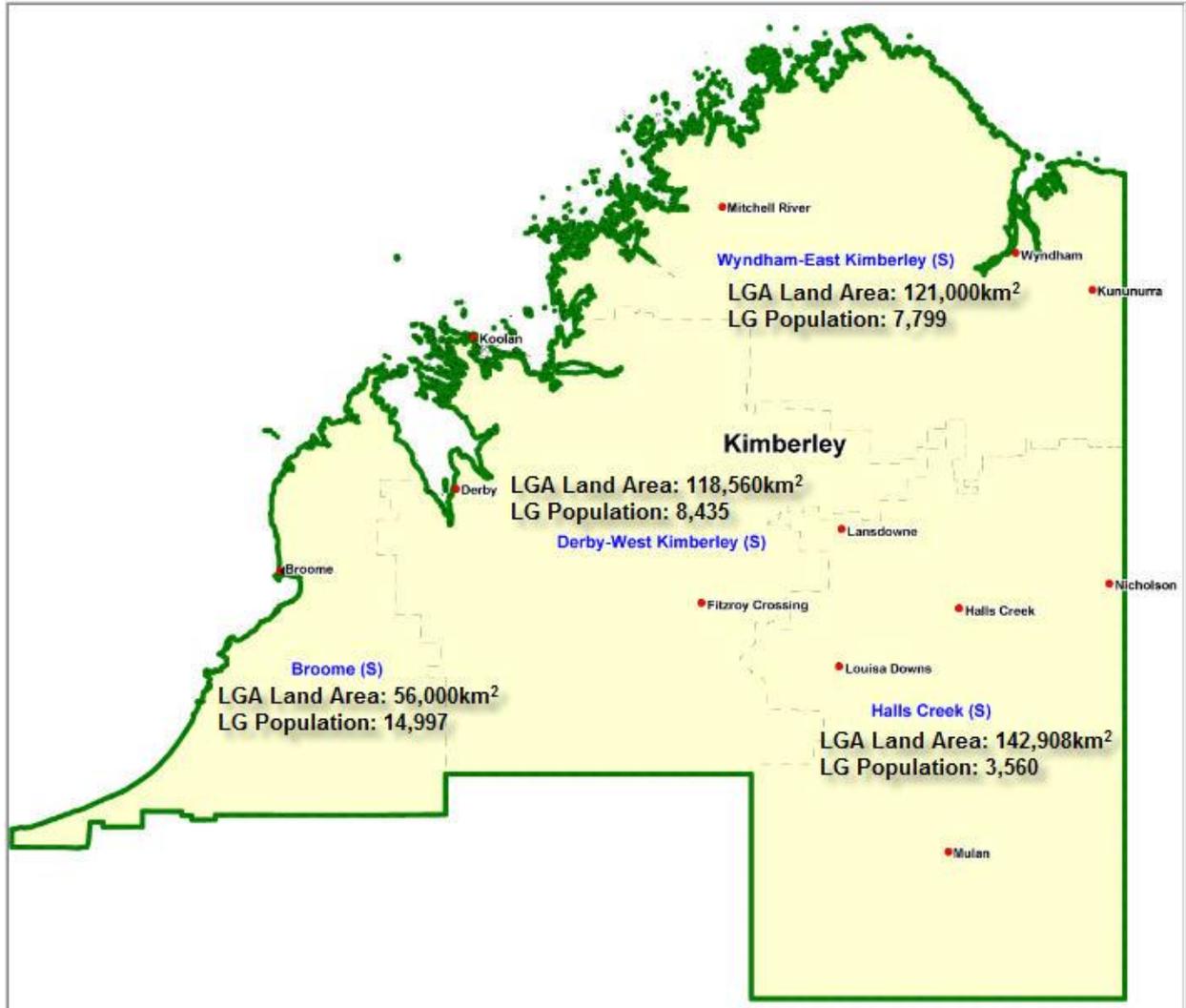
It is forecast that the characteristics of the Kimberley population will alter significantly over subsequent generations. From the macro-economic perspective there will be substantial challenges for Local, State and Commonwealth governments in achieving balanced development from economic, cultural and community perspectives.

The following summarises the population growth for each Shire in recent years: (ABS 2011)

- Shire of Broome – 14,997 people in 2011 and of these 51.1% were male and 48.9% were female. Aboriginal and Torres Strait Islander people made up 29.1% of the population. Average annual growth rate for the 5 years to 2011 of 1.16%.
- Shire of Derby West Kimberley - 8,435 people in 2011 and of these 58.9% were male and 41.1% were female. Aboriginal and Torres Strait Islander people made up 48.1% of the population. Average annual growth rate for the 5 years to 2011 of 2.94%.
- Shire of Halls Creek - 3,560 people in 2011 and of these 51.2% were male and 48.8% were female. Aboriginal and Torres Strait Islander people made up 78.2% of the population. Average annual growth rate for the 5 years to 2011 of 1.98%.
- Shire of Wyndham East Kimberley – 7,799 people in 2011 and of these 51.9% were male and 48.1% were female. Aboriginal and Torres Strait Islander people made up 34.8% of the population. Average annual growth rate for the 5 years to 2011 of 1.79%.

The land area and population for each of the four Local Government Areas is represented in the figure below.

**Figure 7: Kimberley Local Government Areas**



Source: ABS – National Regional Profile, Kimberley, 2011

## Labour Force

The labour force varies significantly between the Local Government Areas. The Shire of Broome, with a larger total population has a proportionately higher number of people in the labour force. The table below presents the labour force for each Local Government Area from 2006 to 2011.

**Table 1: Kimberley Labour Force, 2006-2011**

Local Government Area/Region	Labour Force, June Quarter					
	2006	2007	2008	2009	2010	2011
<b>KIMBERLEY</b>	17,539	17,667	16,471	15,819	17,375	18,159
<b>Broome (S)</b>	7,983	8,042	7,617	7,357	8,080	8,453
<b>Derby-West Kimberley (S)</b>	3,918	3,947	3,627	3,467	3,808	3,980
<b>Halls Creek (S)</b>	1,604	1,615	1,387	1,293	1,420	1,484
<b>Wyndham-East Kimberley (S)</b>	4,034	4,063	3,840	3,702	4,067	4,242

Source: Department of Education, Employment and Workplace Relations, 2006-2011

There is also significant variance between Local Government Areas in the unemployment rate. The most recent statistics indicate an unemployment rate of around 5.8% across the Kimberley. The unemployment rate for the 6 years to 2011 by June Quarter for each Shire is shown in the table below. The Shire of Halls Creek has consistently recorded the highest unemployment rate across the Shires.

**Table 2: Kimberley Unemployment, 2006-2011**

Local Government Area/Region	Unemployment, June Quarter					
	2006	2007	2008	2009	2010	2011
<b>KIMBERLEY</b>	5.6%	5.1%	5.4%	6.6%	8.7%	5.8%
<b>Broome (S)</b>	4.1%	4.7%	4.9%	6.0%	7.1%	4.3%
<b>Derby-West Kimberley (S)</b>	4.5%	4.6%	5.0%	6.1%	9.3%	5.8%
<b>Halls Creek (S)</b>	15.9%	9.0%	12.0%	14.6%	18.7%	13.8%
<b>Wyndham-East Kimberley (S)</b>	5.4%	4.8%	4.7%	5.6%	7.7%	6.0%

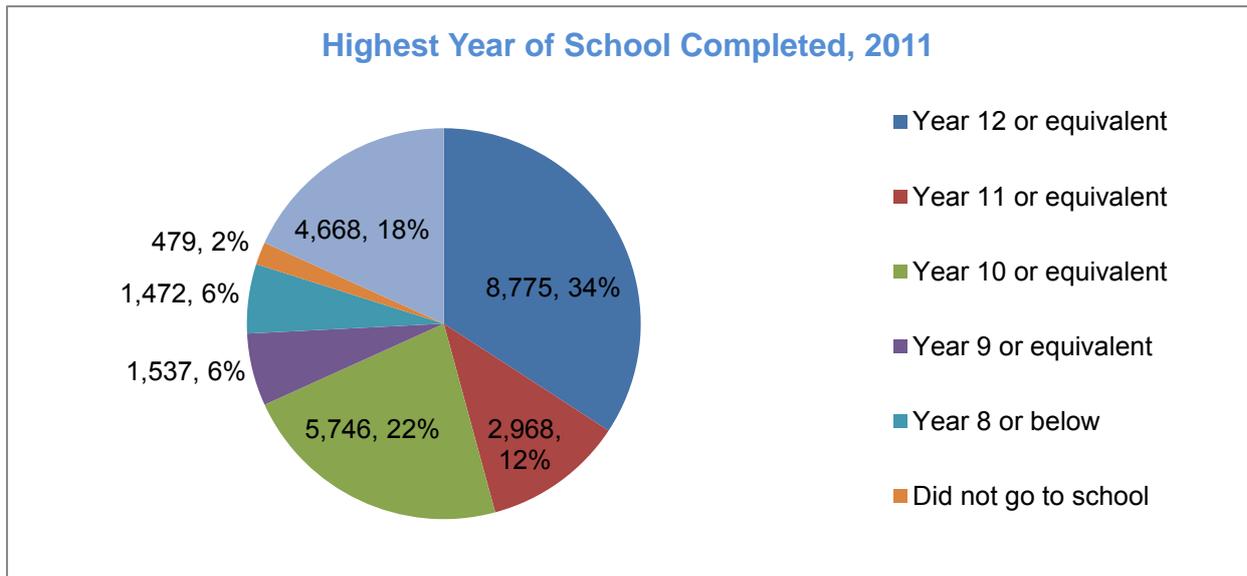
Source: Department of Education, Employment and Workplace Relations, 2006-2011

## Education

In the Kimberley region there were a total of 17,489 school children (68.2%) who had completed Year 10 or above in 2011. This consisted of an equal number of males (8,631) and females (8,858). The highest year of school completed is shown in the figure below:



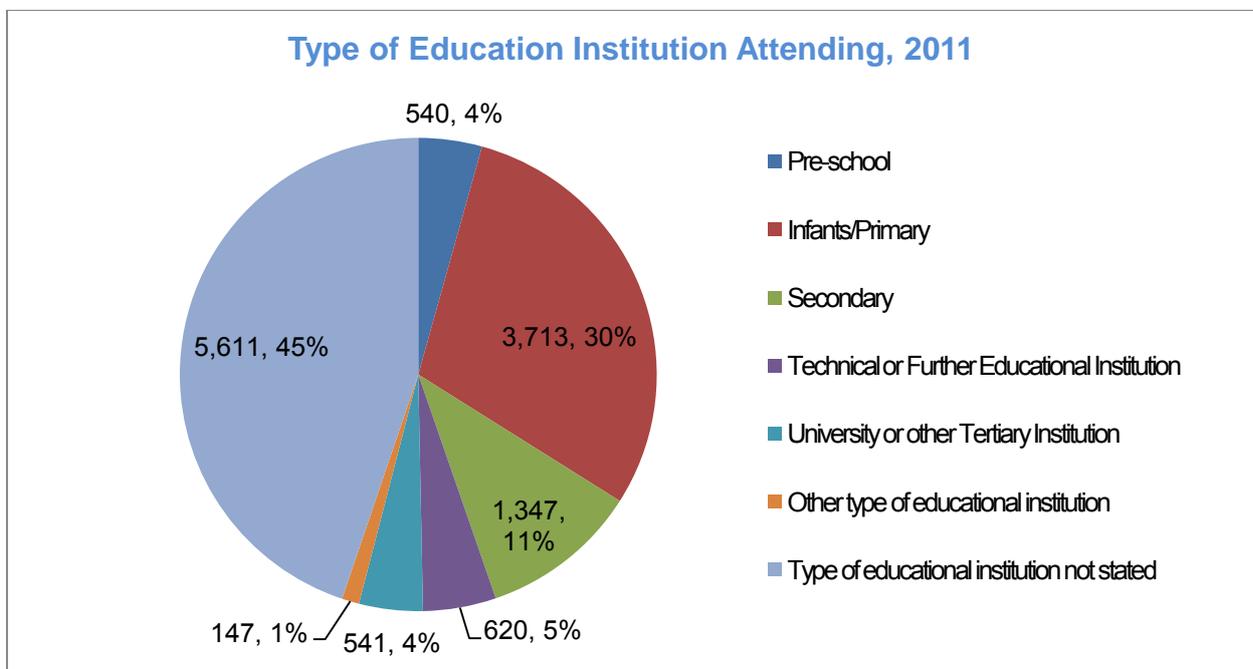
**Figure 8: Kimberley Highest Year of School Completed, 2011**



Source: ABS – Census of Population and Housing, 2011

In terms of the type of education institution in the Kimberley, the major category was infants or primary level, 30% in 2011. The breakdown of the type of education institution in 2011 is shown in the pie chart below:

**Figure 9: Kimberley Type of Education, 2011**

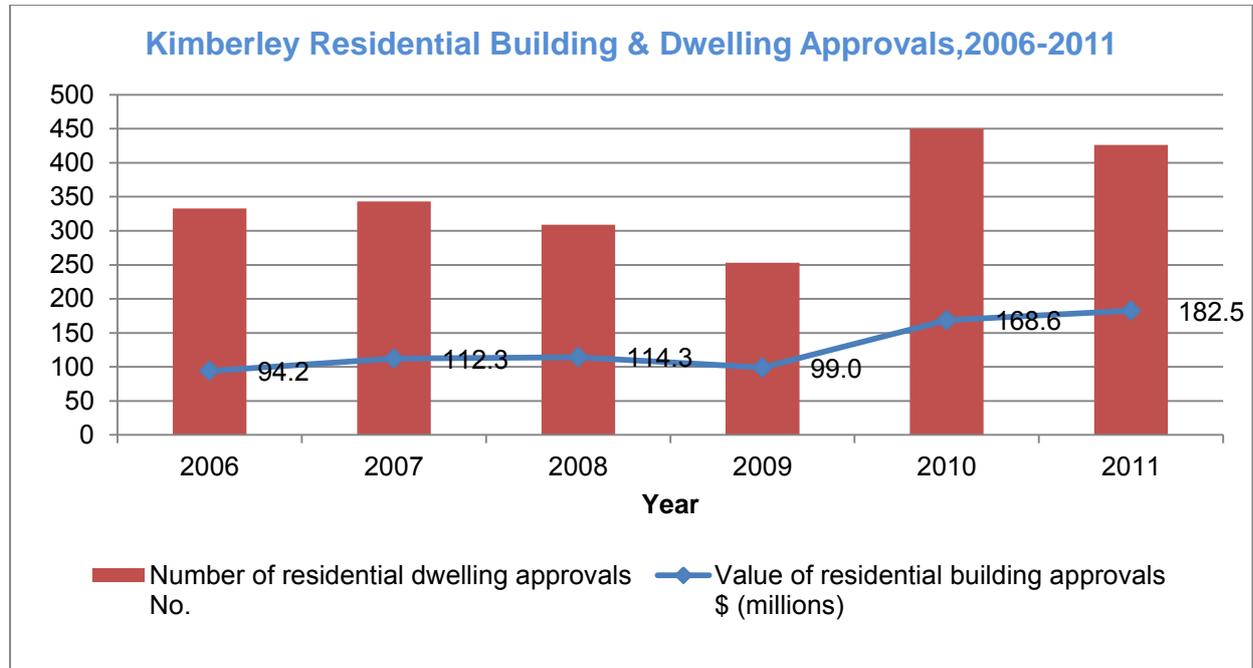


Source: ABS – Census of Population and Housing, 2011

## Housing

The Shires of Wyndham East Kimberley, Halls Creek, Derby West Kimberley and Broome experienced strong growth in building approvals in 2010 and 2011, as shown in the graph below.

**Figure 10: Residential Building & Dwelling Approvals, 2006-2011**



Source: ABS – National Regional Profile, Kimberley, 2006-2011

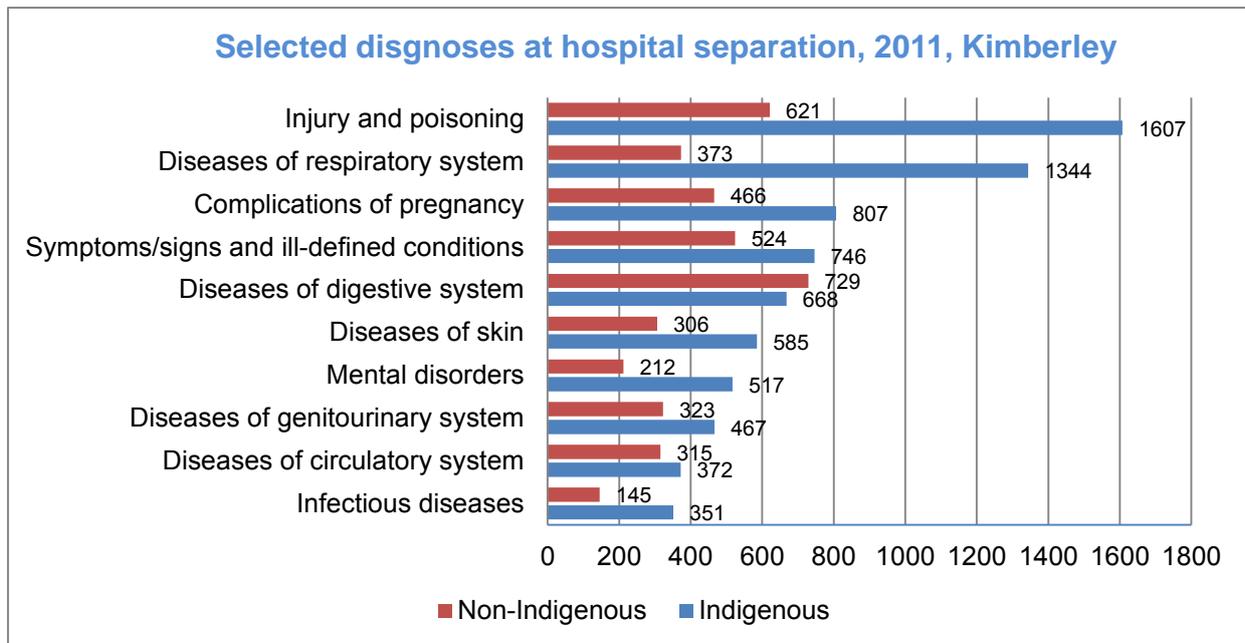
**Health**

There were 23,291 hospital separations for patients residing in the Kimberley region in 2011. About three-quarters of separations (72.5%) were for Indigenous patients. The graph below shows the top ten diagnoses for hospital separations in 2011.



Categories showing significant variation between Indigenous and non-Indigenous separations were Injury and Poisoning, Diseases of the Respiratory System, Complications of Pregnancy, Mental disorders, Diseases of Skin and Infectious Diseases.

**Figure 11: Kimberley Hospital Separations, 2011**



Source: Department of Health – Hospital Separations, Principal Diagnosis by Indigenous Status, 2011

The Kimberley region has the highest incidence of end stage kidney disease (ESKD) and sexually transmitted diseases in Western Australia. Compared to the State smoking is also significantly higher for men and women.

There are higher rates of injury, disease, mortality and morbidity amongst Indigenous people, correlating to their relatively poor socio-economic position.

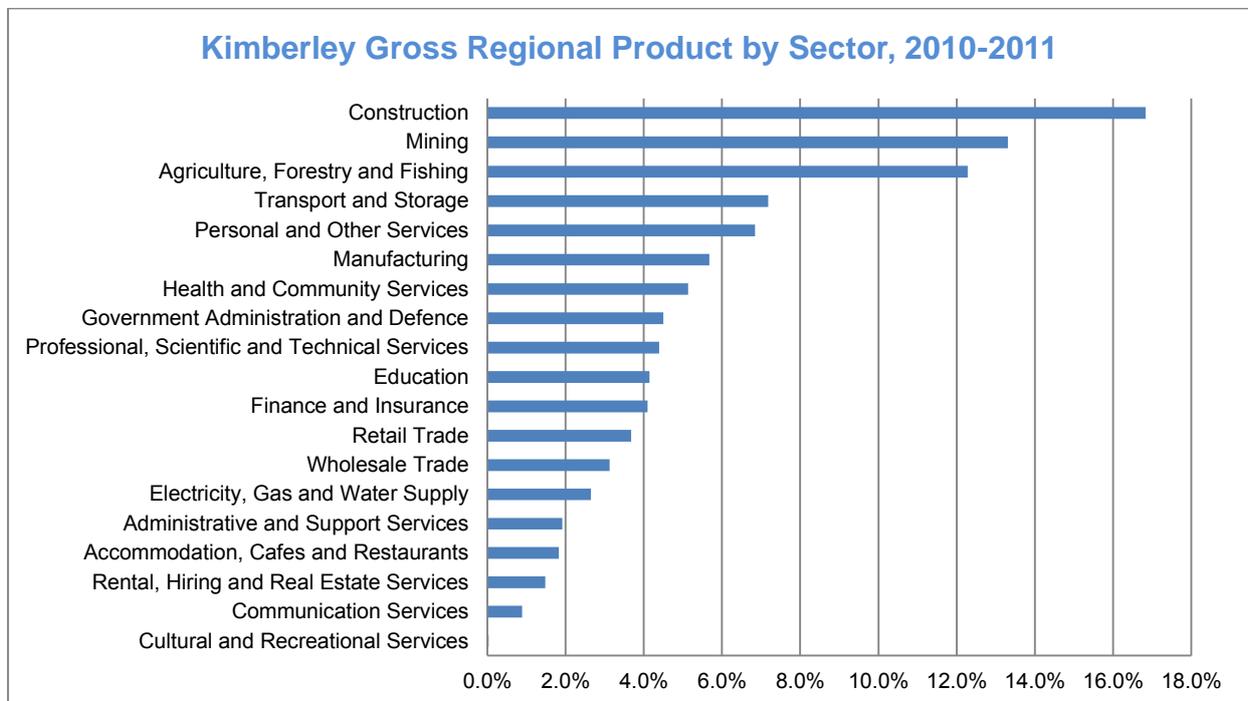
However, there is a significantly lower risk of obesity, lower rates of high blood pressure and lower rates of high cholesterol in the Kimberley region in comparison to the rest of the State.

## Economy

The Kimberley region's Gross Regional Product (GRP) was estimated at \$3.1 billion in 2010-2011. The main industry contributors were Construction, Mining, Agriculture, Forestry and Fishing, Forestry and Fishing. The graph below shows GRP by industry sector in 2010-2011.



**Figure 12: Kimberley Gross Regional Product, 2010-2011**



Source: Department of Regional Development and Lands – Kimberley GRP Estimates, 2010-2011

The top five industries in terms of employment from 2006 Census data were:

1. Health Care and Social Assistance (15.0%)
2. Public Administration and Safety (13.5%)
3. Education and Training (8.9%)
4. Retail Trade (8.6%)
5. Accommodation and Food Services (7.1%)

The construction, mining and agricultural sectors totalled 6.8%, 4.3% and 5.2% respectively.

The table below shows the change in employment by industry from 2001 to 2006 in the Kimberley region.

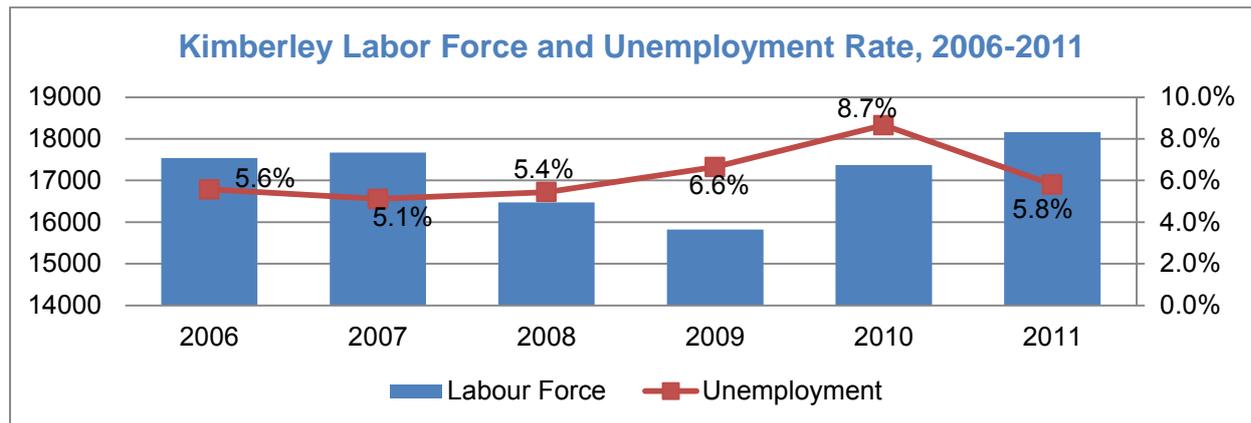
**Table 3: Employment by Industry in the Kimberley**

Industries	% of Total Labour Force	
	2006 Census	2001 Census
Health Care and Social Assistance	15.00%	9.10%
Public Administration and Safety	13.50%	18.20%
Education and Training	8.90%	8.20%
Retail Trade	8.60%	10.40%
Accommodation and Food Services	7.10%	6.70%
Construction	6.80%	5.90%
Other Services	5.30%	7.40%
Agriculture, Forestry and Fishing	5.20%	7.50%
Inadequately described/Not stated	5.20%	3.30%
Transport, Postal and Warehousing	5.10%	5.20%
Mining	4.30%	3.00%
Administrative and Support Services	3.40%	n/a
Manufacturing	3.30%	3.60%
Professional, Scientific and Technical Services	2.10%	n/a
Rental, Hiring and Real Estate Services	1.40%	5.60%
Wholesale Trade	1.20%	2.30%
Electricity, Gas, Water and Waste Services	1.00%	0.80%
Arts and Recreation Services	1.00%	1.50%
Financial and Insurance Services	0.60%	0.70%

Source: ABS – Census of Housing and Population, 2001 and 2006

The Kimberley region's labour force (total number of people employed and unemployed) has exhibited steady growth increasing from 17,539 in June 2006 to 18,159 in June 2011. The number of employed persons in the Kimberley in June 2011 was 17,105 and unemployed persons were 1,054. There was a spike in the unemployment rate from 2008 to 2010 with the onset of the Global Financial Crisis and a decrease in the unemployment rate in 2011 as global economic conditions improved.

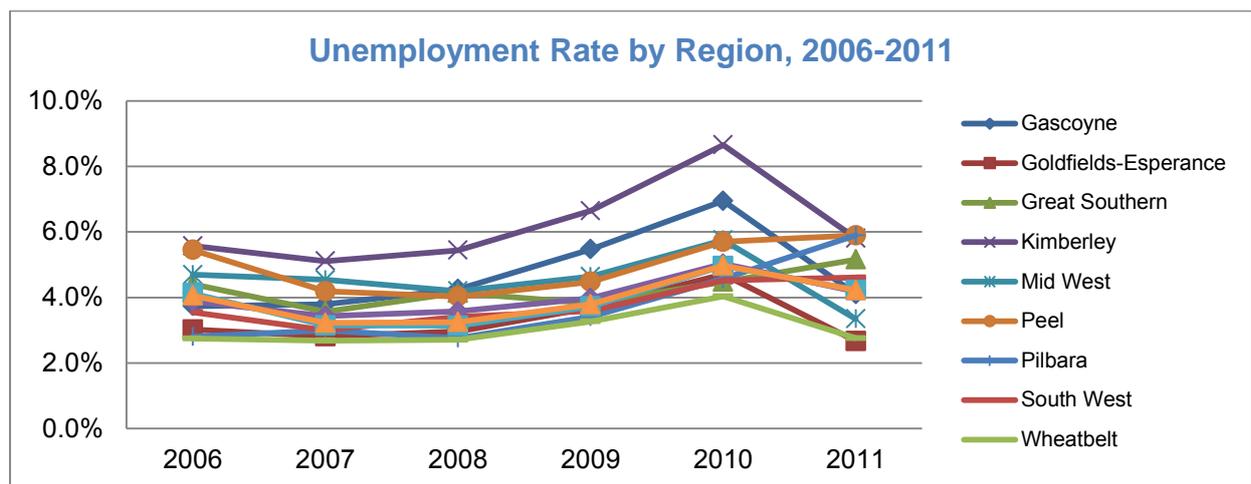
**Figure 13: Kimberley Labour Force and Unemployment Rate, 2006-2011**



Source: Department of Education, Employment and Workplace Relations - Kimberley Labour Force, 2006-2011

The Kimberley region's unemployment rate has been consistently higher than the State's unemployment rate since 2006. The unemployment rate was 5.8% in June 2011, compared to the State rate of 4.2%. The graph below shows a significant drop in the Kimberley region's unemployment rate from 2010 to 2011.

**Figure 14: Unemployment by Region, 2006-2011**

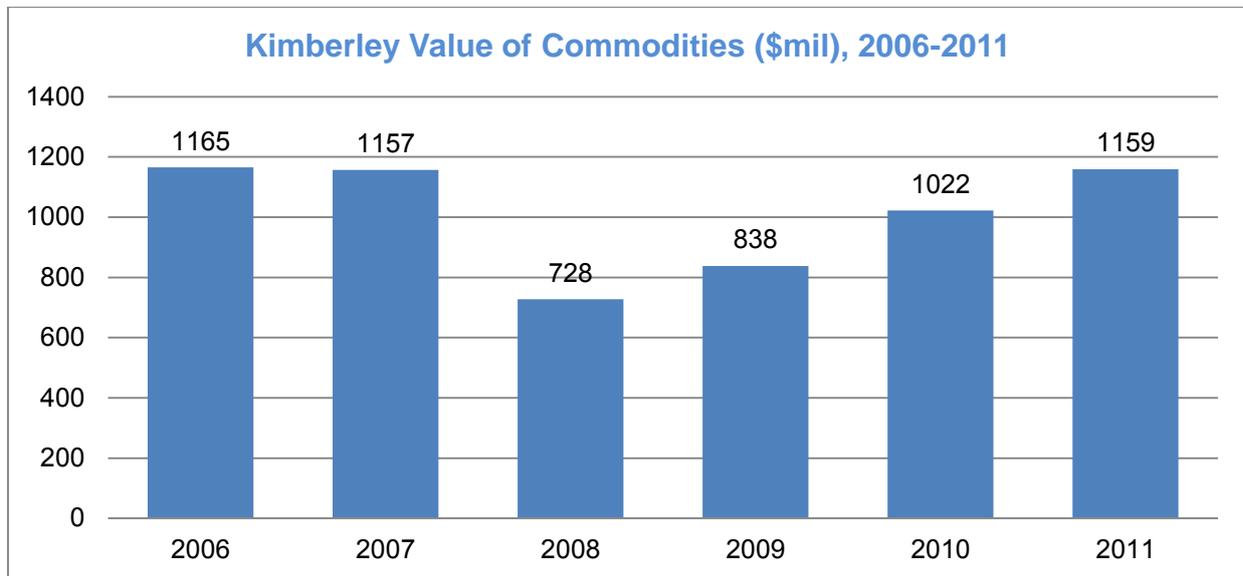


Source: Department of Education, Employment and Workplace Relations - Unemployment by Region, 2006-2011

## Mining

The mining industry is the biggest economic contributor to the Kimberley region, with output valued at \$1,159 million in 2011. The value of commodities, including petroleum, derived from the Kimberley region between 2006 and 2011 is shown in the graph below:

**Figure 15: Kimberley Value of Commodities, 2006-2011**



Source: Department of Mines and Petroleum - Kimberley Value of Commodities, 2006-2011

Diamonds are one of the region's main mineral products with the Argyle Diamond Mine being the largest supplier of pink diamonds in the world. Diamonds are also mined at Ellendale in the Shire of Derby-West Kimberley. Diamonds, gold and silver valued at \$356 million, made up 30.7% of the mining production in 2011. High grade iron ore is mined at Cockatoo and Koolan Islands near King Sound. The iron ore represented 49.5% of mining production with a value of \$574 million. The remaining 19.7% was from other minerals including nickel, copper, cobalt and crude representing a value of \$229 million in 2011.



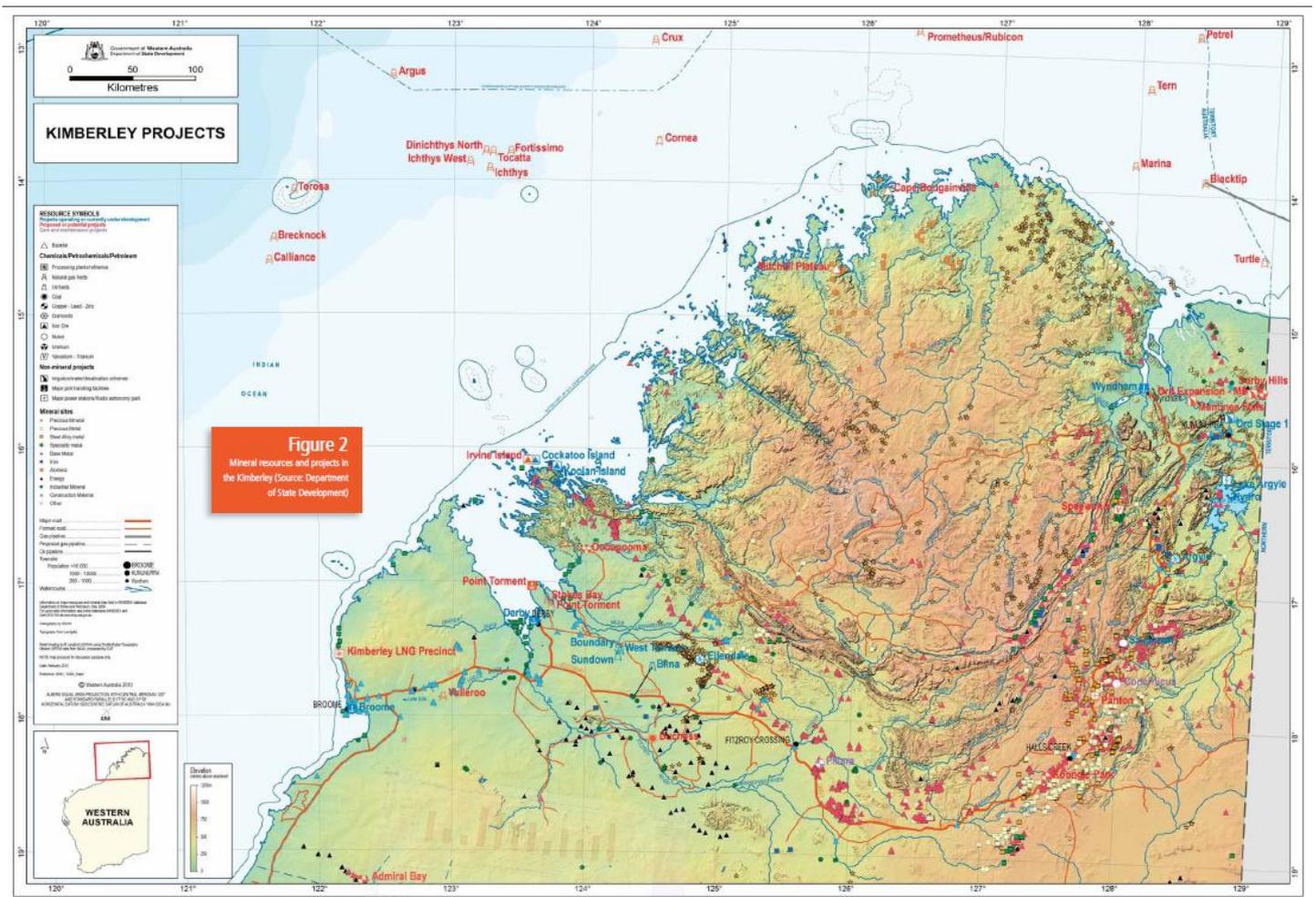
Consistent growth in the Kimberley resources sector over the last 10 years has been a major contributor to the 51 percent increase in Gross Regional Product. That growth is likely to accelerate in the years ahead, primarily driven by the development of the Browse Basin gas resource, in particular the on-shore gas processing and export from the proposed LNG precinct being established in the Kimberley by the state government, with Australian government support. It is estimated that this project will require a construction workforce of 3,500 and an

operational workforce of 520, with substantial spin-offs for small and medium businesses in the region (CME, 2010).

At the Argyle Diamond Mine operation, a decision taken in 2005 to develop underground operations will extend mine life from 2010 to beyond 2018. The Alumina Refinery (Mitchell Plateau) Agreement Act 1971 commits joint venture partners Rio Tinto and Alcoa to the ongoing study of development options, which would be focussed on the Mitchell Plateau and Cape Bougainville tenures. Beyond these major projects, a feasibility study is underway for the Duchess-Paradise thermal coal resource in the Canning Basin (Rey Resources Ltd), with advanced exploration of the largest vanadium deposit in Australia (Niplats Australia Ltd) and a range of base metals prospects underway in the East Kimberley area. There are a wide range of other projects exploring and proving resources in iron ore, platinum, uranium, diamonds, precious metals and industrial metals (CME, 2010).

The following map of the Kimberley region shows the location of mineral, petroleum and gas production and exploration activities.

**Figure 16: Mineral, petroleum and gas projects in the Kimberley region**



Source: Chamber of Minerals and Energy Western Australia, 2010

## Tourism

Domestic visitors to the Kimberley region in 2010 were estimated at 235,000 persons and international tourists at 40,400 persons. Tourism expenditure in 2010 was estimated to be \$265 million.

In 2010 there were an estimated 2,171 people employed directly in servicing the visitor market.

The tourism industry is expected to remain a key contributor to economic activity in the Kimberley. The expectation of industry operators is that the region is still moving through the development stage and is yet to achieve its full potential.



## Agricultural Commodities

The value of agricultural commodities has fluctuated over the years. In 1994-95, the value was \$71.6 million, gradually increasing to \$126.8 million in 2000-01, before declining to \$95.9 million in 2003-04. In 2009-10, the gross value of Kimberley agricultural commodities was \$195 million. The breakdown of this is shown in the table below:

**Table 4: Kimberley Value of Agricultural Commodities, 2010**

Commodity Label	Value (\$)	% Regional Value
Broadacre crops and cereals	2,689,039	1.40%
Other cereals for grain or seed	890,872	0.50%
Chickpeas	537,753	0.30%
Summer pulses	81,918	0.00%
Winter pulses	24,963	0.00%
Summer oilseeds	37,261	0.00%
Cultivated turf	4,139,471	2.10%
Nurseries undercover	380,805	0.20%
Nurseries outdoor	106,631	0.10%
Cereal cut for hay	652,506	0.30%
Other corps cut for hay	28,229	0.00%
Pasture cut for hay	493,946	0.30%
Total value of vegetables	20,331,630	10.40%
Mangoes	3,687,708	1.90%
All other orchard fruit	5,646	0.00%
Bananas	62,152	0.00%
All other berry or tropical fruit	441,015	0.20%
<b>Total value of crops</b>	<b>34,591,545</b>	<b>17.70%</b>
Cattle and calves slaughterings	93,186,675	47.80%
Cattle and calves - other disposals	67,089,598	34.40%
Sheep and lambs slaughterings	36,052	0.00%
Sheep and lambs - other disposals	32,907	0.00%
<b>Total value of livestock slaughterings and other disposals</b>	<b>160,345,881</b>	<b>82.20%</b>
Wool (shorn)	57,886	0.00%
Wool (other)	3,550	0.00%
<b>Total value of livestock products</b>	<b>61,436</b>	<b>0.00%</b>
<b>Total Value of Kimberley Agricultural Products</b>	<b>194,999,540</b>	<b>100.00%</b>

Source: ABS - Agricultural Resource Management Survey / Department of Regional Development, 2010

## Pastoral Industry

The majority of the Kimberley region’s land area is under pastoral lease, with an estimated 100 cattle stations.

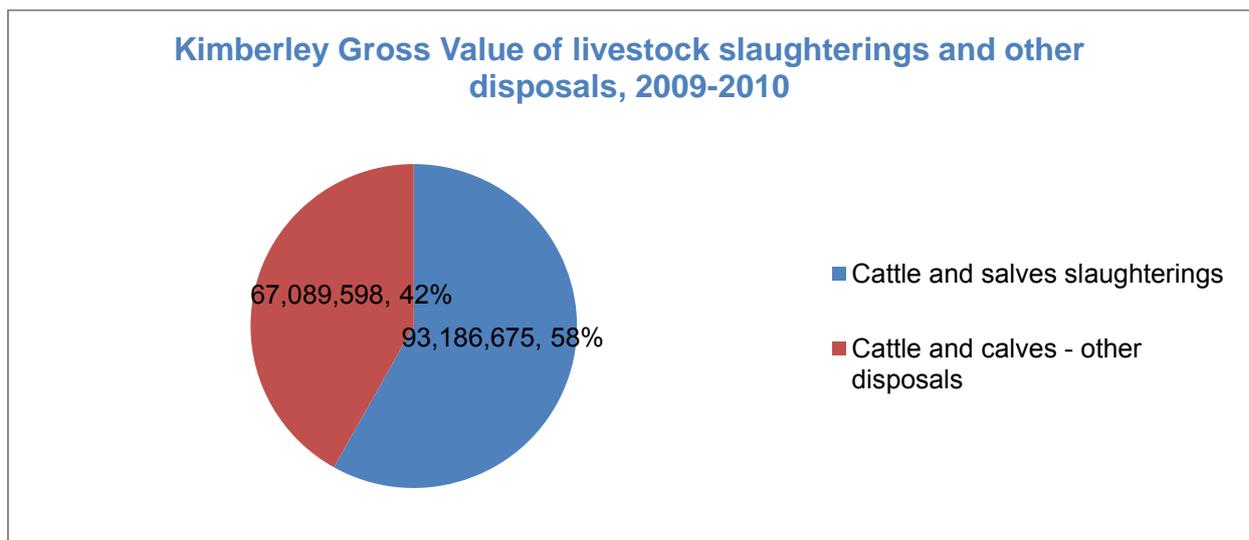
The Kimberley is the closest pastoral zone to Australia’s biggest export market, Indonesia. Annual export figures to Indonesia are around 600,000 head of cattle. Approximately 300,000 cattle are exported out of Darwin annually and between 60,000 to 80,000 are exported out of Broome each year.



The suspension of the live cattle trade to Indonesia in 2011 had a significant impact on the Kimberley region.

The value of cattle disposals from the Region was \$160 million in 2009-10, which made up 82% of the total value of agriculture.

**Figure 17: Kimberley Gross Value of Livestock Production, 2009-2010**



Source: ABS - Agricultural Resource Management Survey/Department of Regional Development, 2010

## Horticulture

The Horticulture industry has undergone considerable growth in the East Kimberley region with the 8,000 hectare, \$220 million Ord East Kimberley Expansion Project. In 2009-10, the Department of Agriculture and Food Western Australia estimated the total value of horticulture production from the Ord River Irrigation Area to be more than \$20 million.

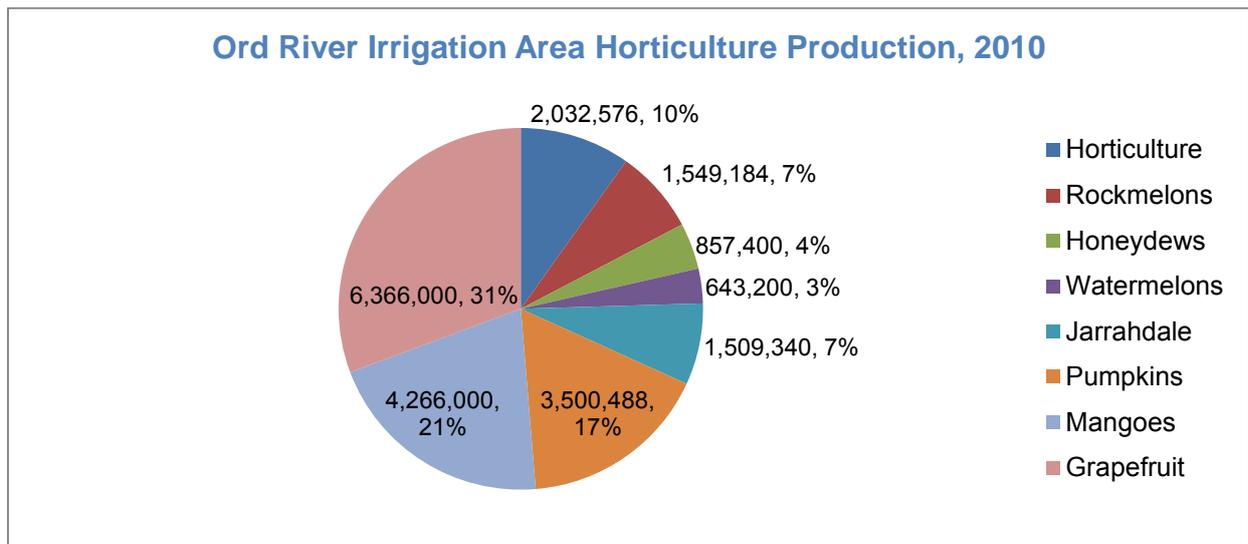


Commercial horticultural production in the West Kimberley occurs on a relatively small scale in the Derby and Broome Shires, with only a few large-scale commercial businesses.

Most of the crops grown in the Kimberley take advantage of the mild to warm 'winter', when they can be grown "out of season" to other Australian production areas

The breakdown of production by horticultural crop is shown in the pie chart below.

**Figure 18: Ord River Irrigation Area Horticulture Production, 2010**



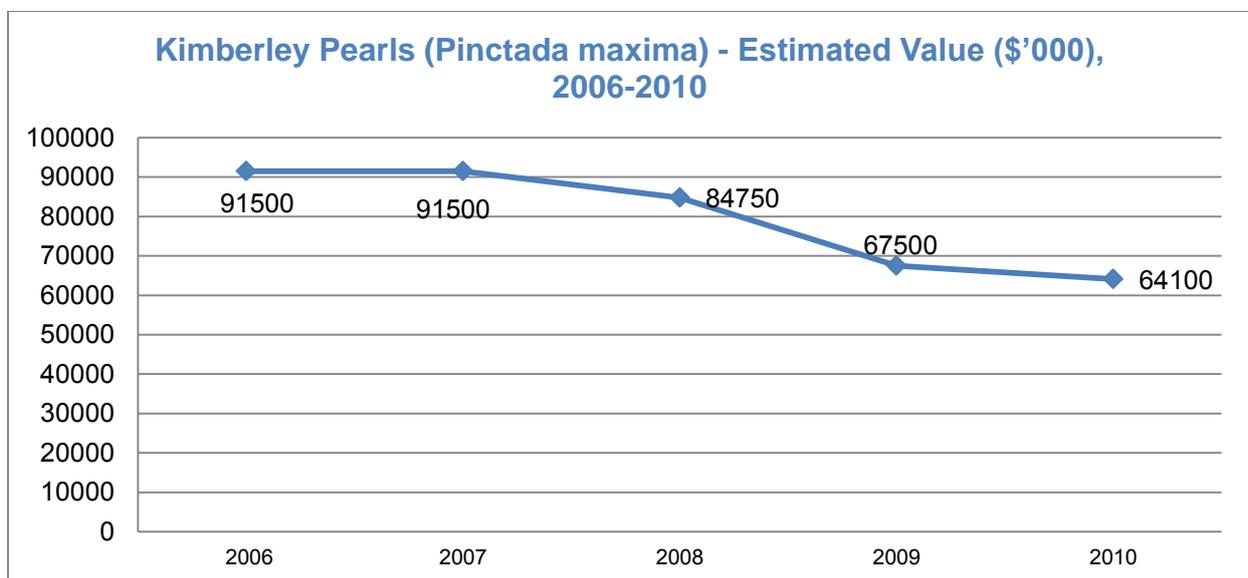
Source: Department of Agriculture and Food Western Australia - ORIA Horticulture Production, 2010

## Aquaculture

The total value of aquaculture in the Kimberley region for 2009-10 was \$67 million. Pearling is an historic industry operating out of Broome with pearl farms along the Kimberley coast. The Kimberley pearl industry peaked at \$91.5 million in 2005-06 and 2006-07, but declined to an estimated \$64.1 million in 2009-10. This decline was caused by the Global Financial Crisis in 2008-09 which has significantly impacted this industry and it was reported that during that time at least 50% of the pearling industry was in 'standby' mode, awaiting improved economic conditions.



**Figure 19: Kimberley Pearls (*Pinctada maxima*) – Estimated Value, 2006-2010**



Source: Department of Fisheries and Department of Regional Development and Lands, Kimberley Pearls (*Pinctada maxima*), 2006-2010

## 7. Strategic Drivers

The Kimberley Regional Group Strategic Planning Workshop was held on February 2012. In this workshop, the following Strategic Drivers or major forces that can shape the future of the Kimberley Region were identified together with an assessment of their relative impact, and what Local Government's regional role could potentially be.

**Table 5: Regional Strategic Drivers**

Strategic Driver	Impact				Local Government Regional Role
	Community	Economy	Environment	Governance	
<b>Housing</b> <ul style="list-style-type: none"> <li>•Affordability, availability, ownership</li> <li>•For key workers</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>•Housing construction facility/project (employment and training, Local Government key worker accommodation)</li> <li>•High level advocacy/facilitation</li> <li>•Land release</li> <li>•Public and private partnership</li> <li>•Building local capacity (regional supply)</li> <li>•Sewerage</li> </ul>
<b>Closing the Gap</b> <ul style="list-style-type: none"> <li>•High % Indigenous population</li> <li>•Accountability for service delivery</li> <li>•Indigenous services – access roads, maintenance (funding)</li> <li>•COAG Agreement</li> <li>•Housing</li> <li>•State planning</li> <li>•ICC/ROC/LIP</li> <li>•Indigenous opportunities and partnerships</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>•Advocate/lobby to influence Local Government service delivery to Aboriginal communities and improving living conditions and life outcomes</li> <li>•Participation in planning and decision making – strong engagement with the community</li> </ul>
<b>Remoteness</b> <ul style="list-style-type: none"> <li>•High level of costs - freight, buildings, insurance premiums</li> <li>•Bureaucracy don't understand</li> <li>•Link to workforce – attracting good people</li> </ul>	High	High	High (built)	High	<ul style="list-style-type: none"> <li>•Advocate for increased funding, subsidies, tax reform, etc.</li> <li>•Social infrastructure (identify and lobby for funding, building, ownership)</li> <li>•Supporting/driving regional preferred suppliers – standards, % weighting</li> </ul>

Strategic Driver	Impact				Local Government Regional Role
	Community	Economy	Environment	Governance	
<b>Tourism</b> <ul style="list-style-type: none"> <li>• Infrastructure</li> <li>• Regional tourism</li> <li>• Tourism marketing and branding</li> <li>• Network coordination</li> </ul>	High	High	High (built) Medium (natural)	High	<ul style="list-style-type: none"> <li>• Access to facilities</li> <li>• Advocacy and marketing</li> <li>• Promoting access</li> <li>• Investigate a differential rating system</li> <li>• Being the driver to enhance visitor's experience in the region (regional branding)</li> <li>• Regional approach to event management</li> <li>• Engaging Aboriginal people in the process</li> <li>• Assisting and facilitating development of Aboriginal tourism enterprises</li> </ul>
<b>Transport</b> <ul style="list-style-type: none"> <li>• Air services and airports</li> <li>• Regional arterial roads and road services</li> <li>• Port development and services</li> </ul>	High	High	High (built) Medium (natural)	High	<ul style="list-style-type: none"> <li>• Advocating for minimum service and infrastructure levels</li> <li>• Advocate and participate in State Transport Strategy</li> <li>• Development of arterial roads, air, and sea networks for the Kimberley region</li> <li>• Advocate for strategic improvements – Tanami road, ports (Point Torment), access to remote airports (RFDS), development of new and extension to existing airports</li> <li>• Promote developer/resource companies contribution to leverage Local Government costs</li> </ul>
<b>Land Tenure</b> <ul style="list-style-type: none"> <li>• Land governance</li> <li>• Pastoral use</li> <li>• Planning scheme, Local Planning strategy and policies</li> <li>• Local contribution</li> <li>• State planning policies</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>• Collaboration on model scheme</li> <li>• High level of advocacy for changes to the Works Planning Act, Mining Act, Indigenous Land Tenure, Pastoral lease and diversification, etc</li> <li>• Land Corporation</li> </ul>
<b>Resource development</b> <ul style="list-style-type: none"> <li>• Oil, gas and minerals</li> <li>• Housing affordability</li> <li>• Availability of staff</li> <li>• Impact on organisation</li> <li>• Impact on the community</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>• Advocate for mining rates on developers</li> <li>• Lobby to increase contribution through State Agreements in relation to community capacity</li> <li>• Lobby for Kimberley Development Commission funding</li> <li>• Advocate for whole of life mine planning</li> </ul>

Strategic Driver	Impact				Local Government Regional Role
	Community	Economy	Environment	Governance	
<b>Water</b> <ul style="list-style-type: none"> <li>• Allocation, supplies</li> <li>• Regional scheme</li> <li>• Water needs to supply development</li> <li>• Water licencing</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>• Regional allocation plan</li> <li>• Advocate for town water supply/security - better allocation of funding</li> <li>• Investigate the mechanism for water licencing/water trading (including sewerage and wastewater)</li> </ul>
<b>Land Use</b> <ul style="list-style-type: none"> <li>• Natural heritage listing</li> <li>• Tourism impact</li> <li>• Resource development</li> <li>• Indigenous control / management of land</li> <li>• Heritage – pastoral, indigenous, culture and historic heritage, archive support records</li> </ul>	High	High	High	High	<ul style="list-style-type: none"> <li>• Investigate the impact of natural heritage listing at the regional level</li> <li>• Develop State of Environment Report at regional level to guide development</li> <li>• Advocate for KRG to be part of the regional planning process.</li> <li>• Kimberley Aboriginal Caring for Country Plan</li> <li>• Conflict between pastoral land and the Department of Planning</li> </ul>
<b>Social Development</b> <ul style="list-style-type: none"> <li>• Health</li> <li>• Senior services</li> <li>• GP services, dental, allied health, St John, mental health</li> <li>• Welfare reform</li> <li>• Appropriate education</li> <li>• Youth development</li> <li>• Employment and training</li> </ul>	High	High	High (built)	High	<ul style="list-style-type: none"> <li>• Identify /lobby/advocate/collaborate for allied and general health services to the region</li> <li>• Advocate for quality of life and health outcomes in the region</li> <li>• Promote welfare reform</li> <li>• Aged care (infrastructure)</li> <li>• Education</li> <li>• Youth development and child care</li> </ul>

Source: KRG Strategic Planning Workshop, February 2012

## 8. Key Regional Issues

In addition to the Strategic Drivers there are a number of Key Regional Issues that were identified as significant in a regional context:

**Table 6: Key Regional Issues**

Key Regional Issues	Description
<b>Labour availability</b>	<ul style="list-style-type: none"> <li>• Workforce - competing demands - salary level and industries</li> </ul>
<b>Power - issue under remoteness</b>	<ul style="list-style-type: none"> <li>• Renewable energy - solar energy efficiency and tidal</li> <li>• Underground</li> <li>• Capacity</li> </ul>
<b>Tax reform</b>	<ul style="list-style-type: none"> <li>• Tax incentive</li> <li>• Zone rebate and allowances (remoteness)</li> <li>• Department of Housing threshold</li> </ul>
<b>Communication – cost associated with issue of remoteness</b>	<ul style="list-style-type: none"> <li>• Optical speed transmission</li> <li>• National Broadband Network</li> <li>• Tower for black spots</li> </ul>
<b>Waste management</b>	<ul style="list-style-type: none"> <li>• Modernise waste management</li> <li>• Recycling management</li> </ul>
<b>Emergency services</b>	<ul style="list-style-type: none"> <li>• Balancing the impact</li> <li>• Fire services</li> <li>• Drivers from the Kelty Report</li> </ul>
<b>Governance</b>	<ul style="list-style-type: none"> <li>• Diversified income</li> <li>• Investment and revenue businesses</li> <li>• Sustainability of the organisation</li> <li>• Freight income stream</li> <li>• Employment – legislative impact</li> </ul>
<b>Resource sharing</b>	<ul style="list-style-type: none"> <li>• Information Technology support</li> <li>• Mobilisation</li> </ul>
<b>Asset Management</b>	<ul style="list-style-type: none"> <li>• Advocacy for Asset Management</li> <li>• Funding for Asset Management</li> <li>• Partner with Aboriginal people</li> <li>• Closing the Gap</li> <li>• Procurement policy</li> </ul>

Source: KRG Strategic Planning Workshop, February 2012

## 9. Emerging Regional Issues

A number of emerging regional issues have also been identified by the Kimberley Regional Group:

**Table 7: Emerging Regional Issues**

Emerging Regional Issues	Stakeholders	Timeframe	Impact			
			Community	Economy	Environment	Governance
<b>Defence White Paper – Broome RAN Naval Base</b>	<ul style="list-style-type: none"> <li>• Department of Defence</li> <li>• Community of Broome</li> <li>• Chambers of Commerce</li> <li>• Aust Government</li> </ul>	10 years	<ul style="list-style-type: none"> <li>• Housing affordability</li> </ul>	<ul style="list-style-type: none"> <li>• Housing sector</li> <li>• Business development</li> <li>• Commercial centre</li> </ul>		<ul style="list-style-type: none"> <li>• Capacity issues for local Shire to manage development; dependent on development timeframes</li> </ul>
<b>Defence White Paper - RAAF Curtin Airport lease changes</b>	<ul style="list-style-type: none"> <li>• Department of Defence</li> <li>• Shire of Derby/West Kimberley</li> <li>• Community</li> <li>• Resource Sector</li> </ul>	1-5 years	<ul style="list-style-type: none"> <li>• Need to retain regular and reliable Regular Public Transport services</li> </ul>	<ul style="list-style-type: none"> <li>• Cost of relocation to Derby Airport</li> <li>• Resources sector</li> </ul>		
<b>Derby - Oil, Gas, Coal and other resource development</b>	<ul style="list-style-type: none"> <li>• Resources sector</li> <li>• Shire of Derby/ West Kimberley</li> <li>• Community</li> <li>• WA Government</li> </ul>	2-10 years	<ul style="list-style-type: none"> <li>• Social and community infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Transport infrastructure</li> <li>• Support Infrastructure</li> </ul>		
<b>Ord River Irrigation Scheme Stage 3</b>	<ul style="list-style-type: none"> <li>• WA Government</li> <li>• NT Government</li> <li>• Aust Government</li> <li>• Miriwung Gajerrong Corporation</li> <li>• Local Govt WA</li> <li>• Local Govt NT</li> </ul>	5-10 years	<ul style="list-style-type: none"> <li>• Social and Community infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Transport infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Aust and State Government requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Cross border jurisdiction changes required</li> <li>• Native title issues</li> </ul>
<b>Bonaparte Gulf – Mining, Oil and Gas exploration</b>	<ul style="list-style-type: none"> <li>• WA Government</li> <li>• NT Government</li> <li>• Aust Government</li> <li>• Local Govt WA</li> <li>• Local Govt NT</li> </ul>	5-10 years	<ul style="list-style-type: none"> <li>• Infrastructure upgrades and housing requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Major airport and port development</li> </ul>	<ul style="list-style-type: none"> <li>• Aust and State Government requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Cross border jurisdiction changes required</li> <li>• Native title issues</li> </ul>

Source: Kimberley Regional Group, June 2012

## 10. Goals, Outcomes & Strategies

The table below provides an overview of the Region’s Strategic Goals and Outcomes:

KRA	Goals	Outcomes
<b>Governance</b>	<b>A collaborative group demonstrating strong Regional Governance</b>	<ul style="list-style-type: none"> <li>• Effective and efficient governance, protocols and systems</li> <li>• Secure funding for regional priorities</li> <li>• Effective engagement with Aboriginal governance structures</li> <li>• Business efficiency and improved services through collaboration</li> <li>• Recognition of Kimberley Local Government issues and opportunities at the Regional, State, National and International levels</li> <li>• Alignment and integration of regional and local priorities for Member Councils</li> </ul>
<b>Natural Environment</b>	<b>Responsible management of the environment</b>	<ul style="list-style-type: none"> <li>• Improved institutional fire management regimes</li> <li>• Integrated waste management</li> <li>• Secure quality water supply</li> <li>• Reuse of waste water</li> </ul>
<b>Built Environment</b>	<b>Improved and secure transport, communications, community and essential services</b>	<ul style="list-style-type: none"> <li>• Attractive townscapes supporting communities</li> <li>• Improved regional arterial road network, ports and airports</li> <li>• Adequate land supply</li> <li>• High standard of infrastructure planning for Indigenous communities</li> <li>• Reliable and adequate power and communications</li> </ul>
<b>Community</b>	<b>A vibrant community based on equity, inclusion and opportunity for all</b>	<ul style="list-style-type: none"> <li>• Innovative and joined up approach to Housing development, ownership and design through community participation</li> <li>• Improved regional outcomes in education and health</li> <li>• Best practice in community services</li> <li>• Advocate for Improved range and standard of municipal services to Aboriginal communities</li> </ul>
<b>Economy</b>	<b>A sustainable and diverse economy</b>	<ul style="list-style-type: none"> <li>• Generational advantage that captures the wealth for the region</li> <li>• Improved outcomes in employment</li> <li>• A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the regions assets (human, environment and built)</li> </ul>



**Shire of Derby /  
West Kimberley**



## Governance

*Goal 1: A collaborative group demonstrating strong regional governance*

**Outcome 1.1 Effective and efficient governance, protocols and systems**

**Strategy 1.1.1 Develop/negotiate effective business processes and systems for the Kimberley Regional Group**

**Strategy 1.1.2 Establish a governance structure for the Kimberley Regional Group**

**Strategy 1.1.3 Develop and implement a regional Procurement Strategy**

**Outcome 1.2 Secure funding for regional priorities**

**Strategy 1.2.1 Develop business cases for regional priorities**

**Outcome 1.3 Effective engagement with Aboriginal governance structures**

**Strategy 1.3.1 Develop relationships and consultation processes with Aboriginal groups at a regional level**

**Outcome 1.4 Business efficiency and improved services through collaboration**

**Strategy 1.4.1 Promote and facilitate mutually beneficial, cost efficient shared services arrangements**

**Outcome 1.5 Recognition of Kimberley Local Government issues and opportunities at the regional, state, national and international levels**

**Strategy 1.5.1 Engage and collaborate with stakeholders to champion the interests of the Kimberley Regional Group and the region**

**Outcome 1.6 Alignment and integration of regional and local priorities for Member Councils**

**Strategy 1.6.1 Ensure two-way communication between the Kimberley Regional Group and Member Councils enabling local and regional perspective and priorities to be considered**



## Natural Environment

*Goal 2: Responsible management of the environment*

**Outcome 2.1 Secure quality water supply**

**Strategy 2.1.1 Advocate for improved water supply (quality and quantity) where required**

**Outcome 2.2 Integrated waste management**

**Strategy 2.2.1 Develop and implement a regional Integrated Waste Management Strategy and operational plan**

**Outcome 2.3 Improved institutional fire management regimes**

**Strategy 2.3.1 Lobby for a Northern Fire Authority Department**

**Outcome 2.4 Reuse of waste water**

**Strategy 2.4.1 Advocate for affordable and improved waste water management that considers innovative technology**



## Built Environment

*Goal 3: Improved and secure transport, communications, community and essential services*

**Outcome 3.1 Attractive townscapes supporting communities**

**Strategy 3.1.1 Develop partnerships with relevant State agencies to progress town enhancement projects across the region recognising the unique character of each individual town**

**Outcome 3.2 Improved regional arterial road network, ports and airports**

**Strategy 3.2.1 Develop and provide input to regional transport priorities, policies and plans**

**Outcome 3.3 Adequate land supply**

**Strategy 3.3.1 Advocate and investigate land development alternatives in partnership with Local Government and relevant stakeholders**

**Outcome 3.4 High standard of infrastructure planning for Indigenous communities**

**Strategy 3.4.1 Lobby the Western Australian Planning Commission (WAPC) for improvement and involvement by Shires into Community Layout Planning process**

**Outcome 3.5 Reliable and adequate power and communications**

**Strategy 3.5.1 Advocate for improved Information and Communication Technology (ICT) infrastructure for the region**

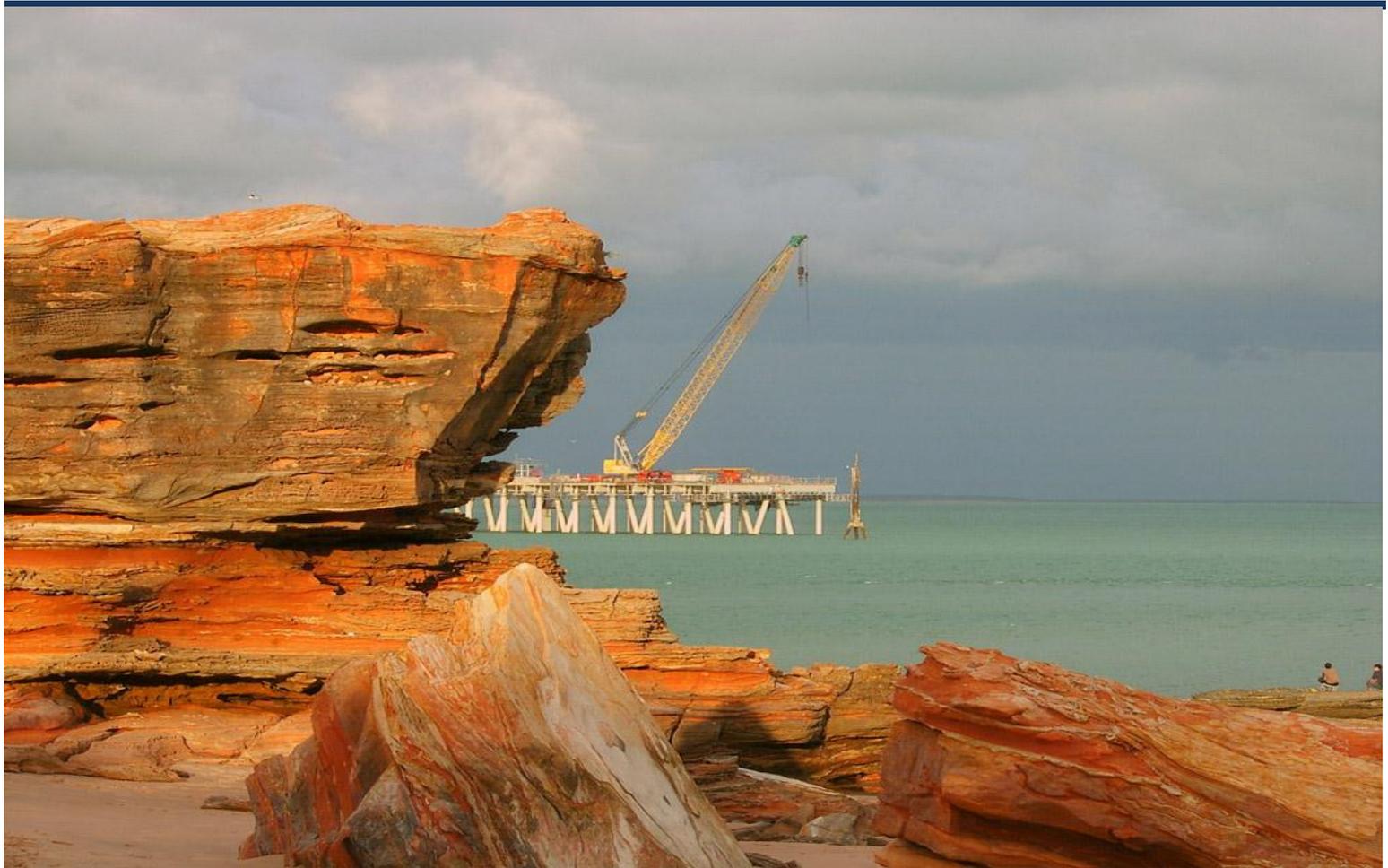
**Strategy 3.5.2 Request that the Kimberley Regional Planning Committee ensure that the Kimberley infrastructure development plan adequately forecasts utility provision for strategic needs of Kimberley Local Governments**



## Community

*Goal 4: A vibrant community based on equity, inclusion and opportunity for all*

- Outcome 4.1 Innovative and joined up approach to housing development, ownership and design through community participation**
- Strategy 4.1.1 Encourage appropriate housing design, style and appropriate management for the local environment**
  - Strategy 4.1.2 Promote a planned approach to manage population growth and access to a choice of housing options**
- Outcome 4.2 Improved regional outcomes in education and health**
- Strategy 4.2.1 Advocate for improved regional health services and infrastructure**
  - Strategy 4.2.2 Advocate for improved regional education and training opportunities**
- Outcome 4.3 Best practice in community services**
- Strategy 4.3.1 Collaborate with State Government and other agencies to ensure co-ordinated provision of services**
- Outcome 4.4 Advocate for an improved range and standard of municipal services to Aboriginal communities**
- Strategy 4.4.1 Liaise with stakeholder organisations to determine the future of municipal service delivery to Aboriginal communities**



# Economy

*Goal 5: A sustainable and diverse economy*

**Outcome 5.1** Generational advantage that captures the wealth for the region

**Strategy 5.1.1** Support regional partnerships with both private and public sector for future investment in the region

**Outcome 5.2** Improved outcomes in employment

**Strategy 5.2.1** Promote and advocate at the state and national level our region's readiness for investment opportunities

**Strategy 5.2.2** Support and advocate regional employment programs and opportunities

**Outcome 5.3** A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the region's assets (human, environment and built)

**Strategy 5.3.1** Promote tourism opportunities at a regional level

## 11. Strategic Measures

An important part of the Integrated Planning process, is the monitoring and reporting of results achieved in the implementation of the Kimberley Strategic Community Plan.

The following outcome based key performance indicators have been identified to track progress against the outcomes of this Plan. This is a preliminary list of key performance indicators which may be revised when the Plan is reviewed.

**Table 8: Strategic Measures**

KRA	Goals	Outcomes	Key Performance Measures
Governance	A collaborative group demonstrating strong regional governance	Effective and efficient governance, protocols and systems	<ul style="list-style-type: none"> <li>Shire satisfaction with the business process and systems for KRG</li> </ul>
		Secure funding for regional priorities	<ul style="list-style-type: none"> <li>Value of grants secured for regional priorities</li> </ul>
		Effective engagement with Aboriginal governance structures	<ul style="list-style-type: none"> <li>Number of Aboriginal governance bodies with which consultation is undertaken</li> </ul>
		Business efficiency and improved services through collaboration	<ul style="list-style-type: none"> <li>Shire satisfaction with the business process and systems for KRG</li> <li>% change in cost of services by Shire for shared services</li> </ul>
		Recognition of Kimberley Local Government issues and opportunities at the regional, state, national and international levels	<ul style="list-style-type: none"> <li>Progress measured by completion of action in Regional Business Plan</li> </ul>
		Alignment and integration of regional and local priorities for Member Councils	<ul style="list-style-type: none"> <li>Shire satisfaction with the business process and systems for KRG</li> </ul>
Natural Environment	Responsible management of the environment	Secure quality water supply	<ul style="list-style-type: none"> <li>% compliance with the Australian Drinking Water Guidelines for potable water supply in Shire towns</li> </ul>
		Integrated waste management	<ul style="list-style-type: none"> <li>Progress measured by completion of action in Regional Business Plan</li> <li>% of waste diverted from landfill</li> </ul>
		Improved institutional fire management regimes	<ul style="list-style-type: none"> <li>Progress measured by completion of action(s) in Regional Business Plan</li> </ul>
		Reuse of waste water	<ul style="list-style-type: none"> <li>% of waste water that is reused in Shire towns</li> </ul>

KRA	Goals	Outcomes	Key Performance Measures
Built Environment	Improved and secure transport, communications, community and essential services	Attractive townscapes supporting communities	<ul style="list-style-type: none"> <li>• % community satisfaction with town entrances and streetscapes</li> </ul>
		Improved regional arterial road network, ports and airports	<ul style="list-style-type: none"> <li>• Community satisfaction with the road network across the region</li> <li>• Community satisfaction with airports across the region</li> <li>• Industry satisfaction with port facilities across the region</li> </ul>
		Adequate land supply	<ul style="list-style-type: none"> <li>• % change in number of residential dwelling sites across the region</li> </ul>
		High standard of infrastructure planning for Indigenous communities	<ul style="list-style-type: none"> <li>• Progress measured by completion of action(s) in Regional Business Plan</li> </ul>
		Reliable and adequate power and communications	<ul style="list-style-type: none"> <li>• % community satisfaction with Information and Technology infrastructure across the region</li> <li>• % community satisfaction with power supply across the region</li> </ul>
Community	A vibrant community based on equity, inclusion and opportunity for all	Innovative and joined up approach to housing development, ownership and design through community participation	<ul style="list-style-type: none"> <li>• % community satisfaction with housing options in Shire towns</li> </ul>
		Improved regional outcomes in education and health	<ul style="list-style-type: none"> <li>• % community satisfaction with regional health services and facilities</li> <li>• % community satisfaction with regional education and training opportunities</li> </ul>
		Best practice in community services	<ul style="list-style-type: none"> <li>• % community satisfaction with community services across the region</li> </ul>
		Advocate for an improved range and standard of municipal services to Aboriginal communities	<ul style="list-style-type: none"> <li>• Progress measured by completion of action(s) in Regional Business Plan</li> </ul>

KRA	Goals	Outcomes	Key Performance Measures
Economy	A sustainable and diverse economy	Generational advantage that captures the wealth for the region	<ul style="list-style-type: none"> <li>Value of regional investment through public and private sector partnership developments</li> </ul>
		Improved outcomes in employment	<ul style="list-style-type: none"> <li>% change in employment across the region</li> </ul>
		A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the region's assets (human, environment and built)	<ul style="list-style-type: none"> <li>% change in the number of visitors across the region</li> <li>% change in visitor's spending across the region</li> <li>% change in number of nights spent by visitors across the region</li> </ul>

## 12. Stakeholder Alignment

The Plan recognises the importance of partnerships with external stakeholders, including Commonwealth, State and Local Governments to ensure that there is a collaborative approach to providing appropriate services and infrastructure in the Kimberley region. The table below shows the alignment of key stakeholder plans with the strategies of this Plan.

**Table 9: Stakeholder Alignment**

No	Strategies	Related Plan Alignment	Plan Origin
1.1.1	Develop/negotiate effective business processes and systems for the Kimberley Regional Group	None identified	None
1.1.2	Establish a governance structure for the Kimberley Regional Group	None identified	None
1.1.3	Develop and implement a regional Procurement Strategy	Indigenous Economic Development Strategy 2011–2018	Commonwealth
1.2.1	Develop business cases for regional priorities	KDC Strategic Plan 2009	State
1.3.1	Develop relationships and consultation processes with Aboriginal groups at a regional level	Regional Development Policy Framework 2011	State
		Kimberley Science and Conservation Strategy 2011	State
		Indigenous Economic Development Strategy 2011–2018	Commonwealth
		A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011	Local Govt
		National Partnership Agreement on Remote Service Delivery 2008	Commonwealth
1.4.1	Promote and facilitate mutually beneficial, cost efficient shared services arrangements	Gascoyne Strategic Plan 2011	Regional
1.5.1	Engage and collaborate with stakeholders to champion the interests of the Kimberley Regional Group and the region	Pilbara Regional Council Strategic Plan 2011-2014	Regional
1.6.1	Ensure two-way communication between the Kimberley Regional Group and Member Councils enabling local and regional perspective and priorities to be considered	Shire of Broome Strategic Plan 2011 - 2016	Shire
2.1.1	Advocate for improved water supply (quality and quantity) where required	Kimberley Regional Water Plan 2010–2030	State
		Kimberley Science and Conservation Strategy 2011	State
2.2.1	Develop and implement a regional Integrated Waste Management Strategy and operational plan	Western Australian Waste Strategy 2012	State
2.3.1	Lobby for a Northern Fire Authority Department	Regional Plan Kimberley 2011	Commonwealth

No	Strategies	Related Plan Alignment	Plan Origin
2.4.1	Advocate for affordable and improved waste water management considering innovative technology	Kimberley Regional Water Plan 2010–2030	State
3.1.1	Develop partnership with relevant state agencies to progress town enhancement projects across the region recognising the unique character of each individual town	State Planning Strategy 1997	State
3.2.1	Develop and provide input to regional transport priorities, policies and plans	Freight & Logistics Services in the East Kimberley Region 2010	State
		Roads 2025 Regional Road Development Strategy Kimberley, 2007	Local Govt
		Regional Plan Kimberley 2011	Commonwealth
		Local Government Roads and Transport Strategy 2006-2016	Local Govt
		State Planning Strategy 1997	State
3.3.1	Advocate and investigate land development alternatives in partnership with Local Government and relevant stakeholders	Affordable Housing Strategy 2010-2020	State
		State Planning Strategy 1997	State
3.4.1	Lobby the Western Australian Planning Commission (WAPC) for improvement and involvement by Shires into Community Layout Planning process	Indigenous Economic Development Strategy 2011–2018	Commonwealth
		National Partnership Agreement on Remote Service Delivery 2008	Commonwealth
		State Planning Policy 1997	State
3.5.1	Advocate for improved Information and Communication Technology (ICT) infrastructure for the region	None identified	External
3.5.2	Request that the Kimberley Regional Planning Committee ensure that the Kimberley infrastructure development plan adequately forecasts utility provision for strategic needs of Kimberley Local Governments	Western Australian State Sustainability Strategy 2003	State
		Energy 2031 - Strategic Energy Initiative Directions Paper, 2011	State
		A Sustainable Street Lighting Project Proposal for Western Australia 2011	Local Govt
4.1.1	Encourage appropriate housing design, style and appropriate management for the local environment	Affordable Housing Strategy 2010-2020	State
4.1.2	Promote a planned approach to manage population growth and access to a choice of housing options	State Sustainability Strategy – Western Australia 2003	State
4.2.1	Advocate for improved regional health services and infrastructure	Regional Development Policy Framework 2011	State
		Regional Plan Kimberley 2011	Commonwealth
		National Partnership Agreement on Closing the Gap in Indigenous Health Outcomes 2009	Commonwealth

No	Strategies	Related Plan Alignment	Plan Origin
		Western Australian Strategic Plan for Safety and Quality in Health Care 2008-2013	State
		WA Health Operational Plan 2010-2011	State
		WA Health Strategic Intent 2010-2015	State
4.2.2	Advocate for improved regional education and training opportunities	Regional Development Policy Framework 2011	State
		Regional Plan Kimberley 2011	Commonwealth
		National Education Agreement 2009	Commonwealth
4.3.1	Collaborate with State Government and other agencies to ensure co-ordinated provision of services	Shire of Broome Strategic Plan 2011 - 2016	Shire
4.4.1	Liaise with stakeholder organisations to determine the future of municipal service delivery to Aboriginal communities	A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011	Local Govt
5.1.1	Support regional partnerships with both private and public sector for future investment in the region	KDC Strategic Plan 2009	State
5.2.1	Promote and advocate at the state and national level our region's readiness for investment opportunities	KDC Strategic Plan 2009	State
5.2.2	Support and advocate regional employment programs and opportunities	Regional Plan Kimberley 2011	Commonwealth
		Kimberley Tafe Strategic Plan 2010-2012	State
5.3.1	Promote tourism opportunities at a regional level	Regional Plan Kimberley 2011	Commonwealth
		Tourism Western Australia Strategic Plan 2010-2011	State
		Australia's North West Tourism Development Priorities 2010-2015	State

A more detailed version of the Related Plan alignment can be found in Appendix 1.

The following table identifies the top five stakeholder plans in terms of related strategy alignment:

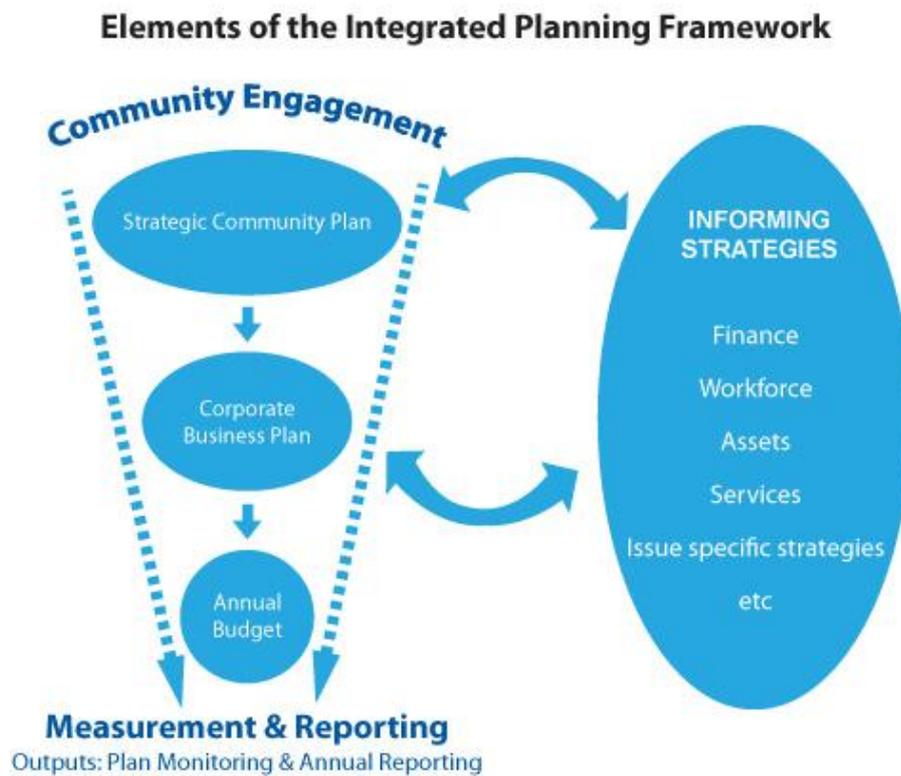
**Table 10: Alignment of Strategies with Key Stakeholder Related Plans**

Key Stakeholder Related Plan	Number of Strategies Aligned
Regional Plan Kimberley 2011	6
Regional Development Policy Framework	3
KDC Strategic Plan	3
Indigenous Economic Development Strategy 2011–2018	3
National Partnership Agreement on Remote Service Delivery	2

### 13. Regional Strategic Plan Implementation

The Kimberley Strategic Community Plan drives the development of the Regional Business Plan, which in turn drives the Implementation Plan and Annual Budget (refer figure below).

**Figure 20: Local Government Integrated Planning Framework**



The progressive linking in of informing strategies will ensure that all components of the Kimberley Regional Group’s operations are integrated into the planning and resourcing framework.

Implementation of the Kimberley Strategic Community Plan is achieved through the development of priority actions and projects as part of the Regional Business Planning and subsequent Implementation Planning processes.

Key performance indicators at the strategic and operational levels provide valuable information on how effectively and efficiently the Kimberley Strategic Community Plan and the Regional Business Plan have been implemented.

## 14. Glossary

ABS	Australian Bureau of Statistics
CEO	Chief Executive Officer
CME	Chamber of Minerals and Energy WA
COAG	Council of Australian Governments
DEC	Department of Environment and Conservation
DLG	Department of Local Government
EOI	Expression of Interest
FaHCSIA	Department of Families Housing Community Services and Indigenous Affairs
FTE	Full Time Equivalent
GIS	Geographic Information System
Govt	Government
IT	Information Technology
KDC	Kimberley Development Commission
KPI	Key Performance Indicator
KRG	Kimberley Regional Group
LG	Local Government
OHS	Occupational Health and Safety
RDA	Regional Development Australia
SDWK	Shire of Derby West Kimberley
SHC	Shire of Halls Creek
SOB	Shire of Broome
SWEK	Shire of Wyndham East Kimberley
WA	Western Australia
WAAMI	Western Australian Asset Management Improvement
WALGA	Western Australian Local Government Association
WAPC	Western Australian Planning Commission

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## 16. Appendix 1: Stakeholder Alignment

### Goal 1: A collaborative group demonstrating strong Regional Governance

#### Outcome 1.1: Effective and efficient governance, protocols and systems

**Strategy 1.1.1: Develop/negotiate effective business processes and systems for the Kimberley Regional Group**

Related Plans	Strategy Alignment
None identified	Not Applicable

**Strategy 1.1.2: Establish a governance structure for the Kimberley Regional Group**

Related Plans	Strategy Alignment
None identified	Not Applicable

**Strategy 1.1.3: Develop and implement a regional Procurement Strategy**

Related Plans	Strategy Alignment
Indigenous Economic Development Strategy 2011–2018	<p><b>Priority 3:</b> Skills Development and Jobs</p> <p><b>Objective 3.4</b> Improve Indigenous employment outcomes through government procurement and service delivery</p> <p><b>Strategy:</b> Reform government procurement to improve Indigenous employment outcomes from government investment</p>

#### Outcome 1.2: Secure funding for regional priorities

**Strategy 1.2.1: Develop business cases for regional priorities**

Related Plans	Strategy Alignment
KDC Strategic Plan 2009	<p><b>Service 2:</b> The Commission will identify, co-ordinate and promote through its major strategies the establishment, improvement and diversification of the region's infrastructure, industries and enterprises.</p> <p><b>Strategy:</b> Facilitate access to Funding for Initiatives in the Region - The Commission will facilitate the allocation of direct grant funding to significant projects and identify and facilitate appropriate funding from other sources.</p>

**Outcome 1.3: Effective engagement with Aboriginal governance structures**

***Strategy 1.3.1: Develop relationships and consultation processes with Aboriginal groups at a regional level***

Related Plans	Strategy Alignment
Regional Development Policy Framework 2011	<p><b>Priority 6:</b> Leadership and Decision-making</p> <p><b>Outcomes:</b></p> <p>Emerging community leaders to be identified and developed and encouraged to participate in local and regional development initiatives.</p> <p>Regional groups to be well represented in decision-making and planning forums on matters that impact upon the wellbeing and development of regional communities and businesses.</p> <p>Strategic planning for regional land use, service delivery and infrastructure provision to be well integrated across and within State, Commonwealth and Local Governments. This integration includes the participation of private and not-for-profit sectors.</p> <p>Indigenous leaders to be encouraged to develop local solutions to regional and Indigenous challenges and to enhance Indigenous representation at all levels of government policy development, program roll-out and service delivery.</p>
Kimberley Science and Conservation Strategy 2011	<p><b>Theme 2:</b> Employing and involving local Aboriginal people and maintaining the Kimberley's rich culture</p> <p><b>Objective:</b> To promote joint management and recognition of Aboriginal customary activities</p> <p><b>Strategy:</b></p> <p>Involve Aboriginal people in managing land and sea country through joint management arrangements as well as in making decisions and being involved in the planning and implementation of land management activities.</p>
Indigenous Economic Development Strategy 2011–2018	<p><b>Priority 1:</b> Strengthening Foundations</p> <p><b>Objective 1.7</b> Support good governance and increase coordinated government</p> <p><b>Strategies:</b></p> <p>Support good corporate governance</p> <p>Increase place based partnerships</p>
A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011	<p><b>Strategy Component 3.5:</b> Governance Development and Community Engagement</p> <p><b>Outcomes:</b></p> <p>Improved community engagement to underpin council decision-making that better reflects local community aspirations, needs and priorities.</p>
National Partnership Agreement on Remote Service Delivery 2008	<p><b>C9 Indigenous engagement principle:</b> Engagement with Indigenous men, women and children and communities should be central to the design and delivery of programs and services.</p>

**Outcome 1.4: Business efficiency and improved services through collaboration**

***Strategy 1.4.1: Promote and facilitate mutually beneficial, cost efficient shared services arrangements***

Related Plans	Strategy Alignment
Gascoyne Strategic Plan 2011	<p><b>Outcome R4.5:</b> Improved collaborative business performance and quality of service delivery.</p> <p><b>Strategy R4.5.1:</b> Promote and facilitate organisational enhancement through the delivery of shared services.</p> <p><b>Strategy R4.5.2:</b> Promote and support the implementation of organisational performance management systems aligned to Strategic Community Plans.</p>

**Outcome 1.5: Recognition of Kimberley Local Government issues and opportunities at the regional, state, national and international levels**

***Strategy 1.5.1: Engage and collaborate with stakeholders to champion the interests of the Kimberley Regional Group and the region***

Related Plans	Strategy Alignment
Pilbara Regional Council Strategic Plan 2011-2014	<p><b>Key Result Area 2:</b> A Voice for the Pilbara</p> <p><b>Strategy 2:</b> Improve collaboration and communication with key regional bodies to champion the interests of the PRC and the Region.</p>

**Outcome 1.6: Alignment and integration of regional and local priorities for Member Councils**

***Strategy 1.6.1: Ensure two-way communication between the Kimberley Regional Group and Member Councils enabling local and regional perspective and priorities to be considered***

Related Plans	Strategy Alignment
Shire of Broome Strategic Plan 2011-2016	<p><b>Strategy 1:</b> Work with agencies and others to ensure affordable and equitable services and infrastructure.</p> <p><b>Outcome 2:</b> Actively participate in the Kimberley Zone of Local Government and the RLG</p> <p><b>Action:</b> Ensure two-way communication between the Zone and Shire of Broome Council exists allowing local perspective and priorities to be considered by Zone and vice versa</p>

**Goal 2: Responsible management of the environment**

**Outcome 2.1: Secure quality water supply**

***Strategy 2.1.1: Advocate for improved water supply (quality and quantity) where required***

Related Plans	Strategy Alignment
Kimberley Regional Water Plan 2010–2030	<p><b>Strategic Objective 1:</b> Understand Kimberley water resources and water systems</p> <p><b>Strategy 1.3:</b> Understand urban and industry water resource demand and use</p> <p><b>Strategic Objective 4:</b> Sustain and grow Kimberley towns and communities</p> <p><b>Strategy 4.1:</b> Drive improvement of water services and source protection in remote communities</p> <p><b>Strategy 4.2:</b> Protect and develop drinking water sources</p>
Kimberley Science and Conservation Strategy 2011	<p><b>Objective:</b> To plan for the future</p> <p><b>Strategy:</b>                      Implement the strategic directions and actions for sustainable management of water resources and water services in the Kimberley to 2030 as outlined in the Kimberley regional water plan.</p>

**Outcome 2.2: Integrated waste management**

***Strategy 2.2.1: Develop and implement a regional Integrated Waste Management Strategy and operational plan***

Related Plans	Strategy Alignment
Western Australian Waste Strategy 2012	<p><b>Strategic objective 3:</b> Develop best practice guidelines, measures and reporting frameworks and promote their adoption.</p> <p><b>Incentive strategies:</b></p> <p>3 f. Fund trials of systems that achieve best practice outcomes to determine their applicability to WA.</p> <p>3 g. Provide funding support to encourage the adoption of systems that achieve best practice outcomes.</p> <p><b>Strategic objective 5:</b> Communicate messages for behaviour change and promote its adoption, and acknowledge the success of individuals and organisations that act in accord with the aims and principles in the Strategy and assist in its implementation.</p> <p><b>Incentive strategies:</b></p> <p>5 e. Develop and support programs and initiatives, including awards, that acknowledge, celebrate and reward excellence in waste avoidance, resource recovery and reduced landfilling behaviours and outcomes and that contribute to the implementation of this Strategy.</p>

**Outcome 2.3: Improved institutional fire management regimes**

***Strategy 2.3.1: Lobby for a Northern Fire Authority Department***

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<b>Goal:</b> More resources and staffing allocated to essential services <b>Strategy:</b> Increase resources to Police, Fire, Ambulance and FESA in the region

**Outcome 2.4: Reuse of waste water**

***Strategy 2.4.1: Advocate for affordable and improved waste water management that considers innovative technology***

Related Plans	Strategy Alignment
Kimberley Regional Water Plan 2010–2030	<b>Strategic objective 1:</b> Understand Kimberley water resources and water systems <b>Strategy 1.2:</b> Understand threats and impacts on water resources  <b>Strategic Objective 4:</b> Sustain and grow Kimberley towns and communities <b>Strategy 4.1:</b> Drive improvement of water services and source protection in remote communities <b>Strategy 4.4:</b> Realise the potential of wastewater <b>Strategy 4.5:</b> Encourage water sensitive communities

**Goal 3: Improved and secure transport, communications, community and essential services**

**Outcome 3.1: Attractive townscapes supporting communities**

***Strategy 3.1.1: Develop partnerships with relevant State agencies to progress town enhancement projects across the region recognising the unique character of each individual town***

Related Plans	Strategy Alignment
State Planning Strategy 1997	<p><b>Community Planning Priorities:</b>                      Develop Liveable Neighbourhoods: Community Design Code firstly for outer and middle suburbs and subsequently for inner suburbs and regional towns.                      Prepare commercial centres strategies for key regional centres to be incorporated in district schemes.</p> <p><b>General Planning Priorities for Regions:</b>                      The State Government assists local governments with the preparation of joint town planning schemes where regional issues cross local government boundaries.</p>

**Outcome 3.2: Improved regional arterial road network, ports and airports**

***Strategy 3.2.1: Develop and provide input to regional transport priorities, policies and plans***

Related Plans	Strategy Alignment
Freight & Logistics Services in the East Kimberley Region 2010	<p><b>Strategic Themes:</b>                      (1) the need for targeted and scalable port development,                      (2) the need for investment in road and freight infrastructure to support the growth across the region</p> <p><b>Key findings</b>                      Need to undertake targeted and scalable port investment as demand grows                      Need to ensure that road networks and related freight infrastructure are enhanced to support the freight task</p>
Roads 2025 Regional Road Development Strategy Kimberley, 2007	<p>Specific development strategies for 9 State Roads and 51 Local Government Roads.</p>

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<p><b>Goals:</b>                      To demonstrate the economic benefits of provision of essential transport routes for resources industries.                      All year access and linkages.</p> <p><b>Strategies:</b>                      Identify what infrastructure upgrade requirements are essential for the on-going expansion of resources industries in the region particularly road networks, ports, and air routes                      Rebuild and upgrade Highway 1                      Lobby for public transport services in towns                      Lobby for affordable inter and intra state transport                      Lobby for support for regular public transport between Asia and the Kimberley region</p>
Local Government Roads and Transport Strategy 2006- 2016	<p><b>Objectives:</b>                      Permanent funding arrangements for local government to ensure a well-managed and sustainable local road system that provides appropriate access and levels of service for all communities.                      Transport systems in urban areas that provide a balance between urban amenity, freight efficiency and viable alternatives to the use of private motor vehicles.                      Equitable access, particularly to essential services, through integrated planning and the provision of appropriate transport services for all regional communities.                      Efficient movement of freight vehicles through local communities in a way which recognises community concerns for safety and amenity.                      The long-term financial sustainability of local road and transport systems.</p>
State Planning Strategy 1997	<p><b>8.2.1 Kimberley Region</b>  <b>Strategy:</b> Provide strategic transport linkages within and to the Kimberley.</p>

**Outcome 3.3: Adequate land supply**

***Strategy 3.3.1: Advocate and investigate land development alternatives in partnership with Local Government and relevant stakeholders***

Related Plans	Strategy Alignment
Affordable Housing Strategy 2010-2020	<p><b>Strategy Two</b> - Improve Housing Supply                      Housing strategies will be developed by local government authorities and regional bodies</p> <p><b>Key Reform Areas: 4.</b> Develop Alternative Tenures  <b>Strategy:</b> Alternative tenure arrangements will be explored.</p>
State Planning Strategy 1997	<p><b>Strategies:</b>                      3.4: Coordination of Land Supply                      4.0: Plans for Our Future - The plans will allocate land uses and establish land development priorities in particular regions of the State.</p>

**Outcome 3.4: High standard of infrastructure planning for Indigenous communities**

***Strategy 3.4.1: Lobby the Western Australian Planning Commission (WAPC) for improvement and involvement by Shires into Community Layout Planning process***

Related Plans	Strategy Alignment
Indigenous Economic Development Strategy 2011–2018	<p><b>Objective 1.2</b> Ensure infrastructure investments support economic development  <b>Strategies:</b>                      Improve essential infrastructure</p>
National Partnership Agreement on Remote Service Delivery 2008	<p><b>Outputs:</b>                      (i) the identification of gaps in priority local infrastructure</p>
State Planning Policy 1997	<p><b>3.2 Aboriginal Communities</b>  <b>Objectives:</b>                      To provide for the recognition of Aboriginal settlements through local planning schemes and strategies.                      To collaboratively plan for the orderly and coordinated development of Aboriginal settlements.</p>

**Outcome 3.5: Reliable and adequate power and communications**

***Strategy 3.5.1: Advocate for improved Information and Communication Technology (ICT) infrastructure for the region***

Related Plans	Strategy Alignment
None identified	Not Applicable

***Strategy 3.5.2: Request that the Kimberley Regional Planning Committee ensure that the Kimberley infrastructure development plan adequately forecasts utility provision for strategic needs of Kimberley Local Governments***

Related Plans	Strategy Alignment
Western Australian State Sustainability Strategy 2003	<b>Objective:</b> Adoption of best practice energy management in the Western Australian community, including government.
Energy 2031 - Strategic Energy Initiative Directions Paper, 2011	<b>Objective:</b> Continue the State Underground Power Program, following review and adjustment to improve cost effectiveness
A Sustainable Street Lighting Project Proposal for Western Australia 2011	<b>Objectives:</b> To enhance Local Governments capacity to buffer the effects of energy pricing increases and street lighting maintenance costs over time To identify the Capital Cost of a replacement program for WA and how this may be appropriately shared by all levels of government for the benefit of the Australian community.

**Goal 4: A vibrant community based on equity, inclusion and opportunity for all**

**Outcome 4.1: Innovative and joined up approach to housing development, ownership and design through community participation**

***Strategy 4.1.1: Encourage appropriate housing design, style and appropriate management for the local environment***

Related Plans	Strategy Alignment
Affordable Housing Strategy 2010-2020	<p><b>Strategy Two - Improve Housing Supply</b>                      Housing strategies will be developed by local government authorities and regional bodies</p> <p><b>Key Complementary Actions</b>                      Work with industry and local government to encourage more diversity in the size, nature and choice of dwellings offered in the marketplace, in large part through demonstration trials and wider choice in local housing strategies.                      Explore a best practice model for local government development approvals                      Use public/private partnerships to leverage government assets, attract private investment and trial innovative developments.</p>

***Strategy 4.1.2: Promote a planned approach to manage population growth and access to a choice of housing options***

Related Plans	Strategy Alignment
State Sustainability Strategy – Western Australia 2003	<p><b>Vision:</b> A responsive housing system in Western Australia that meets the changing needs, aspirations and choices of all citizens in a sustainable way.</p> <p><b>Objectives:</b>                      Provide the appropriate mix of public housing, affordable private housing and community housing.                      Ensure that this housing provides on-going sustainability benefits.</p> <p><b>Strategies:</b>  <b>5.13:</b> Develop a Community Housing Framework involving standards and accreditation to ensure world best practice in the provision of this housing type and create opportunities for partnerships.  <b>5.14:</b> Use the sustainability agenda to facilitate the community housing sector through its ability to create synergies such as community scale technologies, local government support and ethical investment.</p>

**Outcome 4.2: Improved regional outcomes in education and health**

***Strategy 4.2.1: Advocate for improved regional health services and infrastructure***

Related Plans	Strategy Alignment
<p>Regional Development Policy Framework 2011</p>	<p><b>Priority 3: Health</b>                      Outcomes:                      Health infrastructure and service delivery that reflects the needs of each region.                      Regional delivery of health services coordinated across all levels of government to provide universal access to healthcare for all regional residents.                      Regional patients to receive timely, high quality and continuous health care that meets their needs throughout their lives.                      Older regional residents to have access to health and community support services that promote independent and healthy living.                      Health professionals and allied health workers, who are attracted to live, work and remain in regional areas.</p>
<p>Regional Plan Kimberley 2011</p>	<p><b>Goal:</b> Improved delivery of health services in the region  <b>Strategies:</b>                      Involvement in Regional health committees including District Health Advisory Committee                      Better coordination of all health service providers                      Health precinct with buildings for community health and social care services to deliver services from specific structures and spaces to communities, shared by health and social care organisations  <b>Goal:</b> Ensure a physically and mentally healthy population in the Kimberley  <b>Strategies:</b>                      Improve community participation (i.e. volunteers, players, etc.)                      Improve and / or provide facilities  <b>Goal:</b> Reduce the need for remote community residents to travel to Broome for on-going health needs                      Advocate for the establishment of dialysis unit in the Central Kimberley                      Facilities like in Broome at BRAHMS need to also be out at Fitzroy Crossing and Halls Creek                      Greater use of remote diagnostics and telehealth – Improved telecommunications – Next G mobile and broadband coverage – NBN roll out – Improved and frequent telehealth facilities                      Support family support services (i.e.: Kununurra Neighbourhood House)                      Encourage childcare availability  <b>Goal:</b> Increase the number of private and public medical practitioners                      Influence government to introduce housing and benefit initiatives that encourages location to the region                      Mobile health clinics in 4x4 trucks                      Train regional nurses up to diagnose and prescribe without the need for doctors                      Strategies needed to keep people in the Kimberley and ensure the continuity of programs                      Support medical training in the region to add junior medical resource</p>

Related Plans	Strategy Alignment
	<p>More health professionals (mental health) outside the Government structure</p> <p>Youth (10 to 18) mental health through diversion programs involvement</p> <p>Address other needs i.e. childcare and education standards</p> <p>Encourage local aspiration and training to take on health profession qualifications (school and university, etc.)</p>
National Partnership Agreement on Closing the Gap in Indigenous Health Outcomes 2009	<p><b>Initiative:</b> Primary health care service that can deliver</p> <p>Introduce minimum service standards for all organisations providing primary health care services to Aboriginal and Torres Strait Islander populations.</p> <p>Ensure that primary health care services have the capacity to deliver the coordination and continuity of care necessary to meet the needs of Aboriginal and Torres Strait Islander clients.</p> <p>Expand allied health and acute care services to address the increased referrals for coordinated care by primary health care services.</p> <p>Review and refocus own purpose outlays in primary health care to prioritise core service provision and evidence-based regional priorities.</p>
Western Australian Strategic Plan for Safety and Quality in Health Care 2008-2013	<p><b>Objective C:</b> Expanded patient-centred health service planning and delivery.</p> <p><b>Strategies:</b></p> <p>Ensure consumers, carers and the community are well informed.</p> <p>Improve communication between patients and health care providers.</p> <p>Empower patients/carers/community to be part of health care planning and delivery.</p> <p>Ensure organisational structures and health care providers effectively integrate patients / carers / community in planning and delivering health care.</p> <p><b>Objective D:</b> Equitable access to safe, high quality health care.</p> <p><b>Strategies:</b></p> <p>Identify and apply elements and levers to improve access to health care for all sectors of the community.</p>
WA Health Operational Plan 2010-2011	<p><b>Key Goal 1:</b> Caring for People and the Community</p> <p><b>Priority 1.1:</b> Implement initiatives outlined in WA Strategic Plan for Safety and Quality in Health Care 2008-13</p>
WA Health Strategic Intent 2010-2015	<p><b>Making the best use of funds and resources</b></p> <p>Developing and rolling out a system of Activity Based Funding and Management for our health services and hospitals - providing a clearer link between the dollars we spend and the services we provide to patients and the community.</p> <p>Building and redeveloping hospitals and facilities to house first-class health services for Western Australians closer to where they live.</p>

**Strategy 4.2.2: Advocate for improved regional education and training opportunities**

Related Plans	Strategy Alignment
<p>Regional Development Policy Framework 2011</p>	<p><b>Priority 4:</b> Education</p> <p><b>Outcomes:</b></p> <p>The closure of the gap in education achievement and participation between regional and metropolitan students.</p> <p>Improved attendance and retention rates for primary and secondary schooling in regional and remote areas.</p> <p>Standards of achievement in literacy and numeracy for regional students that match national and state averages.</p> <p>Teachers to be attracted to take up and remain in teaching posts in regional areas.</p> <p>A broad range of educational options to be available and accessible to students in regional areas.</p>
<p>Regional Plan Kimberley 2011</p>	<p><b>Goal:</b> Offer relevant and accessible training that is linked to employment</p> <p><b>Strategies:</b></p> <p>Fund Work Readiness programs</p> <p>Introduce case management processes</p> <p>Develop school based trade training, supported by an Industry Trade Training Centre</p> <p>Encourage adult night training for lifelong learning (could be non-vocational)</p> <p>Improve business training services and introduce professional development opportunities</p> <p><b>Goal:</b> Develop mechanisms that better communicate educational offerings, to break down barriers to access</p> <p><b>Strategies:</b></p> <p>Establish linkages with Universities, TAFE and other Kimberley based educational institutions</p> <p>Develop an interactive RDA website to link with these institutions</p> <p>Engage Aboriginal Workforce Development Centre to assist</p> <p><b>Goal:</b> Introduce a more equitable training rebate cost system</p> <p><b>Strategies:</b></p> <p>Make decision and policy makers aware that training rebates are based on Perth costs and need to be reconsidered in light of regional costs</p> <p>Equalise opportunities between country and city (flights to Perth equivalence)</p> <p><b>Goal:</b> Lift educational standards and curriculum diversity in the region</p> <p><b>Strategies:</b></p> <p>Resume discussion through forums such as the Kimberley Economic Forum and ICC's Education Steering Committee</p> <p>Improve the ability of local schools to provide and meet local needs</p> <p>Develop a private school option, that is located on the campus of an educational institution and offers an Outdoor Education focus</p> <p>Develop an Agricultural College for the East Kimberley</p> <p>Encourage education providers to set up shop in Kimberley communities, e.g.: up the Dampier Peninsula to "help them grow their own"</p> <p><b>Goal:</b> To develop mechanisms that address the issues of remoteness through shared</p>

	<p>knowledge and partnerships</p> <p>Develop partnership with Desert Knowledge</p> <p>Develop partnerships and alliances with Indigenous organisations to increase employment</p> <p>Provide access to quality teachers and trainers</p> <p>Provide access for families to early childhood support and education in order for them to access employment and training</p> <p>Enhance training opportunities (flexible ones)</p>
National Education Agreement 2009	<p><b>Objectives:</b></p> <p>(a) all children are engaged in and benefiting from schooling;</p> <p>(b) young people are meeting basic literacy and numeracy standards, and overall levels of literacy and numeracy achievement are improving;</p> <p>(c) Australian students excel by international standards;</p> <p>(d) schooling promotes the social inclusion and reduces the educational disadvantage of children, especially Indigenous children; and</p> <p>(e) young people make a successful transition from school to work and further study.</p>

**Outcome 4.3: Best practice in community services**

***Strategy 4.3.1: Collaborate with State Government and other agencies to ensure co-ordinated provision of services***

Related Plans	Strategy Alignment
Shire of Broome Strategic Plan 2011-2016	<p><b>Strategy 1:</b> Work with agencies and others to ensure affordable and equitable services and infrastructure.</p> <p><b>Outcome 2:</b> Actively participate in the Kimberley Zone of Local Government and the RLG</p> <p><b>Action:</b> Work with State Government agencies to enable co-ordinated provision of services</p>

**Outcome 4.4: Advocate for an improved range and standard of municipal services to Aboriginal communities**

***Strategy 4.4.1: Liaise with stakeholder organisations to determine the future of municipal service delivery to Aboriginal communities***

Related Plans	Strategy Alignment
<p>A Capacity Building Strategy for Rural-Remote and Indigenous Local Government 2011</p>	<p><b>Goals:</b></p> <p>Role and Expectations of Rural-Remote and Indigenous Local Government                      Local Government Service Delivery to Remote Indigenous Communities</p> <p><b>Outcomes:</b></p> <p>A shared understanding of the role local government can realistically play in delivering local government and essential services to remote Indigenous communities.                      An improved understanding and appreciation of the full cost of providing services to remote Indigenous communities.                      A coordinated inter-government approach to service delivery in remote Indigenous communities.</p> <p><b>Strategies:</b></p> <p>Research the capacity of local government to deliver core municipal and other non-core services to remote Indigenous communities. This would involve:                      An overview of current challenges to service delivery to remote Indigenous communities and likely future funding scenarios                      An assessment of who is available to deliver noncore local government services and at what cost                      Analysis of alternative and innovative service delivery models (e.g. unincorporated areas of NSW, SA Lands Trust, State Government institutions, large Indigenous NGOs) – how do these work, what are their strengths/benefits and weaknesses compared to a system of local government                      Reviewing the progress of the NT reform of local government and continuing challenges                      International comparisons – particularly Canada                      Examining the feasibility of an inter-governmental approach to remote community service delivery (including joint planning, shared service delivery contributions etc.)                      An examination of opportunities to extend resource sector partnerships with Indigenous communities to provide support for the establishment of Indigenous enterprises that can deliver local government services to remote Indigenous communities on an individual or regional basis.</p>

**Goal 5: A sustainable and diverse economy**

**Outcome 5.1: Generational advantage that captures the wealth for the region**

***Strategy 5.1.1: Support regional partnerships with both private and public sector for future investment in the region***

Related Plans	Strategy Alignment
KDC Strategic Plan 2009	<p><b>Service 1:</b> The Commission will provide effective regional development policies and strategies together with accurate and accessible information on the region to facilitate and support the economic and social development of the region.</p> <p><b>Strategy:</b> Strategic Project Development, Support and Coordination - The Commission will facilitate and coordinate support for strategic regional economic development projects using a whole of government approach to ensure optimum allocation and leverage of resources.</p> <p><b>Service 2:</b> The Commission will identify, co-ordinate and promote through its major strategies the establishment, improvement and diversification of the region’s infrastructure, industries and enterprises.</p> <p><b>Strategy:</b> Development Processes, Partnerships and Government Policies - The Commission will establish effective partnerships with key stakeholders to facilitate the achievement of strategic objectives and will monitor and contribute to formal development processes to advance and assist development outcomes.</p>

**Outcome 5.2: Improved outcomes in employment**

***Strategy 5.2.1: Promote and advocate at the state and national level our region’s readiness for investment opportunities***

Related Plans	Strategy Alignment
KDC Strategic Plan 2009	<p><b>Service 1:</b> Strategic Project Development, Support and Coordination - The Commission will facilitate and coordinate support for strategic regional economic development projects using a whole of government approach to ensure optimum allocation and leverage of resources.</p>

**Strategy 5.2.2: Support and advocate regional employment programs and opportunities**

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<p><b>Goal:</b> Offer relevant and accessible training that is linked to employment</p> <p><b>Strategies:</b>                      Fund Work Readiness programs                      Develop school based trade training, supported by an Industry Trade Training Centre                      Encourage adult night training for lifelong learning (could be non-vocational)                      Improve business training services and introduce professional development opportunities</p> <p><b>Goal:</b> Increase level of participation in the economy</p> <p><b>Strategies:</b>                      More support for workplaces to host, support and train lowly skilled people: – employer readiness programs – alternative education programs</p> <p><b>Goal:</b> Demonstrate the economic impacts of the inadequate supply of housing and slow land release strategies</p> <p><b>Strategies:</b>                      Workers camp accommodation – Make a proper town                      Lobby/advocate for affordable housing and viability of ownership by low income earners to alleviate pressure on State housing</p> <p><b>Goal:</b> Increase the pool of available housing for health care workers</p> <p><b>Strategies:</b>                      Promote discussion for the urgent need to increase housing for health care workers                      Target more remote towns (Fitzroy Crossing and Halls Creek)</p>
Kimberley Tafe Strategic Plan 2010-2012	<p><b>Major Strategy:</b> A skilled workforce</p> <p><b>Strategic Objective:</b> Support industry in the Kimberley region to improve and develop employment opportunities</p>

**Outcome 5.3: A highly valued Kimberley product through the development of a sustainable market and tourism experience supported by the region’s assets (human, environment and built)**

**Strategy 5.3.1: Promote tourism opportunities at a regional level**

Related Plans	Strategy Alignment
Regional Plan Kimberley 2011	<p><b>Goal:</b> Increase level of participation in the economy</p> <p><b>Strategies:</b>                      Ensure links between RDA and tourism                      Support Indigenous tourism at local and regional level (especially in the East Kimberley)                      Support tourism businesses to train and improve infrastructure</p> <p><b>Goal:</b> Provide more cultural focus in tourism in the region</p> <p><b>Strategies:</b>                      Add value to current programmes through membership of tourism associations and contribution to industry forums                      Promote current programmes through the RDA website                      Build capacity of Traditional Owners to become major tourism owners and operators, e.g.: resorts, etc.                      Fund key infrastructure                      Marketing – target international, national and regional                      Enhance facilities i.e. roadside stops</p>
Tourism Western Australia Strategic Plan 2010-2011	<p><b>Strategic Objectives:</b>                      Deliver innovative marketing to make Western Australia famous for its extraordinary experiences and convert consumer preference to visitation.                      Develop, attract and market events that fortify Western Australia’s brand, deliver visitors to the State, activate infrastructure, add social vibrancy and encourage community participation.                      Facilitate tourism infrastructure development to provide access to, and accommodation around, Western Australia’s extraordinary tourism experiences.</p>
Australia's North West Tourism Development Priorities 2010-2015	<p><b>Priorities:</b>                      Establish the Kimberley as a key aviation gateway                      Facilitate the development of accommodation in national and conservation parks and on pastoral stations                      Increase caravan park capacity across the Kimberley region                      Improve the reliability and safety of access to key natural attractions accessible via the Great Northern Highway                      Facilitate the development of indigenous product and interpretation in the region                      Improve and expand visitor amenities along major touring routes                      Improve boating facilities in the Kimberley region                      Facilitate the development of convention/conference facilities in the region                      Ensure tourism related development is incorporated in the rejuvenation of precincts and</p>

Related Plans	Strategy Alignment
	townships Seal key coastal access roads around Broome

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## 12.6 ELECTED MEMBER REPORTS

### Cr John Moulden

26 July 2012	T24 11/12 Gravel Re-sheeting Gibb River-Kalumburu Road Tender Opening
31 July 2012	Kimberley Zone Meeting, Perth
1-4 August 2012	Local Government Week, Perth
7 August 2012	Opening Visitor Centre Information Bay
7 August 2012	Tourism WA's key initiatives for 2012-2013 - Tourism WA Chairman, Mr Peter Prendiville, CEO, Ms Stephanie Buckland and Director Projects, Ms Renata Lowe
15 August 2012	Standing Committee on Public Administration, Legislative Council, Parliament of Western Australia – Video Conference
15 August 2012	Clontarf Northern Carnival – Awards Presentation
20 August 2012	Landcorp Regional Meeting - Monty House (Chair), David Rowe (Deputy), Peter Cooke, David Rowe, Jenny Smithson, Simon Read, George McCullagh (Directors) Ross Holt (CEO), Hilary Wilkins (Regional Manager)
21 August 2012	Audit (Risk and Finance) Committee Meeting

### Cr Ralph Addis

31 July 2012	Kimberley Zone Meeting, Perth
7 August 2012	Tourism WA's key initiatives for 2012-2013 - Tourism WA Chairman, Mr Peter Prendiville, CEO, Ms Stephanie Buckland and Director Projects, Ms Renata Lowe
21 August 2012	Audit (Risk and Finance) Committee Meeting

### Cr Di Ausburn

7 August 2012	Tourism WA's key initiatives for 2012-2013 - Tourism WA Chairman, Mr Peter Prendiville, CEO, Ms Stephanie Buckland and Director Projects, Ms Renata Lowe
21 August 2012	Audit (Risk and Finance) Committee Meeting

### Cr Jane Parker

31 July 2012	Kimberley Zone Meeting, Perth
1-4 August 2012	Local Government Week, Perth
7 August 2012	Tourism WA's key initiatives for 2012-2013 - Tourism WA Chairman, Mr Peter Prendiville, CEO, Ms Stephanie Buckland and Director Projects, Ms Renata Lowe
21 August 2012	Audit (Risk and Finance) Committee Meeting

**Cr Cissy Gore Birch-Gault**

31 July 2012 Kimberley Zone Meeting, Perth

1-4 August 2012 Local Government Week, Perth

**Cr Jackie McCoy**

1-4 August 2012 Local Government Week, Perth

7 August 2012 Tourism WA's key initiatives for 2012-2013 - Tourism WA Chairman, Mr Peter Prendiville, CEO, Ms Stephanie Buckland and Director Projects, Ms Renata Lowe

**COUNCIL DECISION**

**Moved: Cr D Ausburn  
Seconded: Cr R Addis**

**That Council notes the Elected Members report from 26 July – 21 August 2012.**

**Carried Unanimously 6/0**

## 12.7 CHIEF EXECUTIVE OFFICER REPORTS

The Chief Executive Officer attended the following meetings on behalf of Council:

25 July 2012	Department of Housing, Louise Paterson
26 July 2012	Petrol Sniffing Strategy Evaluation, FaHCSIA
27 July 2012	Ord Expansion Update, Brett Chivers, Benchmark Projects
31 July 2012	Kimberley Zone Meeting, Perth
1-4 August 2012	Local Government Week, Perth
7 August 2012	Opening Visitor Centre Information Bay
7 August 2012	Tourism WA's key initiatives for 2012-2013 - Tourism WA Chairman, Mr Peter Prendiville, CEO, Ms Stephanie Buckland and Director Projects, Ms Renata Lowe
9 August 2012	Kimberley Regional Planning Committee, Department of Planning Presentation – After EKDP
10 August 2012	Ord Valley Muster Board Meeting
15 August 2012	Clontarf Northern Carnival – Awards Presentation
15 August 2012	Department of Housing, Louise Paterson
20 August 2012	Landcorp Regional Meeting - Monty House (Chair), David Rowe (Deputy), Peter Cooke, David Rowe, Jenny Smithson, Simon Read, George McCullagh (Directors) Ross Holt (CEO), Hilary Wilkins (Regional Manager)
21 August 2012	Audit (Risk and Finance) Committee Meeting

### **COUNCIL DECISION**

**Moved: Cr R Addis**

**Seconded: Cr D Ausburn**

**That Council notes the Chief Executive Officer report from 26 July – 21 August 2012.**

**Carried Unanimously 6/0**

**13. MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN**

Nil

**14. QUESTIONS BY MEMBERS OF WHICH DUE NOTICE HAS BEEN GIVEN**

Nil

**COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr R Dessert**

**That Council accepts matters of urgent business.**

**Carried unanimously 6/0**

## 15. URGENT BUSINESS APPROVED BY THE PERSON PRESIDING OR BY DECISION

### 15.1.1 Proposed Residential Unit Development – Lot 10 Riverfig Avenue, Kununurra

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Hodge Collard Preston Architects
<b>LOCATION:</b>	Lot 10 Riverfig Avenue, Kununurra
<b>AUTHOR:</b>	Jennifer Ninyette, Senior Planning Officer
<b>REPORTING OFFICER:</b>	Nick Kearns, Director Development Services
<b>FILE NO:</b>	A5815, A5816P, A5817P, A5818P, A5819P, A5820P

#### **PURPOSE**

For Council to consider an application for planning consent for a residential unit development on Lot 10 Riverfig Avenue, Kununurra.

#### **BACKGROUND**

Lot 10 Riverfig Avenue is located on the southern intersection of Riverfig Avenue opposite White Gum Park, and has street frontages to both Riverfig Avenue and the Messmate Way service road.

Lot 10 has an area of 2173m<sup>2</sup>, and is surrounded by single storey residential units to the south and mixed use development to the east. The Gateway shopping complex is located on the opposite corner. A location plan is provided at Attachment 1.

The development proposed consists of a two storey residential block housing 20 single bedroom units (10 on the each floor) fronting Messmate Way, and another two storey residential block housing 6 single bedroom units (3 on each floor) fronting Riverfig Avenue. In total 26 residential single bedroom units are proposed.

A copy of the proposed site plan is shown at Attachment 1.

This site was previously utilised for residential use as part of Pandanus Court with 6 single bedroom grouped dwellings established on the lot. The original structures are still on site and will need to be demolished to enable the proposed redevelopment of the site.

#### **Development Proposed**

There are a total of 26 single bedroom units proposed. Each unit has a floor area of 55.48m<sup>2</sup> which comprises a bedroom, combined kitchen, dining and living area, combined bathroom and laundry, balcony and store room.

The site plan reflects 28 car parking bays on site and 5 visitor bays within Council's road reserve, and driveway access to the property via Riverfig Avenue.

Floor plans and elevations of the building are provided at Attachment 3.

## **STATUTORY IMPLICATIONS**

### **Town Planning Scheme No. 7**

The subject site is zoned 'Town Centre' under the Shire of Wyndham - East Kimberley Town Planning Scheme No.7 (Scheme), for which the use and development of residential units is permitted.

Clause 5.10.4 of the Scheme states that group residential development within the 'Town Centre' zone may be permitted in accordance with the provisions and standards of the R50 density code outlined in the Residential Design Codes. This is providing the development is compatible with the surrounding land use.

The Residential Design Codes are a policy prepared and adopted under the Planning and Development Act. Clause 5.7 of the Town Planning Scheme adopts the codes. As they are adopted under the Scheme, the Codes take on the effect of Scheme provisions.

Parking standards for residential purposes is guided by the Residential Design Codes.

The Scheme also provides under Clause 8.4.2 that cash in lieu payment shall not be less than the estimated cost to the owner or developer of providing and constructing the parking spaces required by the Scheme, plus the value of the area of the land which would have been occupied by the parking spaces.

The application has been submitted with the intent that a number of visitor parking bays will be provided within Council's road reserve, with the site plan showing 5 parking bays off site.

As, such Council's consideration is required as to whether they will allow the developer to construct bays within the road reserve in lieu of providing all visitor parking on-site.

### **Residential Design Codes**

The Residential Design Codes of Western Australia (R Codes) provides a comprehensive basis for the control, through local government, of residential development throughout Western Australia.

Under the R Codes, the proposed units are classified as multiple dwellings and are assessed as such below.

#### **ONE BEDROOM UNITS - MULTIPLE DWELLINGS**

For multiple dwellings in areas of higher density areas (i.e. greater than R30) the R codes outline that development of the building should be at a bulk and scale consistent with the existing or future desired built form of the locality.

The acceptable development provisions illustrate one way of meeting the performance criteria, which for a density coding of R50, is for the development to comply with a maximum plot ratio of 0.6.

Officers calculate the plot ration for the multiple dwellings to be (approx.) 0.66, however, the R Codes do provide for flexibility if Council consider that the proposed density and building bulk is a desirable outcome.

As such building height should be consistent with the desired height of buildings in the locality, and to recognise the need to protect the amenities of adjoining properties. The

proposed two storey is considered to be consistent with the desired height of buildings within the Town Centre, and is in keeping with the adjacent development currently in progress on adjoining Lot 11 (Riverfig Avenue) and a two storey grouped dwelling development located to the south on Messmate Way.

## SETBACKS

The units have been orientated to maximise street frontages for balconies to provide for surveillance of Messmate Way and Riverfig Avenue, and are proposed to be constructed close (a minimum of 1.5m) to the front boundaries, as may be permitted in the Town Centre zone. This also allows the residential parking to be reasonably concealed from the street, and therefore will provide a desirable streetscape.

In relation to the side and rear boundaries, the required setbacks are generally calculated based on the height of the building, the length of the wall and the location of windows. Based on this the recommended minimum boundary setback for the units are as follows:

	Recommended Setback	Proposed Setback
Units along Messmate Way (side)	2m	2m
Units along Riverfig Ave (side)	2m	1.5m

It is acknowledged that the owner of Lot 10 also owns adjoining Lot 11, currently being redeveloped, and due to the an existing 3m wide easement located between the two development there is considered to be minimal impact of the proposed setbacks on this neighbouring property.

High level windows and obscure glass are intended for windows facing the side boundaries to negate any overlooking issues, along with the provision of screening to stairwells and access ways to ensure compliance with the privacy provisions of the R Codes.

## BALCONIES

The balconies proposed are shown to be 5m<sup>2</sup> (2.5m x 2m). The R Codes states that balconies should as a minimum be 10m<sup>2</sup> or of a sufficient size to serve as a functional space, i.e. be wide enough to accommodate outdoor seating/furniture to encourage activity and use. As such, it is considered that the proposed balconies have measurements that are adequate to accommodate a small table and chairs, particularly as not more than 2 people will occupy the unit.

## PARKING

Parking requirements for multiple dwellings are based on the size of the dwelling. As a minimum, on-site parking provision for small units (under 75m<sup>2</sup> or 1 bedroom) is 1 parking space per dwelling, with 0.25 visitor bays to be provided per dwelling, to be rounded up to the nearest whole number. The R Codes also stipulate the number of bicycle spaces required to be provided as 1 per each 3 dwellings for residents and 1 bicycle space per each 10 dwellings for visitors.

As such, 33 parking spaces (26 for residents, 7 visitor bays) and 10 bicycle spaces are required to be provided as a minimum.

The application proposes to provide 28 bays on-site, 26 for residents and 2 visitor bays, with the remaining 5 visitor bays to be provided within Council's road reserve.

It is noted that a bicycle store area has been provided on the submitted plans.

## BUILDING DESIGN

Part 7.4.3 of the R Codes outlines performance criteria that each dwelling within the development is of a sufficient size to cater for the needs of the residents, and that development must provide diversity in dwellings to ensure that a range of types and sizes is provided.

This proposal is seeking only to provide single bedroom units. The applicant's planning consultant (Whelans) has provided written justification (Attachment 4) for this to be supported on the basis that:

1. It is considered that the size of the units is sufficient to meet the needs of the residents which meet the performance criteria of the R Codes;
2. It is appropriate to have density in the town centre and single bed units are more appropriately located close to town and its amenities;
3. It will contribute to needed housing stock to cater for transient residents of Kununurra seeking to live in town without having to purchase a larger dwelling;
4. It creates an opportunity to create affordability for first home owners;
5. Creates more opportunity for residents to transition from other less formal forms of accommodation, such as caravans and 'dongas';
6. Although not providing diversity within the development itself, it will provide diversity within the Town Centre.

## ESSENTIAL FACILITIES

There are a number of essential facilities that are required to be provided for multiple dwellings which includes:

- a communal area set aside for clothes drying; and
- the provision of an enclosed, lockable storage area, accessible from outside the dwelling, with the acceptable development providing a minimum dimension of 1.5m and an internal area of at least 4m<sup>2</sup> for each multiple dwelling; and
- Adequate rubbish storage and collection areas.

There is not a communal area set aside for clothes drying, instead it is proposed that each unit will be provided with internal clothes dryers as stated in correspondence from the developers planning consultant at Attachment 4.

Each unit has been provided with an enclosed lockable 2.4m<sup>2</sup> store accessible from both the living area and the balcony. Again, Whelans in their submission (Attachment 4) have stated that although the storage areas are smaller than the acceptable development provision, the stores meet the performance criteria because they are adequate for the needs of the residents given that no more than two people can occupy the residence, and there is no need to store large garden implements of the like.

Two proposed communal bin storage areas has been provided, however there is no area identified for bin collection, which is recommended to occur onsite. Alternatively a suitable hardstand area for bin collection should be provided

## STAIRS

The second storey units are served by stairs only, in which case the Residential Design Codes stipulate that access above ground level to all multiple dwelling should be totally

protected from weather. There are four sets of stairs (two to each block) proposed to service these units, with all sets only partially covered. It is recommended that at least one set of stairs to each block should be wholly covered.

## **LANDSCAPING**

The site plan indicates minimal landscaping shown onsite, with the most of the open space being utilised for parking and access, bin storage, pedestrian access and therefore being sealed. Instead the majority of landscaping is also proposed within the property frontage, and within the road reserve as shown on the site plan.

The R Codes outline that the space around buildings is designed to allow for planting, and landscaping of the site should be undertaken with appropriate planting, paving and other landscaping that:

- Meets the projected needs of the residents;
- Enhances security and safety for residents; and
- Contributes to the streetscape.

As such it is recommended that landscaping and reticulation be installed at the cost of the developer within Council's road reserve and adjacent to the property boundary.

The R Codes allow for a reasonable amount of flexibility in relation to development of multiple dwellings, and ultimately the onus is on Council to consider if the proposal is in line with the future desired built form for Kununurra.

## **POLICY IMPLICATIONS**

There are no policy implications associated with this item.

## **FINANCIAL IMPLICATIONS**

The application fee of \$9830 has been paid.

The cost of construction of the proposed parallel and visitor parking bays, footpath and any landscaping within Council's road reserve will be borne by the developer.

## **STRATEGIC IMPLICATIONS**

The Local Planning Strategy (LPS) identifies the need to improve the quality and quantity of housing choices for residents, and improve the urban design of Kununurra.

The Kununurra Strategic Directions report identifies the site as potentially suitable for tourist accommodation, and it is acknowledged that the proposed units are suitable for short term accommodation and/or seasonal workers.

## **COMMUNITY CONSULTATION**

No community consultation is required in relation to the proposed development.

## **COMMENT**

The developer is seeking Council's consideration to allow visitor parking to be constructed within Council's road reserve.

Shire Officers consider that dispensation for some required parking to be provided within the Messmate Road and Riverfig Avenue road reserve could be supported, subject to all parking for residential tenants being onsite and the Shire being satisfied in regards to access for waste and utility areas onsite, as redevelopment of the site would achieve a more desirable outcome for the town centre.

Development of visitor parking within the road reserve will not impede other surrounding use or development, and is consistent with a number of recently approved developments.

From an aesthetics point of view, the proposed development is considered to provide an attractive streetscape, with articulation of the building to both streets. Passive surveillance is also provided from the balconies of the units over the road reserve and to the Messmate Way nature strip.

## **ATTACHMENTS**

### **Attachment 1 – Location Plan**

Attachment 2 – Site Plan

Attachment 3 – Floor Plan and Elevations

Attachment 4 – Letter from Planning Consultant (Whelans)

## **VOTING REQUIREMENT**

Simple Majority

## **OFFICER'S RECOMMENDATION**

That Council grant planning consent for the development of Multiple Dwellings on Lot 10 Riverfig Avenue, Kununurra, subject to the following conditions:

1. All development shall be in accordance with the attached approved plan(s) dated 21 August 2012 and subject to any modifications required as a consequence of any condition(s) of this approval. The endorsed plans shall not be modified or altered without the prior written approval of the local government.
2. A landscaping plan must be prepared and include the following detail, to the satisfaction of the Shire:
  - a. The location, number and type of existing and proposed trees and shrubs, including calculations for the landscaping area;
  - b. Any lawns to be established;
  - c. Any natural landscape areas to be retained;
  - d. Those areas to be reticulated or irrigated; and
  - e. Verge treatments.

The landscaping must be completed prior to occupation of the development, and must be maintained at all times to the satisfaction of the Shire.

3. The street setback area and all verge areas must be landscaped and reticulated, prior to the occupation of the development and must be maintained at all times.
4. All parking, driveway and access areas must comply with the below requirements for the duration of the development:

- a. Be designed in accordance with Australia/New Zealand Standard AS/NZS 2890.1:2004, Parking Facilities, Part 1: Off-Street Car Parking unless otherwise specified by this approval;
  - b. Be constructed, sealed, kerbed, drained and marked prior to the development being occupied and maintained thereafter;
  - c. Have lighting installed, prior to the occupation of the development; and
  - d. Confine all illumination on the land in accordance with the requirements of Australian Standards AS 4282-1997, Control of the Obtrusive effects of Outdoor Lighting at all times.
5. Stairwells adjacent to boundaries shall be adequately screened with slatted or solid screening to a height of 1.6 metres, to the satisfaction of the local government.
  6. At least one stairwell to access each second storey shall be totally protected from weather.
  7. All side and rear fencing shall consist of solid panel type construction to a maximum height of 1.8 metres. All fences and gates to front setbacks and front boundaries shall be a minimum of 50% transparent above 1.2 metres to a height of 1.8 metres.
  8. Bin storage area(s) must be designed with a size suitable to service the development and screened from view of the street, to the satisfaction of the Shire.
  9. The bin hardstand in-situ concrete pad area constructed on the street verge area to accommodate the required number of bins to be placed for collection day. The location of the in-situ concrete pad on the street verge subject to the written approval of the Shire, and shall be capable of storing a minimum of 26 standard rubbish bins for collection.
  10. Each unit is to be provided with adequate clothes washing and drying facilities in accordance with Health Act (Laundries and Bathrooms) Regulations.
  11. Lighting and ventilation for all water closets shall be in accordance with Sewerage (Light, Ventilation & Construction) Regulations 1971.
  12. All plumbing pipes shall be ducted within the walls of the building.
  13. Roof downpipes must not openly discharge onto the ground or paved surface. All downpipes must discharge into a suitable piped stormwater drainage system.
  14. Provision of a minimum of 10 bicycle parking spaces to be provided onsite.
  15. No site earthworks or development shall occur that will cause additional runoff of stormwater to adjacent properties.
  16. All stormwater drainage must be contained and disposed of on-site at all times, to the satisfaction of the local government.
  17. Earthworks and batters must be stabilised to prevent sand blowing and dust nuisance, for the duration of the development.
  18. A Dust Management Plan must be prepared in accordance with the Department of Environment and Conservation's Draft A guideline for the Development and Implementation of a Dust Management Program to the satisfaction of the Shire, prior to applying for a Building Permit – Certified.

19. The existing crossover must be removed and the verge, footpath, kerbing and landscaping must be reinstated to the satisfaction of the Shire, prior to the occupation of the development.
20. All works in the road reserve, including construction of a crossover or footpath, installation of on-street car parking spaces, planting of street trees, bicycle parking devices, street furniture and other streetscape works and works to the road carriageway must be to the specification of the Shire.

## **COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr D Ausburn**

**That Council grant planning consent for the development of Multiple Dwellings on Lot 10 Riverfig Avenue, Kununurra, subject to the following conditions:**

1. All development shall be in accordance with the attached approved plan(s) dated 21 August 2012 and subject to any modifications required as a consequence of any condition(s) of this approval. The endorsed plans shall not be modified or altered without the prior written approval of the local government.
2. A landscaping plan must be prepared and include the following detail, to the satisfaction of the Shire:
  - a. The location, number and type of existing and proposed trees and shrubs, including calculations for the landscaping area;
  - b. Any lawns to be established;
  - c. Any natural landscape areas to be retained;
  - d. Those areas to be reticulated or irrigated; and
  - e. Verge treatments.

The landscaping must be completed prior to occupation of the development, and must be maintained at all times to the satisfaction of the Shire.

3. The street setback area and all verge areas must be landscaped and reticulated, prior to the occupation of the development and must be maintained at all times.
4. All parking, driveway and access areas must comply with the below requirements for the duration of the development:
  - a. Be designed in accordance with Australia/New Zealand Standard AS/NZS 2890.1:2004, Parking Facilities, Part 1: Off-Street Car Parking unless otherwise specified by this approval;
  - b. Be constructed, sealed, kerbed, drained and marked prior to the development being occupied and maintained thereafter;
  - c. Have lighting installed, prior to the occupation of the development; and
  - d. Confine all illumination on the land in accordance with the requirements of Australian Standards AS 4282-1997, Control of the Obtrusive effects of Outdoor Lighting at all times.

5. Stairwells adjacent to boundaries shall be adequately screened with slatted or solid screening to a height of 1.6 metres, to the satisfaction of the local government.
6. At least one stairwell to access each second storey shall be totally protected from weather.
7. All side and rear fencing shall consist of solid panel type construction to a maximum height of 1.8 metres. All fences and gates to front setbacks and front boundaries shall be a minimum of 50% transparent above 1.2 metres to a height of 1.8 metres.
8. Bin storage area(s) must be designed with a size suitable to service the development and screened from view of the street, to the satisfaction of the Shire.
9. The bin hardstand in-situ concrete pad area constructed on the street verge area to accommodate the required number of bins to be placed for collection day. The location of the in-situ concrete pad on the street verge subject to the written approval of the Shire, and shall be capable of storing a minimum of 26 standard rubbish bins for collection.
10. Each unit is to be provided with adequate clothes washing and drying facilities in accordance with Health Act (Laundries and Bathrooms) Regulations.
11. Lighting and ventilation for all water closets shall be in accordance with Sewerage (Light, Ventilation & Construction) Regulations 1971.
12. All plumbing pipes shall be ducted within the walls of the building.
13. Roof downpipes must not openly discharge onto the ground or paved surface. All downpipes must discharge into a suitable piped stormwater drainage system.
14. Provision of a minimum of 10 bicycle parking spaces to be provided onsite.
15. No site earthworks or development shall occur that will cause additional runoff of stormwater to adjacent properties.
16. All stormwater drainage must be contained and disposed of on-site at all times, to the satisfaction of the local government.
17. Earthworks and batters must be stabilised to prevent sand blowing and dust nuisance, for the duration of the development.
18. A Dust Management Plan must be prepared in accordance with the Department of Environment and Conservation's Draft A guideline for the Development and Implementation of a Dust Management Program to the satisfaction of the Shire, prior to applying for a Building Permit – Certified.
19. The existing crossover must be removed and the verge, footpath, kerbing and landscaping must be reinstated to the satisfaction of the Shire, prior to the occupation of the development.

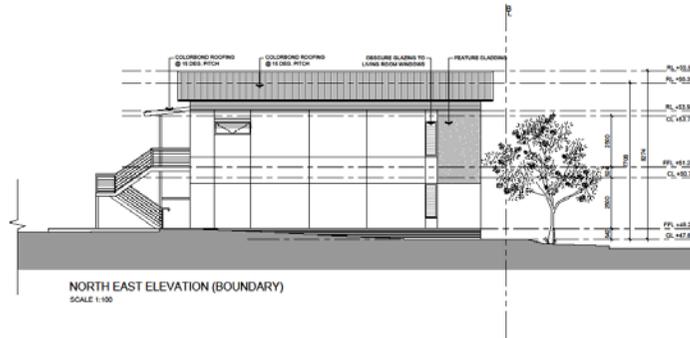
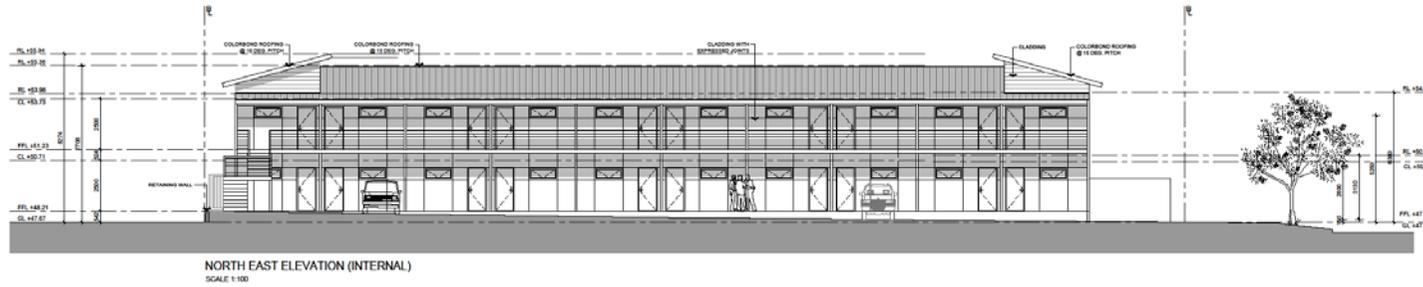
**20. All works in the road reserve, including construction of a crossover or footpath, installation of on-street car parking spaces, planting of street trees, bicycle parking devices, street furniture and other streetscape works and works to the road carriageway must be to the specification of the Shire.**

**Carried Unanimously 6/0**

**Attachment 1 – Location Plan**

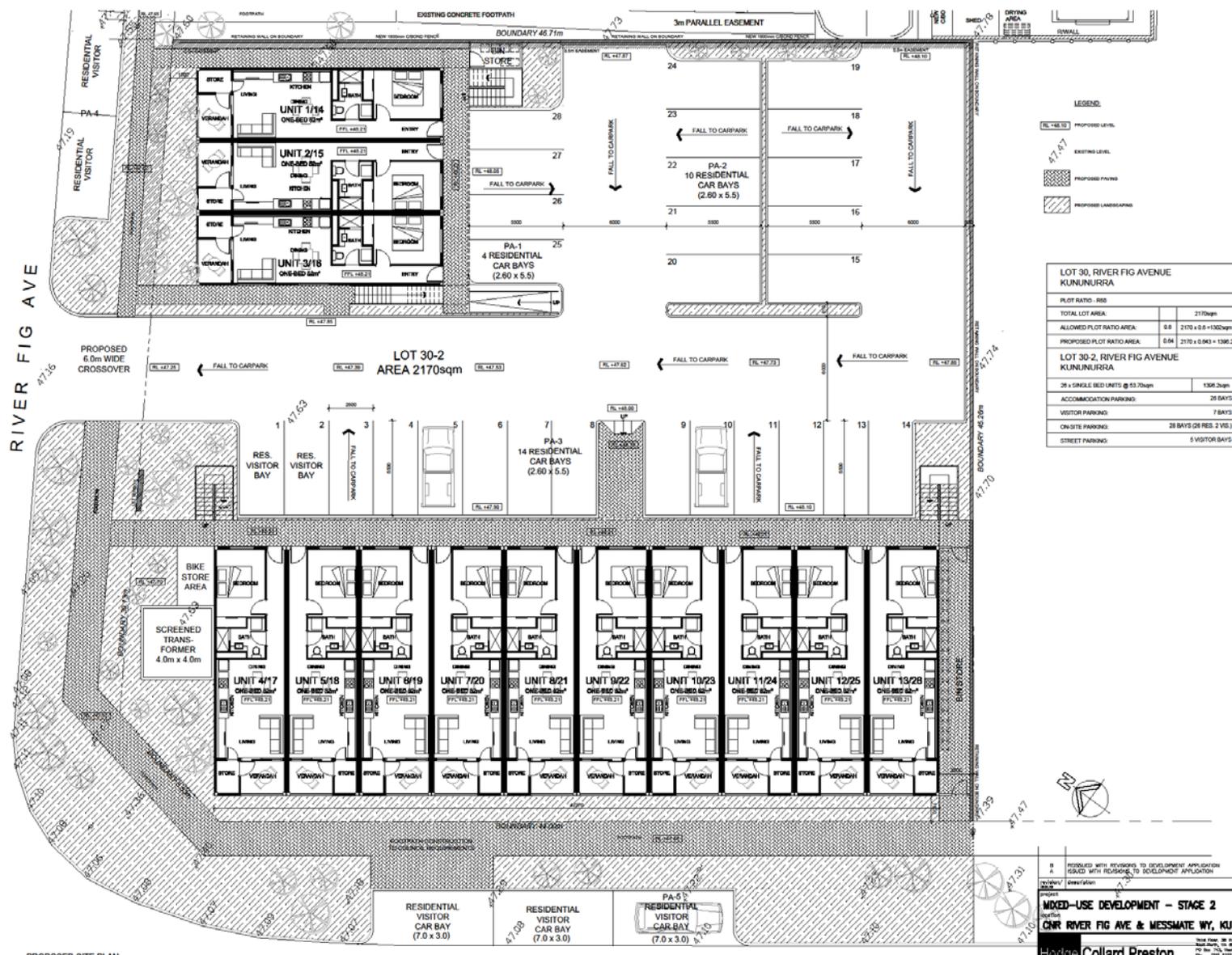


# Attachment 2 – Site Plan



A	ISSUED FOR DEVELOPMENT APPLICATION	LY	MP	17.06.2012
DATE	DESCRIPTION	DATE	CREATED	DATE
PROJECT	MIXED-USE DEVELOPMENT - STAGE 2	DRAWN	DATE	DESCRIPTION
LOCATION	CNR RIVER FIG AVE & MESSMATE WY, KUNUNURRA	CHECKED	DATE	DESCRIPTION
Hodge Collard Preston ARCHITECTS		DATE	DATE	SCALE
1:3000		DATE	DATE	SCALE
52.10		DATE	DATE	SCALE
SK40		DATE	DATE	SCALE

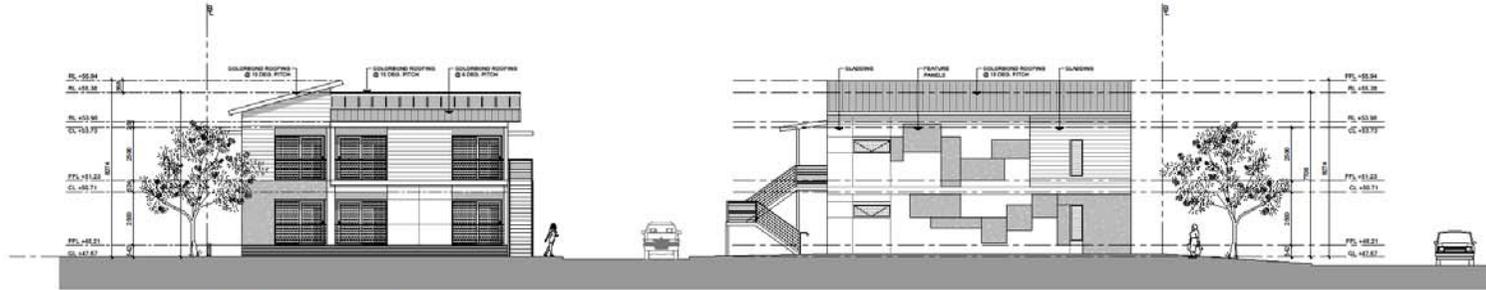




PROPOSED SITE PLAN

REVISED WITH REVISIONS TO DEVELOPMENT APPLICATION	LY	HP	11.08.2012
ISSUED WITH REVISIONS TO DEVELOPMENT APPLICATION	DM	HP	15.08.2012
DESCRIPTION	APPROVAL	DATE	
MIXED-USE DEVELOPMENT - STAGE 2	DM	PROPOSED	
CNR RIVER FIG AVE & MESSMATE WY, KUNUNURRA	HP	SITE PLAN	
DATE	15.08.2012	SCALE	1:100
PROJECT NO	52.10	DWG NO	SK38
Hodge Collard Preston			

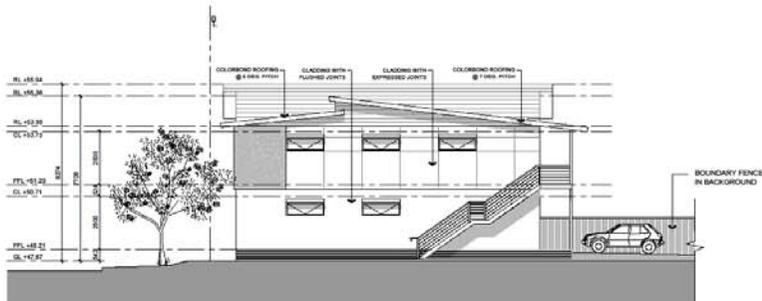
# Attachment 3 – Floor Plan and Elevations



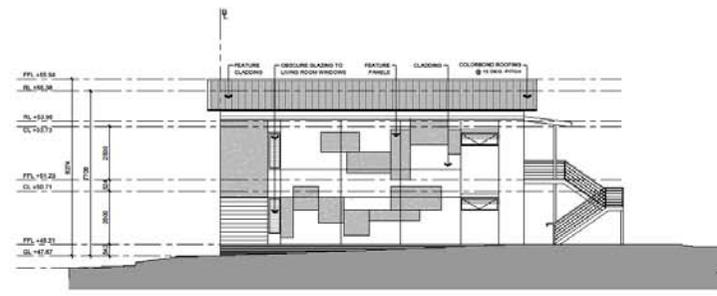
NORTH WEST ELEVATION (RIVER FIG AVENUE)  
SCALE 1:100



SOUTH WEST ELEVATION (MESSMATE WAY)  
SCALE 1:100



SOUTH WEST ELEVATION (INTERNAL)  
SCALE 1:100



SOUTH EAST ELEVATION (BOUNDARY)  
SCALE 1:100

A	REVISED FOR DEVELOPMENT APPLICATION	LT	SP	17.08.2013
REVISED FOR DEVELOPMENT APPLICATION		SP	SP	SP
Project		Drawn	Illustration	

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## Attachment 4 – Letter from Planning Consultant (Whelans)



Our ref: 13348-18 : KW  
15 August 2012

Shire of Wyndham East Kimberley  
PO Box 614  
KUNUNURRA 6743

Attn: Nick Kearns

Via email: [nick.kearns@swek.wa.gov.au](mailto:nick.kearns@swek.wa.gov.au)

Dear Nick,

### PROPOSED MIXED USED DEVELOPMENT CORNER MESSMATE WAY AND RIVERFIG AVENUE KUNUNURRA

We provide the following advice regarding proposed development of Stage II at the above described site. This assessment has been undertaken on behalf of the landowner, and it is understood this advice will be submitted with the Development Application prepared by Hodge Collard Preston architects in support of the application.

This assessment uses areas and dimensions as noted on the supplied plans. These plans are revised plans, with modifications made as a result of discussion with council officers.

#### URBAN DESIGN AND ARCHITECTURE

The proposed development will make substantial contribution to the aesthetics and function of the town centre of Kununurra. Significantly, the design features buildings addressing the street with all off street parking located at the rear of the development behind buildings.

The development will contribute to the streetscape of the locality, and has been designed to provide interest to the street through the use of variety of land uses to create activity as well as architecturally attractive design that is articulated and scaled appropriately.

The design features articulation in elevations such as awnings and sunshades, balconies, panels and varied colours and materials.



The proposed development will also contribute to activity of the town centre by creating a mix of land uses that encourages use throughout the day and night, ensuring that the development contributes to a vibrant town centre.

Passive Surveillance is an important component of the 'Designing Out Crime' urban design principles, and all buildings have windows and balconies overlooking public spaces.

The development will provide passive surveillance to the road network, with residential and commercial land uses overlooking Riverfig Avenue and Messmate Way, including the landscaped area of this major entry road.

#### **PLANNING FRAMEWORK**

The subject site is zoned Town Centre under TPS 7. Under this Scheme, development of Residential Development is permitted to the R50 Density Code provisions of the Residential Design Codes [the Codes]. This assessment has been undertaken in accordance with these provisions. Given the development constitutes a multiple dwelling development, the Multiple dwelling provisions of the Codes have been used for the assessment.

#### **SETBACKS**

Setbacks comply with the requirements of the Scheme and the Codes. Screening to the stairwells and access ways for units 1, 4, 5 and 13 [and upper storey units] has been provided to ensure compliance with the privacy provisions of the Codes.

#### **CAR PARKING**

Car parking for Residential use complies with the requirements of the Design Codes, with one bay provided for each single bedroom dwelling, with 26 bays provided on site for the 26 units. 2 visitor bays are provided on site, and five visitor bays are provided in the immediate Messmate Way road reserve.

#### **PLOT RATIO**

Proposed plot ratio complies with the requirements of the Design Codes. Under the Design Codes, plot ratio must be 0.6 [1302 square metres]. Proposed plot ratio is slightly over 0.6 at 1396 square metres. This represents a minor variation of less than 10% [7.2%].

#### **STORAGE**

Under the provisions of the Design Codes, an external storage unit with an area of 4 square metres is required to be provided.

All units are provided with an external storage unit that is 2.4 metres in size, however, it is anticipated that the development can be approved with this reduction, because they are single bedroom multiple dwellings with no courtyard space. This means that storage requirements are reduced as a maximum of 2 people can occupy the dwellings, and there is no need for storage of garden implements such as mowers, whipper-snippers etc.

Additionally, no reduction is required for storage of materials due to cyclones, given that Kununurra is outside of cyclonic building regulations.

A group bicycle store room is also being provided.

No allocation has been made for drying areas, as all units will contain a clothes washer and dryer.

#### HOUSING DENSITY

It is appropriate to have density in the town centre. Single bed units are more appropriately located close to town where residents can utilize the towns amenities. Town Centre is meant to provide a diverse mix of uses, and additional residential development in the Town Centre creates better use of existing infrastructure, while also providing a benefit for security through passive surveillance.

One Bedroom dwellings will create a density that contributes to badly needed housing stock to cater for transient (but not FIFO) residents of Kununurra seeking to live in Kununurra without having to purchase a dwelling larger than their requirements.

One bedroom dwellings create an opportunity to create affordability for first home owners, without having to live in 'dormitory' suburbs where larger lots are prohibiting affordability.

Kununurra has a range of less formal accommodation types, such as caravans and 'dongas'. One bedroom dwellings create more opportunity for residents to transition from these other less formal forms of accommodation.

Residential development in the Town centre creates opportunities for more sustainable development, as residents can be less reliant on vehicles.

Although not providing diversity within this development we are providing diversity within the Town Centre . There have not been any recent developments that contain 1 bedroom units in the Town Centre. This has left a shortage which the proposed development is addressing.

Although on a recently created separate lot, the first stage of this development (next door) does provide some variety of unit types, with a number of 3 bedroom townhouses. The separate lot was created only for capital raising purposes, and the two lots effectively form staged development.

It has also become apparent from the first stage of the development on the adjoining lot that market demand is seeking 1 bedroom units, as these have been the most popular with buyers.

Should you have any enquiries regarding this matter or require additional information from our office, please do not hesitate to contact the undersigned on 08 9208 0111.

Yours sincerely

A handwritten signature in blue ink, appearing to read "K Williams".

Keith Williams  
Senior Town Planner  
WHELANS

## 15.1.2 Local Government Cash for Containers Advocacy Campaign

<b>DATE:</b>	20 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	Shire of Wyndham East Kimberley
<b>AUTHOR:</b>	Kevin Hannagan, Director Infrastructure
<b>REPORTING OFFICER:</b>	Kevin Hannagan, Director Infrastructure
<b>FILE NO:</b>	WM.15.1

### **PURPOSE**

To Gain Council's support for WALGA's advocacy campaign to encourage the State Government to implement a State based Cash for Containers Scheme (otherwise known as a Container Deposit Scheme).

### **BACKGROUND**

WALGA, through the Container Deposit Systems Policy Forum, has developed an advocacy campaign to encourage the State Government to implement a State based Cash for Containers Scheme (otherwise known as a Container Deposit Scheme). Implementing this type of Scheme on a National level has been discussed for a number of years. Environment Ministers will meet in August to decide what future action should occur nationally. However, it is debatable whether a decision will be made at this meeting to pursue the timely implementation of a national Cash for Containers Scheme. See below for further comment.

### **STATUTORY IMPLICATIONS**

There are no statutory implications associated with this report.

### **POLICY IMPLICATIONS**

No policy implications apply in the preparation of this report.

### **FINANCIAL IMPLICATIONS**

There are no financial implications associated with this item.

### **STRATEGIC IMPLICATIONS**

This report aligns with Council's focus on Environment, Key Result Area 4, in the *Shire of Wyndham East Kimberley Strategic Plan 2008*.

- Urban environment is enhanced to reflect our unique environment

### **COMMUNITY CONSULTATION**

Community consultation is not required in relation to this item.

### **COMMENT**

WALGA is initiating an advocacy campaign to ensure that a State based Scheme is implemented and is seeking Local Government's support. A tool kit for Local Government has been developed:

- Fact Sheet: to inform Local Government of the arguments made by the beverage industry, and additional facts that can be used when engaging with local members of Parliament (attached).

- Cash for Containers Event Planner: to assist Local Governments in planning a Cash for Containers event (attached). It is recommended that SWEK not participate in this part of the campaign given the availability of resources to manage such an event.

- Template of a letter: to be amended as appropriate and addressed to the local Member of Parliament, outlining support for a Cash for Containers Scheme (attached).

### **ATTACHMENTS**

Attachment 1 – Fact sheet

Attachment 2 – Cash for Containers Event Planner

Attachment 3 - Letter to Member of Parliament

### **VOTING REQUIREMENT**

Simple Majority

### **OFFICER'S RECOMMENDATION**

That Council:

1. supports WALGA's advocacy campaign to encourage the State Government to implement a State based Cash for Containers Scheme (otherwise known as a Container Deposit Scheme); and
2. sends the attached draft letter to local Member of Parliament and the Environment Minister.

### **COUNCIL DECISION**

**Moved: Cr J Parker**

**Seconded: Cr R Addis**

**That Council:**

1. **supports WALGA's advocacy campaign to encourage the State Government to implement a State based Cash for Containers Scheme (otherwise known as a Container Deposit Scheme); and**
2. **sends the attached draft letter to local Member of Parliament and the Environment Minister.**

**Carried Unanimously 6/0**

**COUNCIL DECISION**

**Minute No. 9874**

**Moved: Cr D Ausburn  
Seconded: Cr R Addis**

**That Council moves behind closed doors to consider Item 16.1.1, Proposed  
Kununurra Administration Centre, 20 Coolibah Drive, Kununurra.**

**Carried Unanimously 6/0**

**Council moves behind closed doors at 5:21pm**

## 16. MATTERS BEHIND CLOSED DOORS

### 16.1.1 Proposed Kununurra Administration Centre, 20 Coolibah Drive, Kununurra

<b>DATE:</b>	21 August 2012
<b>PROPONENT:</b>	Shire of Wyndham East Kimberley
<b>LOCATION:</b>	20 Coolibah Drive, Kununurra
<b>AUTHOR:</b>	Nick Kearns, Director Community Development
<b>REPORTING OFFICER:</b>	Nick Kearns, Director Community Development
<b>FILE NO:</b>	LP.02.41

The business of the meeting is of a confidential nature as it relates to the business affairs of a person. The item will be discussed Behind Closed Doors under Section 5.23 (2) (c) and (e) (ii) as this is a matter that affects:

c) A contract entered into, or which may be entered into, by the local government and which relates to a matter to be discussed at the meeting.

e) A matter that if disclosed, would reveal –

(ii) information that has a commercial value to a person.

#### **PURPOSE**

This report supports a proposal to purchase a property and refurbish it for the purposes of establishing a new Shire administration office in Kununurra.

#### **VOTING REQUIREMENT**

Absolute Majority

**The Officers Recommendation and Decisions are being withheld from the Minutes until transactions are complete as the content may impact on negotiations.**

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**COUNCIL DECISION**

**Minute No. 9879**

**Moved: Cr J Parker**

**Seconded: Cr R Addis**

**That Council moves out from behind closed doors.**

**Carried 6/0**

Council moves out from behind closed doors at 6:36pm

**On behalf of the Shire and Councillors, President John Moulden expressed appreciation to Therese Whalan, Executive Officer for all her hard work and dedication to the Shire for the past 1½ years.**

## **17. CLOSURE**

The Shire President declares the meeting closed at 6.36pm. Thank you to staff for attendance in gallery.